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503-588-6173*

DECISION OF THE PLANNING ADMINISTRATOR

URBAN GROWTH AREA PRELIMINARY DECLARATION / CLASS 3 SITE PLAN REVIEW / CLASS 2 DRIVEWAY APPROACH PERMIT CASE NO. UGA-SPR-DAP17-04

APPLICATION NO. : 17-110371-LD, 17-110376-RP

NOTICE OF DECISION DATE: July 26, 2017

SUMMARY: An Urban Growth Area Preliminary Declaration to determine the public facilities required for development of 3 acres and a Site Plan Review and Driveway Approach Permit for a towing company building and vehicle storage area located at 4350 Ridge Drive NE.

REQUEST: Consolidated application including an Urban Growth Area Preliminary Declaration, a Class 3 Site Plan Review, and a Class 2 Driveway Approach Permit for a towing company building, parking area, and vehicle storage area on property approximately 3 acres, zoned IG (General Industrial), and located at 4350 Ridge Drive NE (Marion County Map and Tax Lot Number 073W01C02000). The proposal consists of a towing company building office and garage, a caretaker's dwelling, a vehicle storage yard, and parking.

APPLICANT(S): Tobron Oregon LLC (Anthony L. Wiltse and Ronda R. Wiltse)

AGENT(S): Mark Rossi for Pinnacle Architecture, Inc.

LOCATION: 4350 Ridge Drive NE - 97301

CRITERIA: UGA Preliminary Declaration- SRC 200.025(d)(e)
Class 3 Site Plan Review- SRC 220.005(f)(3)
Class 2 Driveway Approach Permit- SRC 804.025(d)

FINDINGS: The findings are in the attached Order dated July 26, 2017.

DECISION: The Planning Administrator **APPROVED** Urban Growth Area Preliminary Declaration, Class 3 Site Plan Review, and Class 2 Driveway Approach Permit Case No. 17-04 is hereby GRANTED subject to SRC Chapter 200, 220, and 804, the applicable standards of the Salem Revised Code, conformance with the approved site plan included as Attachment B, and the following conditions of approval:

Condition 1. To meet boundary and linking street requirements, the applicant shall either:

- a. Construct a 30-foot-wide linking street along Ridge Drive NE and Claxter Road NE from the south line of the subject property to the intersection of Claxter Road NE and Salem Industrial Drive NE, and construct a three-quarter street improvement along the entire frontage of Ridge Drive NE; or

NOTICE OF DECISION

PLANNING DIVISION
555 LIBERTY ST. SE, RM 305
SALEM, OREGON 97301
PHONE: 503-588-6173
FAX: 503-588-6005

CITY OF Salem
AT YOUR SERVICE

- b. Pursuant to SRC 200.080(a), pay a temporary access fee of \$1,500 per average daily vehicle trip and complete a Temporary Facilities Access Agreement (TFAA).

Condition 2: Convey land for dedication equal to a half-width right-of-way of 30 feet from the centerline on the development side of Ridge Drive NE.

Condition 3: Provide a paved turnaround area for the proposed parking area.

Condition 4: Provide one paved loading space 12 feet in width, 40 feet in length, and 14 feet high (vertical clearance) or certify that the use does not require a delivery vehicle which exceeds a maximum combined vehicle and load rating of 8,000 pounds.

Condition 5: Provide a paved turnaround with an adequate width and turning radius for solid waste service vehicles.


Condition 6: Provide a solid waste service area meeting the standards of SRC 800.055.

Condition 7: Provide a radius along the north line of the driveways pursuant to PWDS. The southerly driveway approach on Ridge Drive NE shall be closed, as shown on the application materials.

The rights granted by the attached decision must be exercised, or an extension granted, as follows or this approval shall be null and void:

Urban Growth Area Preliminary Declaration	<u>August 11, 2019</u>
Class 3 Site Plan Review	<u>August 11, 2021</u>
Class 2 Driveway Approach Permit	<u>August 11, 2019</u>

Application Deemed Complete:	<u>June 19, 2017</u>
Notice of Decision Mailing Date:	<u>July 26, 2017</u>
Decision Effective Date:	<u>August 11, 2017</u>
State Mandate Date:	<u>October 17, 2017</u>

Case Manager: Pamela Cole, pcole@cityofsalem.net 

This decision is final unless written appeal from an aggrieved party is filed with the City of Salem Planning Division, Room 305, 555 Liberty Street SE, Salem OR 97301, **no later than 5:00 p.m., August 10, 2017.** The notice of appeal must contain the information required by SRC 300.1020 and must state where the decision failed to conform to the provisions of the applicable code section, SRC Chapter(s) 200, 220, and 804. The appeal must be filed in duplicate with the City of Salem Planning Division. The appeal fee must be paid at the time of filing. If the appeal is untimely and/or lacks the proper fee, the appeal will be rejected. The City Council will review the appeal at a public hearing. After the hearing, the City Council may amend, rescind, or affirm the action, or refer the matter to staff for additional information.

The complete case file, including findings, conclusions and conditions of approval, if any, is available for review at the Planning Division office, Room 305, City Hall, 555 Liberty Street SE, during regular business hours.

<http://www.cityofsalem.net/planning>

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BEFORE THE PLANNING ADMINISTRATOR OF THE CITY OF SALEM

**URBAN GROWTH AREA PRELIMINARY DECLARATION /
SITE PLAN REVIEW / DRIVEWAY APPROACH PERMIT
CASE NO. UGA-SPR-DAP17-04
DECISION**

IN THE MATTER OF APPROVAL OF)	UGA-SPR-DAP-
URBAN GROWTH AREA PRELIMINARY)	CASE NO. 17-04
DECLARATION, CLASS 3)	
SITE PLAN REVIEW, AND CLASS 2)	
DRIVEWAY APPROACH PERMIT)	
CASE NO. 17-04)	
4350 RIDGE DRIVE NE)	July 26, 2017

In the matter of the application for an Urban Growth Area Preliminary Declaration, Class 3 Site Plan Review and Class 2 Driveway Approach Permit submitted by Toby Wiltse for Tobron Oregon, LLC, applicant, and represented by Mark Rossi, Pinnacle Architecture, Inc., the Planning Administrator, having received and reviewed evidence and the application materials, makes the following findings and adopts the following order as set forth herein.

REQUEST

Summary: An Urban Growth Area Preliminary Declaration to determine the public facilities required for development of 3 acres and a Site Plan Review and Driveway Approach Permit for a towing company building and vehicle storage area located at 4350 Ridge Drive NE.

Request: Consolidated application including an Urban Growth Area Preliminary Declaration, a Class 3 Site Plan Review, and a Class 2 Driveway Approach Permit for a towing company building, parking area, and vehicle storage area on property approximately 3 acres, zoned IG (General Industrial), and located at 4350 Ridge Drive NE (Marion County Map and Tax Lot Number 073W01C02000). The proposal consists of a towing company building office and garage, a caretaker's dwelling, a vehicle storage yard, and parking.

A vicinity map is included as Attachment A.

DECISION

The Urban Growth Preliminary Declaration, Class 3 Site Plan Review, and Class 2 Adjustment application is **APPROVED** subject to the applicable standards of the Salem Revised Code, the findings contained herein, and the following conditions of approval:

Condition 1: To meet boundary and linking street requirements, the applicant shall either:

- a. Construct a 30-foot-wide linking street along Ridge Drive NE and Claxter Road NE from the south line of the subject property to the intersection of Claxter Road NE and Salem Industrial Drive NE, and construct a three-quarter street improvement along the entire frontage of Ridge Drive NE; or
- b. Pursuant to SRC 200.080(a), pay a temporary access fee of \$1,500 per average daily vehicle trip and complete a Temporary Facilities Access Agreement (TFAA).

Condition 2: Convey land for dedication equal to a half-width right-of-way of 30 feet from the centerline on the development side of Ridge Drive NE.

Condition 3: Provide a paved turnaround area for the proposed parking area.

Condition 4: Provide one paved loading space 12 feet in width, 40 feet in length, and 14 feet high (vertical clearance) or certify that the use does not require a delivery vehicle which exceeds a maximum combined vehicle and load rating of 8,000 pounds.

Condition 5: Provide a paved turnaround with an adequate width and turning radius for solid waste service vehicles.

Condition 6: Provide a solid waste service area meeting the standards of SRC 800.055.

Condition 7: Provide a radius along the north line of the driveways pursuant to PWDS. The southerly driveway approach on Ridge Drive NE shall be closed, as shown on the application materials.

FINDINGS

1. Class 3 Site Plan Review Applicability

Site plan review is intended to provide a unified, consistent, and efficient means to review proposed development that requires a building permit, other than single-family, duplex residential, and installation of signs, to ensure that such development meets all applicable requirements imposed by the Salem Revised Code (SRC). SRC 220.005(b)(3) requires Class 3 Site Plan Review for any development that requires a building permit, and that involves a land use decision or limited land use decision, as those terms are defined in ORS 197.015.

Class 3 Site Plan Review is required for this application pursuant to SRC 220.005(b)(3)(B) because a Class 2 Driveway Approach Permit is requested.

2. Background

On May 18, 2017, Class 3 Site Plan Review, Urban Growth Area Preliminary Declaration and Class 2 Driveway Approach applications were filed. Additional information was

requested from the applicant. After receiving additional information, the applications were deemed complete for processing and notice was mailed on June 19, 2017.

The applicant's proposed site plan (Sheets AS1.10 and C1.0) is included as Attachment B. The applicant's proposed utility plan (Sheet C3.0) is included as Attachment C.

Neighborhood and Citizen Comments:

Notice of the application was sent to the Northgate Neighborhood Association (Northgate) and all property owners of record within 250 feet of the subject property. No comments were received from Northgate or property owners prior to the deadline for comments.

City Department Comments:

The Public Works Department reviewed the proposal and provided a memo which is included as Attachment D.

The Building and Safety Division reviewed the proposal and indicated no site issues.

The Fire Department reviewed the proposal and indicated that they have no concerns with the Urban Growth Area Preliminary Declaration. However, they commented that the fire hydrant shown on the plans appears to be private. For the Fire Department to consider this hydrant, an emergency access easement shall need to be acquired and be recorded. The Fire Department will have requirements at the time plans are submitted for building permits.

Public Agency Comments:

Portland General Electric reviewed the proposal and commented, "Development cost per current tariff and service requirements. 10' Public Utility Easement required on all front street lots."

3. Analysis of Urban Growth Preliminary Declaration Approval Criteria

SRC 200.025(d) states:

The Director shall review a completed application for an Urban Growth Preliminary Declaration in light of the applicable provisions of the Master Plans and the Area Facility Plans and determine:

- (1) The required facilities necessary to fully serve the development; and
- (2) The extent to which the required facilities are in place or fully committed.

Analysis of the development based on relevant criteria in SRC 200 is as follows:

SRC 200.055 – Standards for Street Improvements

Finding: An adequate linking street is defined as the nearest point on a street that has a minimum 60-foot-wide right-of-way with a minimum 30-foot improvement for local streets or a minimum 34-foot improvement for major streets (SRC 200.055(b)). All streets abutting

the property boundaries shall be designed to the greater of the standards of SRC Chapter 803 and the standards of linking streets in SRC 200.055(b).

The existing streets adjacent to the subject property do not meet linking street standards. The nearest adequate linking street is located at the intersection of Claxter Road NE and Salem Industrial Drive NE. The applicant shall be required to convey land for dedication of right-of-way to equal 30 feet from centerline along the entire frontage of Ridge Drive NE. Boundary street improvements are required along the entire frontage of Ridge Drive NE, but may be satisfied through a deferral agreement or temporary facilities access agreement (TFAA). 22nd Avenue NE has a paved width meeting the alley definition and is not subject to boundary street requirements.

Pursuant to SRC 200.080, the proposed development meets the criteria for a temporary facilities access agreement because the existing configuration of Ridge Drive NE provides two travel lanes from the nearest adequate linking street to the subject property and has secondary access via 22nd Avenue NE. The temporary access fee for the subject property is based on approximately \$4.4 million in street improvements needed to improve Ridge Drive NE to local street standards. The Assistant City Traffic Engineer estimates approximately 2,933 average daily vehicle trips are anticipated from future developments located north of Claxter Road NE between Salem Parkway and Interstate 5 that will benefit from the required Ridge Drive NE improvements. The temporary access fee established in the Ridge Drive NE and 22nd Avenue NE area is \$1,500 per average daily vehicle trip based on the 2,933 total trips and 4.4 million dollars in required improvements. The methodology for vehicle trip calculation will be based on the current methodology for calculating vehicle trips to establish Transportation Systems Development Charges.

SRC 200.060 – Standards for Sewer Improvements

Finding: The proposed development shall be linked to adequate facilities by the construction of sewer lines and pumping stations, which are necessary to connect to such existing sewer facilities (SRC 200.060). The nearest available sewer facility is an 18-inch sewer line located in 22nd Avenue NE. No additional sewer mains are needed as a condition of the proposed development.

SRC 200.065 – Standards for Storm Drainage Improvements

Finding: The proposed development shall be linked to existing adequate facilities by the construction of storm drain lines, open channels, and detention facilities, which are necessary to connect to such existing drainage facilities. A 10-inch storm main is located in 22nd Avenue NE and is adequate to serve the proposed development.

SRC 200.070 – Standards for Water Improvements

Finding: The proposed development shall be linked to adequate facilities by the construction of water distribution lines, reservoirs, and pumping stations that connect to such existing water service facilities (SRC 200.070). Existing water mains abutting the subject property provide sufficient capacity to serve the subject property.

SRC 200.075 – Standards for Park Improvements

Finding: The subject property is non-residential. No parks facilities are required.

Based on the findings addressing the applicable criteria of SRC Chapter 200, the following conditions of approval shall apply to the requested UGA:

Condition 1: To meet boundary and linking street requirements, the applicant shall either:

- a. Construct a 30-foot-wide linking street along Ridge Drive NE and Claxter Road NE from the south line of the subject property to the intersection of Claxter Road NE and Salem Industrial Drive NE, and construct a three-quarter street improvement along the entire frontage of Ridge Drive NE; or
- b. Pursuant to SRC 200.080(a), pay a temporary access fee of \$1,500 per average daily vehicle trip and complete a Temporary Facilities Access Agreement (TFAA).

Condition 2: Convey land for dedication equal to a half-width right-of-way of 30 feet from the centerline on the development side of Ridge Drive NE.

4. Analysis of Class 3 Site Plan Review Approval Criteria

SRC 220.005(f)(3) states:

An application for Class 3 Site Plan Review shall be granted if:

- (1) The application meets all applicable standards of the UDC;
- (2) The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately;
- (3) Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians; and
- (4) The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.

Criterion 1:

The application meets all applicable standards of the UDC.

Finding: The applicant is requesting approval for construction of a new towing company building including an office and garage; an off-street parking area; and an unpaved vehicle storage area. An existing single family dwelling would be retained and used as a caretaker's dwelling. With conditions of approval, the proposed development meets all applicable standards

Development Standards – IG (General Industrial) Zone:

SRC 554.005(a) - Uses:

Except as otherwise provided in Chapter 554, the permitted, special, conditional and prohibited uses in the IG zone are set forth in Table 554-1.

Finding: The proposed use is classified as Motor Vehicle Services and is a Permitted use in the IG zone according to Table 554-1. The proposed development includes a 4,634-square foot enclosed building area and 1,157 covered but unenclosed area. The single family dwelling is classified as a dwelling unit for a caretaker on the premises being cared for or guarded and is also a Permitted use.

SRC 554.010(a) – Lot Standards:

There are no minimum lot area or dimension requirements in the IG zone. All uses are required to have a minimum of 16 feet of street frontage.

Finding: The property has frontage on Ridge Drive NE to the west and 22nd Avenue NE to the east, both which exceed the minimum.

SRC 554.010(b) – IG Zone Setbacks:

North: Abutting property is zoned IG (General Industrial). There is no minimum building setback. Vehicle use areas are required to have a perimeter setback a minimum of 5 feet in width with Type A landscaping, except vehicle storage areas in the IG zone are not required to have perimeter setbacks or landscaping.

South: Abutting property is zoned IG (General Industrial). There is no minimum building setback. Vehicle use areas are required to have a perimeter setback a minimum of 5 feet in width with Type A landscaping, except vehicle storage areas in the IG zone are not required to have perimeter setbacks or landscaping.

East: Adjacent to the east is the right-of-way of 22nd Avenue NE. There is a minimum building setback of 5 feet required adjacent to a street. Vehicle use areas are required to be set back a minimum of 6 to 10 feet from a street per SRC 806.035(c)(2), except vehicle storage areas in the IG zone are not required to have perimeter setbacks or landscaping.

West: Adjacent to the west is the right-of-way of Ridge Drive NE. There is a minimum building setback of 5 feet required adjacent to a street. Vehicle use areas are required to be set back a minimum of 6 to 10 feet from a street per SRC 806.035(c)(2), except vehicle storage areas in the IG zone are not required to have perimeter setbacks or landscaping.

Finding: The proposed building is located more than 150 feet from the proposed right-of-way on Ridge Drive NE; 240 feet from the right-of-way of 22nd Avenue NE; 12 feet from the north property line; and 188 feet from the south property line. The existing single family dwelling and garage are more than 100 feet from the proposed right-of-way on Ridge Drive NE; 320 feet from the right-of-way of 22nd Avenue NE; 125 feet from the north property line; and 9 feet from the south property line. The proposed paved vehicle use areas comply with the minimum setback standards of the IG zone and SRC Chapter 806. The fenced, graveled compound area and graveled driveway are vehicle storage areas and are not required to have perimeter setbacks or landscaping; they are set back 3 feet from the north property line; 5'6" from the south property line; more than 30 feet from the east

property line; and more than 180 feet from the west property line. The proposal meets all applicable setbacks

SRC 554.010(c) - Lot Coverage, Height:

There is no maximum lot coverage standard. The maximum building height requirement in the IG zone is 70 feet.

Finding: The proposed building heights are less than 30 feet.

SRC 554.010(d) - Landscaping:

- (1) **Setbacks.** Required setbacks shall be landscaped. Landscaping shall conform to the standards set forth in SRC Chapter 807.
- (2) **Vehicle Use Areas.** Vehicle use areas shall be landscaped as provided under SRC Chapter 806 and SRC Chapter 807.

Finding: The setbacks for the vehicle use area shall be landscaped with a minimum of 1 plant unit per 20 square feet of landscaped area. The development standards of the IG zone for buildings do not require landscaping abutting other properties in the IG zone. No landscaped setbacks are required between the vehicle storage area and property lines. A minimum of 40 percent of the required number of plant units shall be a combination of mature trees, shade trees, evergreen/conifer trees, or ornamental trees. Plant materials and minimum plant unit values are defined in SRC Chapter 807, Table 807-2.

All building permit applications for development subject to landscaping requirements shall include landscape and irrigation plans meeting the requirements of SRC Chapter 807.

Off-Street Parking, Loading, and Driveways SRC 806

SRC 806.005 - Off-Street Parking; When Required.

Off-street parking shall be provided and maintained for each proposed new use or activity.

SRC 806.010 - Proximity of Off-Street Parking to Use or Activity Served.

Required off-street parking shall be located on the same development site as the use or activity it serves, or within the IG zone, off-street parking may be located within 500 feet of the development site containing the use or activity it serves.

SRC 806.015 - Amount of Off-Street Parking.

- a) *Minimum Required Off-Street Parking.* The minimum off-street parking requirement for uses in the Motor Vehicle Services category is 1 space per 900 square feet of floor area. The minimum parking for the caretaker's dwelling is two spaces.
- b) *Compact Parking.* Up to 75 percent of the minimum off-street parking spaces required under this Chapter may be compact parking spaces.
- c) *Carpool and Vanpool Parking.* New developments with 60 or more required off-street parking spaces, and falling within the Public Services and Industrial use classifications, and Business and Professional Services use category, shall designate a minimum of 5 percent of their total off-street parking spaces for carpool or vanpool parking.

- d) *Maximum Off-Street Parking.* Unless otherwise provided in the SRC, off-street parking shall not exceed the amounts set forth in Table 806-2. If the minimum number of spaces equals 20 spaces or less, the maximum allowed is 2.5 times the minimum. If the minimum number of spaces equals more than 20 spaces, the maximum allowed is 1.75 times the minimum.

Finding: The proposed building has a floor area of approximately 5,791 square feet, requiring a minimum of 6 off-street parking spaces ($5,791 / 900 = 6.43$). The maximum off-street parking is 15 parking spaces ($6 \times 2.5 = 15$). The site plan indicates there are 7 parking spaces within the vehicle use area.

The caretaker's dwelling requires two off-street parking spaces. The garage and driveway fulfill this requirement.

- a) *General Applicability.* The off-street parking and vehicle use area development standards set forth in this section apply to the development of new off-street parking and vehicle use areas.
- b) *Location.* Off-street parking and vehicle use areas shall not be located within required setbacks.
- c) *Perimeter Setbacks and Landscaping.* Perimeter setbacks shall be required for off-street parking and vehicle use areas abutting streets, abutting interior front, side, and rear property lines, and adjacent to buildings and structures.

Finding: Perimeter setbacks for the off-street parking area are identified in the building and vehicle use area setback findings above. The proposed development is consistent with the setback requirements of the IG zone (SRC Chapter 554) and SRC Chapter 806 and the SRC Chapter 806 setback requirements of 5 feet adjacent to a building or structure.

- d) *Interior Landscaping.* Interior landscaping shall be provided in amounts not less than those set forth in Table 806-5. For parking areas 5,000-50,000 square feet in size, a minimum of 5 percent of the interior parking area shall be landscaped. A minimum of one deciduous shade tree shall be planted for every 12 parking spaces. Landscape islands and planter bays shall have a minimum planting area of 25 square feet and minimum width of 5 feet.

Finding: The proposed site plan indicates that the off-street parking area is approximately 3,407 square feet, requiring no interior parking lot landscaping.

- e) *Off-Street Parking Area Dimensions.* Off-street parking areas shall conform to the minimum dimensions set forth in Table 806-6.

Finding: The proposed parking spaces, driveway and drive aisle for the off-street parking area meet the minimum dimensional requirements of SRC Chapter 806.

- f) *Additional Off-Street Parking Development Standards 806.035(f)-(m).*

Finding: The proposed off-street parking area is developed consistent with the additional development standards for grade, surfacing, and drainage. Bumper guards and wheel barriers are not required for the parking area. The parking area striping, marking, signage and lighting shall be consistent with SRC Chapter 806. Off-street parking area screening is not required for the proposed parking area.

SRC 806.040 - Driveway Development Standards.

- a) *Access.* Off-street parking and vehicle use areas shall have either separate driveways for ingress and egress, a single driveway for ingress and egress with an adequate turnaround that is always available or a loop to the single point of access.
- b) *Location.* Driveways shall not be located within required setbacks.
- c) *Additional Development Standards 806.040(c)-(g).*

Finding: The interior driveway proposed for the off-street parking area does not conform to the driveway location and dimensional requirements of SRC 806.040 because there is a single driveway but there is no turnaround or loop. To comply with the requirements, the following condition is necessary:

Condition 3: Provide a paved turnaround area for the proposed parking area.

Bicycle Parking

SRC 806.045 - General Applicability.

Bicycle parking shall be provided and maintained for each proposed new use or activity.

SRC 806.055 - Amount of Bicycle Parking.

The minimum bicycle parking requirement for uses in the Motor Vehicle Services category is 1 space per 9,000 square feet of floor area.

Finding: The site plan indicates one bicycle rack, which meets the standard.

SRC 806.060 - Bicycle Parking Development Standards.

- a) *Location.* Bicycle parking areas shall be located within a convenient distance of, and shall be clearly visible from, the primary building entrance. In no event shall bicycle parking areas be located more than 50 feet from the primary building entrance.
- b) *Access.* Bicycle parking areas shall have direct and accessible access to the public right-of-way and the primary building entrance.
- c) *Dimensions.* Bicycle parking spaces shall be a minimum of 6 feet by 2 feet, and shall be served by a minimum 4-foot-wide access aisle.
- d) *Bicycle Racks.* Where bicycle parking is provided in racks, the racks may be floor, wall, or ceiling racks. Bicycle racks shall accommodate the bicyclist's own locking device.

Finding: The proposed site plan indicates that bicycle parking will be provided within 50 feet of the primary building entrance. At the time of building permit review, the plans will be verified for conformance with the bicycle development standards of SRC Chapter 806 and minimum number of required spaces.

Off-Street Loading Areas

SRC 806.065 - General Applicability.

Off-street loading areas shall be provided and maintained for each proposed new use or activity.

SRC 806.075 - Amount of Off-Street Loading.

A minimum of one off-street loading space 12 feet in width, 40 feet in length, and 14 feet high (vertical clearance) is required for each building between 5,000 and 100,000 square feet. An off-street parking area meeting the requirements of this Chapter may be used in place of a required off-street loading space when the use or activity does not require a delivery vehicle which exceeds a maximum combined vehicle and load rating of 8,000 pounds and the off-street parking area is located within 25 feet of the building or the use or activity that it serves.

Finding: For the proposed building of 5,791 square feet, one loading space 12 feet in width, 40 feet in length, and 14 feet high (vertical clearance) is required, or the applicant may certify that the use does not require a delivery vehicle which exceeds a maximum combined vehicle and load rating of 8,000 pounds and the off-street parking area is located within 25 feet of the building or the use or activity that it serves. The proposed site plan has no paved off-street loading area consistent with the requirements of SRC Chapter 806.

Condition 4: Provide one paved loading space 12 feet in width, 40 feet in length, and 14 feet high (vertical clearance) or certify that the use does not require a delivery vehicle which exceeds a maximum combined vehicle and load rating of 8,000 pounds.

Landscaping

Landscape and irrigation plans will be reviewed for conformance with the following requirements of SRC 806 and SRC 807 at the time of building permit application review.

The minimum landscaped area between the building and Ridge Drive NE right-of-way is approximately 215 square feet on the west side of the building (43-foot building width x 5-foot-wide setback). The minimum landscaped area between the parking and vehicle use area and Ridge Drive NE is 200 square feet (20-foot width x 10-foot-wide setback). The minimum landscaped area between the vehicle use area and the abutting IG zoned property to the north is 270 square feet (54 feet x 5 feet = 270). The minimum landscaped area between the vehicle use area and abutting IG zoned property to the south is 413 square feet on the west side (82.5-foot x 5-foot-wide setback). Because the vehicle use area is located between the building and parking area and 22nd Avenue NE, no landscaping is required abutting 22nd Avenue NE. The minimum number of plant units is 55 ($1,098/20 = 54.9$), and at least 40 percent of the plant units, or 22 ($55 \times 0.4 = 22$) shall

be a combination of mature trees, shade trees, evergreen/conifer trees, or ornamental trees. Existing vegetation and landscaping appear to exceed the minimum requirements.

Natural Resources

SRC 601 – Floodplain: The property is not within the 100-year floodplain.

SRC 808 - Preservation of Trees and Vegetation: The City's tree preservation ordinance, under SRC Chapter 808, provides that no person shall remove a significant tree (Oregon White Oak greater than 24 inches in diameter at breast height) (SRC 808.015) or a tree or native vegetation in a riparian corridor (SRC 808.020), unless the removal is excepted under SRC 808.030(a)(2), undertaken pursuant to a permit issued under SRC 808.030(d), undertaken pursuant to a tree conservation plan approved under SRC 808.035, or permitted by a variance granted under SRC 808.045. No protected trees have been identified on the site plan for removal.

SRC 809 - Wetlands: Grading and construction activities within wetlands are regulated by the Oregon Department of State Lands (DSL) and US Army Corps of Engineers. State and Federal wetland laws are also administered by the DSL and Army Corps, and potential impacts to jurisdictional wetlands are addressed through application and enforcement of appropriate mitigation measures.

According to the Salem-Keizer Local Wetland Inventory (LWI) there are no wetlands but there are hydric soil inclusions mapped on the entire subject property. The applicant should contact the Department of State Lands to verify if permits are required for the proposed development.

SRC 810 - Landslide Hazards: A geological assessment or report is required when regulated activity is proposed in a mapped landslide hazard area. The subject property contains no mapped landslide hazard points. The proposed commercial development is assigned 3 activity points. A total of 3 points indicates a low landslide hazard risk, and no geological assessment or report is required.

Solid Waste Service Areas

SRC 800.055(b) – Solid Waste Receptacle Placement Standards.

All solid waste receptacles shall be placed at grade on a concrete pad that is a minimum of 4 inches thick, or on an asphalt pad that is a minimum of 6 inches thick. The pad shall have a slope of no more than 3 percent and shall be designed to discharge stormwater runoff.

1) Pad area. In determining the total concrete pad area for any solid waste service area:

- a. The pad area shall extend a minimum of 1-foot beyond the sides and rear of the receptacle.
- b. The pad area shall extend a minimum 3 feet beyond the front of the receptacle.

Staff Response: The site plan did not include full plans for the solid waste service area. Compliance will be verified at building permit.

2) Minimum Separation.

- a. A minimum separation of 1.5 feet shall be provided between the receptacle and the side wall of the enclosure.
- b. A minimum separation of 5 feet shall be provided between the receptacle and any combustible walls, combustible roof eave lines, or building or structure openings.

Staff Response: The site plan did not include full plans for the solid waste service area. Compliance will be verified at building permit.

3) Vertical Clearance.

- a. Receptacles 2 cubic yards or less in size shall be provided with a minimum of 8 feet of unobstructed overhead or vertical clearance for servicing.
- b. Receptacles greater than 2 cubic yards in size shall be provided with a minimum of 14 feet of unobstructed overhead or vertical clearance for servicing.

Staff Response: The proposed solid waste service area is uncovered and would meet this standard. Compliance will be verified at building permit.

SRC 800.055(d) – Solid Waste Service Area Screening Standards.

- 1) Solid waste, recycling, and compostable service areas shall be screened from all streets abutting the property and from all abutting residentially zoned property by a minimum 6-foot-tall sight-obscuring fence or wall; provided, however, where receptacles, drop boxes, and compactors are located within an enclosure, screening is not required. For the purpose of this standard, abutting property shall also include any residentially zoned property located across an alley from the property.

Staff Response: The site plan indicates that the solid waste service area would be screened with a 6-foot-tall sight-obscuring fence and would meet this standard. Compliance will be verified at building permit.

SRC 800.055(f) – Solid Waste Service Area Vehicle Access.

1) Vehicle Operation Area.

- a. A vehicle operation area shall be provided for solid waste collection service vehicles that is free of obstructions and no less than 45 feet in length and 12 feet in width. Vehicle operation areas shall be made available in front of every receptacle, or, in the case of multiple receptacles within an enclosure, in front of every enclosure opening.
- b. For solid waste service areas having receptacles of 2 cubic yards or less, the vehicle operation area may be located:
 - (i) Directly in front of the permanent location of the receptacle; or
 - (ii) In a location where the receptacle can be safely maneuvered manually not more than 45 feet into a position at one end of the vehicle operation area for receptacle servicing.

c. The vehicle operation area may be coincident with a parking lot drive aisle or driveway, provided that such area is kept free of parked vehicles and other obstructions at all times except for the normal ingress and egress of vehicles.

Staff Response: The site plan indicates a vehicle operation area meeting the standards for receptacles of 2 cubic yards or less. If larger containers will be used, the vehicle operation area must be perpendicular to the front opening of the enclosure. Compliance will be verified at building permit.

d. In the event that access to the vehicle operation area is not a direct approach into position for operation of the service vehicle, a turnaround, designed and constructed pursuant to the Public Works Design Standards, shall be required to allow safe and convenient access for collection service.

Staff Response: The site plan indicates a vehicle operation area parallel to the opening of the enclosure. With the proposed configuration, a vehicle would be able to back up into the driveway that leads to the caretaker dwelling in order to turn around. Compliance will be verified at building permit.

2) Vehicle operation areas shall be designed so that waste collection service vehicles are not required to back onto a public street or leave the premises.

Staff Response: The site plan indicates a vehicle operation area location that would not require backing onto a public street or leaving the premises. With the proposed location of the solid waste service area, a vehicle would be able to back up approximately 125 feet into the driveway that leads to the caretaker dwelling in order to turn around. However, the proposed design of the caretaker driveway does not appear to provide a turning radius that would be sufficient for a large vehicle. In order to ensure compliance with the standards at building permit, the following condition is required:

Condition 5: Provide a paved turnaround with an adequate width and turning radius for solid waste service vehicles.

3) Vehicle operation areas shall be paved with asphalt, concrete, or other hard surfacing approved by the Public Works Director, and shall be adequately designed, graded, and drained to the approval of the Public Works Director.

4) Signs. "No Parking" signs shall be placed in a prominent location on the enclosure, or painted on the pavement in front of the enclosure or receptacle, to ensure unobstructed and safe access for the servicing of receptacles.

Staff Response: The applicant's site plan includes an uncovered solid waste service area 10 feet deep by 14 feet wide with a 4-inch-thick concrete pad and screened with a 6-foot chain link fence with vinyl slats. The location of the service area is acceptable if containers are 2 cubic yards or smaller.

The following condition is required to ensure compliance with all of the applicable standards:

Condition 6: Provide a solid waste service area meeting the standards of SRC 800.055.

Criterion 2:

The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately.

Finding: The existing street system is adequate to serve the proposed development. However, the existing configuration of Ridge Drive NE does not meet current standards for its classification of street per the Salem TSP. Required street improvements and right-of-way dedication are specified in the conditions of approval consistent with SRC Chapter 803. The applicant shall be required to convey land for dedication of right-of-way to equal 30 feet from centerline along the entire frontage of Ridge Drive NE. The boundary street improvements may be satisfied through a deferral agreement or temporary facilities access agreement.

Criterion 3:

Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians.

Finding: The driveway accesses onto Ridge Drive NE and 22nd Avenue NE provide for safe turning movements into and out of the property.

Criterion 4:

The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.

Finding: The Public Works Department has reviewed the applicant's preliminary utility plan for this site. The water, sewer, and storm infrastructure are available within surrounding streets / areas and appear to be adequate to serve the proposed development. The applicant shall design and construct all utilities including sewer, water, and storm drainage according to the PWDS and to the satisfaction of the Public Works Director. The applicant is advised that a sewer monitoring manhole may be required, and the trash area shall be designed in compliance with Public Works Standards.

The proposed plan demonstrates compliance with PWDS Appendix 4E related to green stormwater infrastructure by setting aside at least ten percent of the total new impervious surface area for installation of green stormwater infrastructure. The applicant's engineer shall design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

5. Analysis of Class 2 Driveway Approach Permit Criteria

SRC 804.025(d) states:

A Class 2 Driveway Approach Permit shall be granted if:

- 1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;
- 2) No site conditions prevent placing the driveway approach in the required location;
- 3) The number of driveway approaches onto an arterial are minimized;
- 4) The proposed driveway approach, where possible:
 - i. Is shared with an adjacent property; or
 - ii. Takes access from the lowest classification of street abutting the property;
- 5) The proposed driveway approach meets vision clearance standards;
- 6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;
- 7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;
- 8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and
- 9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Criterion 1:

The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards.

Finding: The proposed driveways will be modified to meet the standards for SRC 804 and PWDS.

Criterion 2:

No site conditions prevent placing the driveway approach in the required location.

Finding: There are no site conditions prohibiting the location of the proposed driveways.

Criterion 3:

The number of driveway approaches onto an arterial are minimized.

Finding: The proposed driveways are not accessing an arterial street.

Criterion 4:

The proposed driveway approach, where possible:

- a) Is shared with an adjacent property; or
- b) Takes access from the lowest classification of street abutting the property.

Finding: The proposed driveways currently access the lowest classification of street abutting the subject property.

Criterion 5:

The proposed driveway approach meets vision clearance standards.

Finding: The proposed driveways meet the PWDS vision clearance standards set forth in SRC Chapter 805.

Criterion 6:

The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access.

Finding: The proposed driveways will not create a known traffic hazard and will provide for safe turning movements for access to the subject property.

Criterion 7:

The proposed driveway approach does not result in significant adverse impacts to the vicinity.

Finding: The driveway approach configurations provide sufficient width to accommodate truck turning movements into the subject property. The applicant shall provide turning radii on both sides of the driveway approaches. To ensure compliance with this standard, the following condition is required:

Condition 7: Provide a radius along the north line of the driveways pursuant to PWDS. The southerly driveway approach on Ridge Drive NE shall be closed, as shown on the application materials.

Criterion 8:

The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections.

Finding: The proposed driveway approaches are located on a local street.

Criterion 9:

The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Finding: The proposed driveway approaches are not located in the vicinity of a residentially zoned area.

6. Based upon review of SRC Chapters 200, 220, and 804, the applicable standards of the Salem Revised Code, the findings contained herein, and due consideration of

comments received, the application complies with the requirements for an affirmative decision.

ORDER

Final approval of Urban Growth Area Preliminary Declaration, Class 3 Site Plan Review, and Class 2 Driveway Approach Permit Case No. 17-04 is hereby GRANTED subject to SRC Chapter 200, 220, and 804, the applicable standards of the Salem Revised Code, conformance with the approved site plan included as Attachment B, and the following conditions of approval:

Condition 1: To meet boundary and linking street requirements, the applicant shall either:

- a. Construct a 30-foot-wide linking street along Ridge Drive NE and Claxter Road NE from the south line of the subject property to the intersection of Claxter Road NE and Salem Industrial Drive NE, and construct a three-quarter street improvement along the entire frontage of Ridge Drive NE; or
- b. Pursuant to SRC 200.080(a), pay a temporary access fee of \$1,500 per average daily vehicle trip and complete a Temporary Facilities Access Agreement (TFAA).

Condition 2: Convey land for dedication equal to a half-width right-of-way of 30 feet from the centerline on the development side of Ridge Drive NE.


Condition 3: Provide a paved turnaround area for the proposed parking area.

Condition 4: Provide one paved loading space 12 feet in width, 40 feet in length, and 14 feet high (vertical clearance) or certify that the use does not require a delivery vehicle which exceeds a maximum combined vehicle and load rating of 8,000 pounds.

Condition 5: Provide a paved turnaround with an adequate width and turning radius for solid waste service vehicles.

Condition 6: Provide a solid waste service area meeting the standards of SRC 800.055.

Condition 7: Provide a radius along the north line of the driveways pursuant to PWDS. The southerly driveway approach on Ridge Drive NE shall be closed, as shown on the application materials.



Pamela Cole,
Planning Administrator Designee

Prepared by Pamela Cole, Planner II

Attachments: A. Vicinity Map

- B. Proposed Site Plan
- C. Utility Plan
- D. Public Works Memorandum

Application Deemed Complete: June 19, 2017
Notice of Decision Mailing Date: July 26, 2017
Decision Effective Date: August 11, 2017
State Mandated Decision Date: October 17, 2017

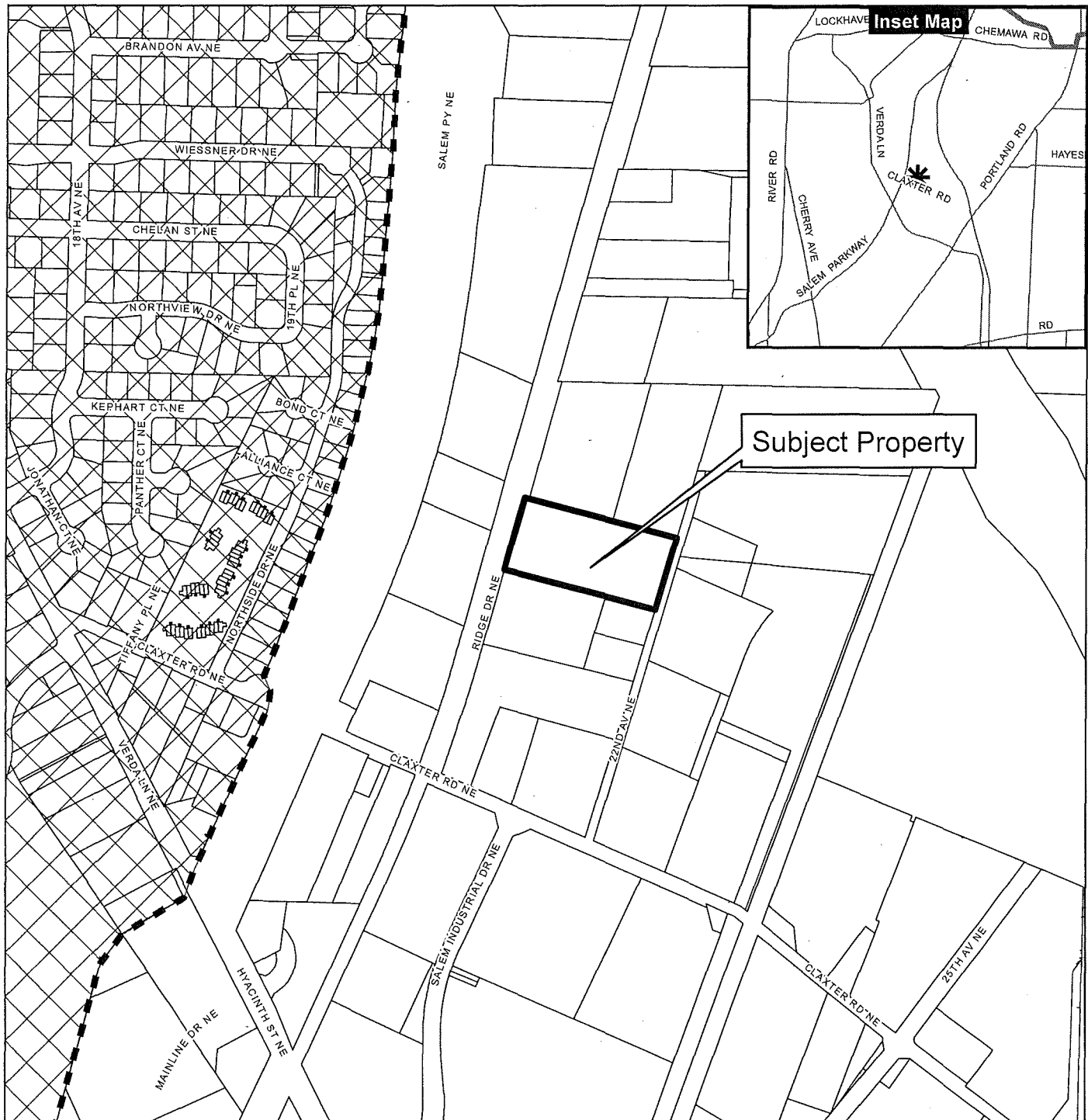
The rights granted by the attached decision for Urban Growth Area Preliminary Declaration No. UGA-SPR-DAP 17-04 must be exercised or an extension granted by August 11, 2019 or this approval shall be null and void. The rights granted by the attached decision for Class 3 Site Plan Review Case No. UGA-SPR-DAP 17-04 must be exercised by August 11, 2021 or this approval shall be null and void. The rights granted by the attached decision for Class 2 Driveway Approach Permit Case No. UGA-SPR-DAP 17-04 must be exercised or an extension granted by August 11, 2019 or this approval shall be null and void.

This decision is final unless written appeal from a party with standing to appeal, along with the appeal fee, is received by the City of Salem Planning Division, Room 320, 555 Liberty Street SE, Salem, OR 97301, **not later than 5:00 pm, August 10, 2017**. The notice of appeal must contain the information required by SRC 300.1020. The appeal fee must be paid at the time of filing. If the notice of appeal is untimely and/or lacks the proper fee, the notice of appeal will be rejected. The Salem City Council will review the appeal at a public hearing. The City council may amend, rescind, or affirm the action or refer the matter to staff for additional information.

Any person with standing may appeal the decision by filing an appeal with the applicable appeal fee with the City of Salem not later than fifteen (15) days after the date this decision is mailed to persons with standing to appeal.

<http://www.cityofsalem.net/planning>

Vicinity Map 4350 Ridge Drive NE



Legend

Taxlots

Urban Growth Boundary

City Limits

Outside Salem City Limits

Historic District

Schools

Parks

CITY OF Salem
AT YOUR SERVICE
Community Development Dept.

0 100 200 400 Feet



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LINE LEGEND	
--- SAN ---	SANITARY SINKER
--- WAT ---	WASTEWATER
--- GAS ---	NATURAL GAS LINE
--- UTILITY ---	UTILITY TRENCH
--- TEL ---	TELEPHONE LINE
--- UG ---	UNDERGROUND POWER LINE
--- STORM ---	STORM SEWER LINE
--- VIS ---	CLEAR VISION AREA
--- EDP ---	SPRINKLER LINE
--- EDP ---	UNDERGROUND POWER
--- CTV ---	CABLE LINE
--- FENCE ---	FENCE LINE
--- PROPERTY ---	PROPERTY LINE
--- SETBACK ---	SETBACK LINE

SHEET NOTES

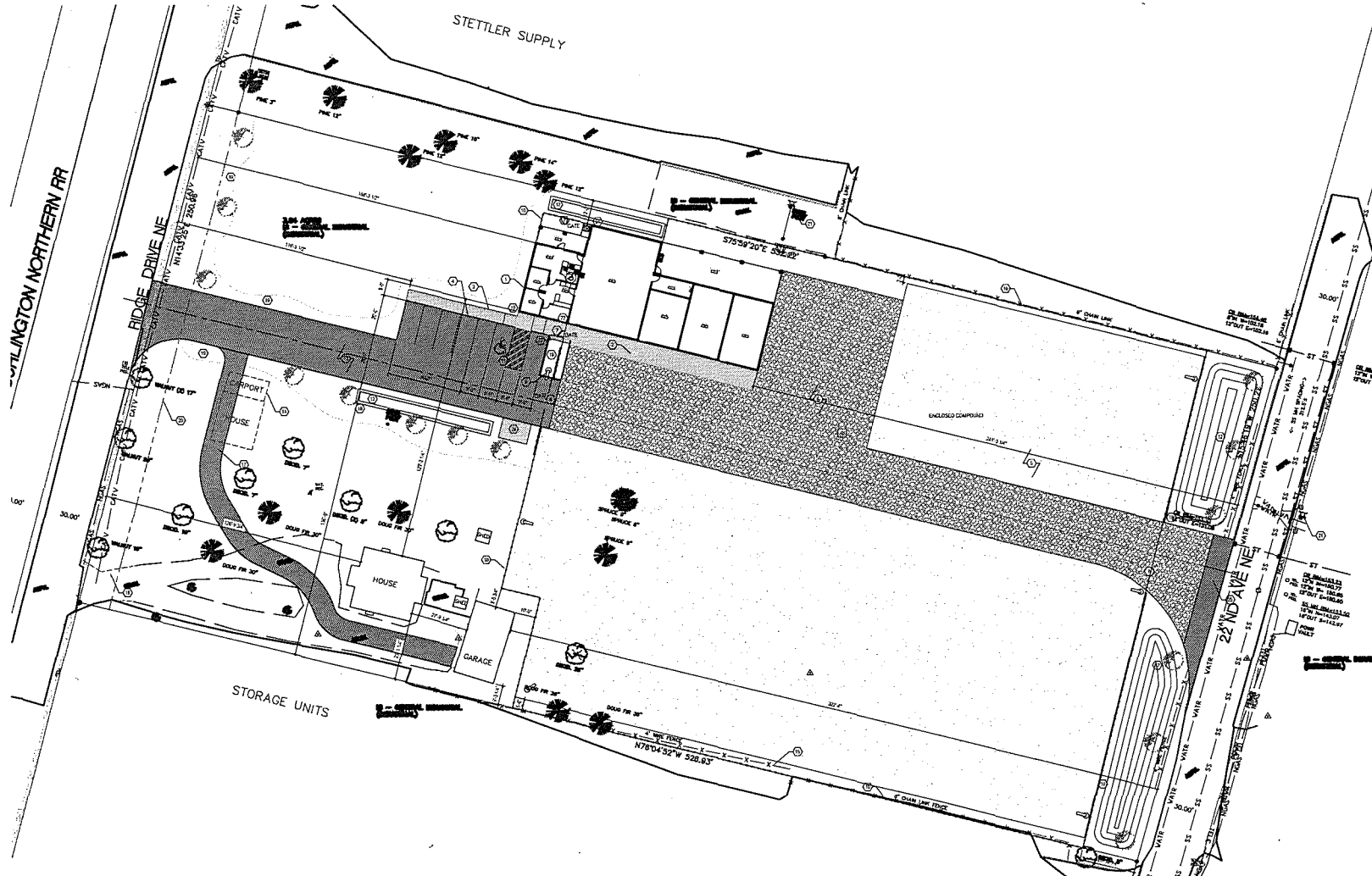
SEE CHS DRAWINGS FOR UTILITIES AND MORE SPECIFIC INFORMATION

KEYED NOTES

- 1 BUILDING LINE
- 2 4" CONCRETE APRON
- 3 4" CONCRETE SIDEWALK
- 4 8" CURB
- 5 HEAVY GRAVEL SECTION
- 6 STANDARD GRAVEL SECTION
- 7 30" MAN GATE
- 8 VEHICLE GATE
- 9 16" TALL SETBACK PLACEMENT

- 10 8" CURB LANE
- 11 ACCESSIBLE PAVING DETAIL WITH CHAIR APPROACH SIGNAGE
- 12 DRAINAGE BASIN OF LANDSCAPING
- 13 HEAVY ASPHALT SECTION
- 14 DEMO EXISTING BUILDING
- 15 DEMO EXISTING FENCE
- 16 ASPHALT APPROACH
- 17 12" WIDE ASPHALT DRIVEWAY
- 18 BURD ACCESS DRIVE

- 19 PROPOSED LANDSCAPE, SEE CHS
- 20 PROPOSED 30" A.D. ELEVATION
- 21 EXISTING FENCE REMAIN
- 22 BUILDING ENTRY
- 23 ONE LANE DRIVE (SHARPER)
- 24 GAS/SEWER STRUCTURE
- 25 8" APRON
- 26 TRAMP LOCATION: NO OVERHEAD BARBERS (SHARP) CONCRETE FINE WHITE GRANULAR FENCE WITH VINYL SLATS, INSTALL IN ACCORDANCE WITH LSC RULES

1 SITE PLAN
1" = 20' 0"

pinnacle
architecture

"ENHANCING LIVES AND COMMUNITIES"

1545 SW BUCK DR, SUITE 101
BEND OR 97701
503.364.0877
WWW.PINNACLEARCHITECTURE.COM



WILTSE TOWING

CLIENT: TOWSON ORIGIN, LLC

PROJECT ADDRESS:
4510 RIDGE DRIVE NE
SALIDA, OREGON

Project Status: _____ Issue Date: _____
DATE: 2/24/2016

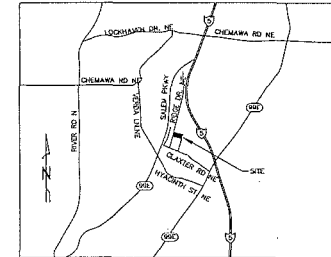
PROJECT NO: _____ SCALE: _____
DESIGN BY: _____ DATE: _____
DRAWN BY: _____ DATE: _____

SITE PLAN

AS1.10

SITE PLAN 4350 RIDGE DRIVE NE

SALEM, OREGON 97301
CHERRYDALE TRACTS, LOT FR 3
LOCATED IN THE SE 1/4 OF SECTION 1,
TOWNSHIP 7 SOUTH, RANGE 3 WEST, W.M.,
MARION COUNTY, OREGON



VICINITY MAP
SCALE: NOT TO SCALE

GENERAL INFORMATION

DEVELOPER:
VISTA PROPERTY INVESTMENTS LLC
1081 WILTS
P.O. BOX 4657
SALEM, OREGON 97301

ARCHITECT:
PRINCE & ARCHITECTURE
MARK ROSS - PROJECT MGR
980 SW 10TH DRIVE, SUITE 101
BEND, OREGON 97702

TOPOGRAPHY SURVEY:
LAZER SITE SURVEYING/REVERSE
RICHARD J. GIBSON
2003 25TH STREET SE
SALEM, OREGON 97302

ENGINEERING:
TTE ENGINEERING & SURVEYING, INC.
DAN P. DURYEE
725 NW HILL STREET
BEND, OREGON 97703

SITE ADDRESS:
4350 RIDGE DRIVE SE
SALEM, OREGON 97301

TAX LOT: 073W01C002000
ZONE: IG - GENERAL INDUSTRIAL
AREA: 3.04 GROSS ACRES, 2.96 NET ACRES
SETBACKS: 0 FT BUILDING, 5 FT VEHICLES

POWER: PGE
GAS: NW NATURAL GAS
PHONE: QWEST
CABLE: COMCAST

EXISTING CONDITIONS: 2 RESIDENTIAL HOMES, GARAGE AND OUT
BUILDINGS, NATIVE VEGETATION/GRASS

PROPOSED USE: NEW INDUSTRIAL BUILDING FOR TOW TRUCK
COMPANY, PAVED & GRAVEL DRIVEWAYS AND PARKING AREAS.
ONSITE: CARETAKER IN EXISTING HOUSE.

FLOOD PLAIN: THE SUBJECT PROPERTY IS LOCATED IN ZONE X,
DETERMINED AS AREAS DETERMINED TO OUTSIDE 500-YEAR
FLOODPLAIN, PER FEMA FLOOD INSURANCE RATE MAP MARION
COUNTY, OREGON AND INCORPORATED AREAS, PANEL 332 OF
1500 MAP NUMBER 416470332; EFFECTIVE DATE: JANUARY 19,
2000.

LEGEND

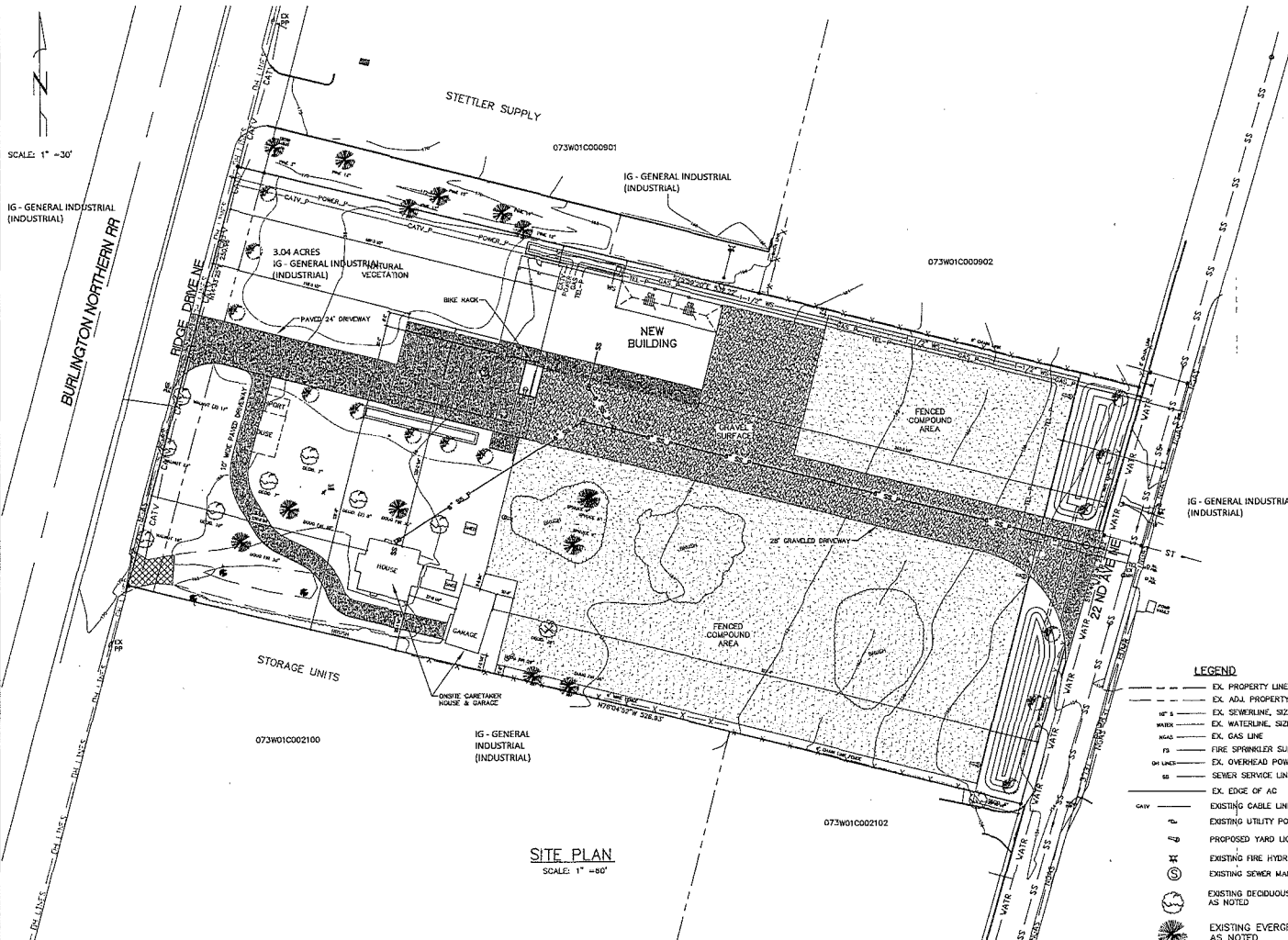
- EX. PROPERTY LINES
- EX. ADJ. PROPERTY LINES
- EX. SEWERLINE, SIZE AS INDICATED
- EX. WATERLINE, SIZE AS INDICATED
- EX. GAS LINE
- FS FIRE SPRINKLER SUPPLY LINE
- EX. OVERHEAD POWER LINE
- SEWER SERVICE LINE
- EX. EDGE OF AC
- EXISTING CABLE LINES
- EXISTING UTILITY POLE
- PROPOSED YARD LIGHT
- EXISTING FIRE HYDRANT
- EXISTING SINKER MANHOLE
- EXISTING DECIDUOUS TREES AS NOTED
- EXISTING EVERGREEN TREES AS NOTED
- TREES TO BE REMOVED

SHEET INDEX

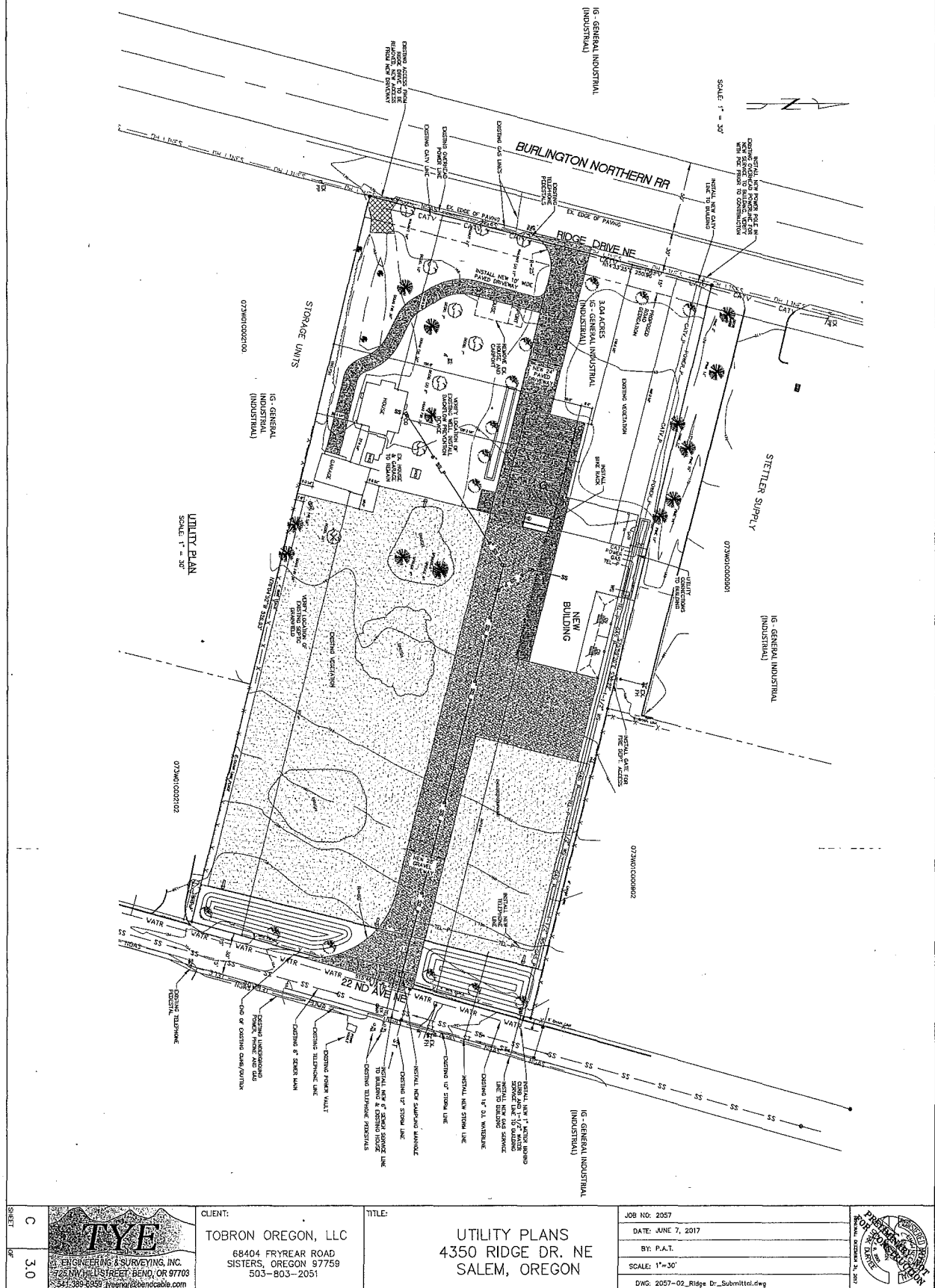
- C1.0 COVER SHEET, APPROVALS
- C2.0 EXISTING CONDITIONS
- C3.0 UTILITY PLANS
- C4.0 GRADING PLANS
- ESP1.0 EROSION & SEDIMENT CONTROL COVER
- ESP2.0 EROSION & SEDIMENT CONTROL PLAN
- ESP3.0 EROSION & SEDIMENT CONTROL DETAILS

SITE PLAN

SCALE: 1" = 80'




JOB NO: 2057	DATE: JUNE 7, 2017
BY: P.A.L.	SCALE: AS SHOWN
DWG: 2057-02_Ridge Dr_Submittal.dwg SUBMITTED: DECEMBER 31, 2017	
COVER - SITE PLAN 4350 RIDGE DR. NE SALEM, OREGON	
CLIENT:	TOBRON OREGON, LLC 68404 FRYREAR ROAD SISTERS, OREGON 97759 503-803-2051
DESIGNER:	 TTE ENGINEERING & SURVEYING, INC. 725 NW HILL STREET, BEND, OR 97703 503-803-2051
SHEET	1.0

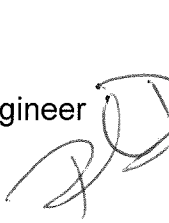




MEMO

TO: Pamela Cole, Planner II
Community Development Department

FROM:  Glenn J. Davis, PE, CFM, Chief Development Engineer
Public Works Department

DATE: July 21, 2017 

**SUBJECT: PUBLIC WORKS RECOMMENDATIONS
SITE PLAN REVIEW UGA-SPR-DAP 17-04 (17-110376-RP)
4350 RIDGE DRIVE NE**

PROPOSAL

Consolidated application including an Urban Growth Area Preliminary Declaration, a Class 3 Site Plan Review, and a Class 2 Driveway Approach Permit for a towing company building, parking area, and vehicle storage area on property approximately 3 acres, zoned IG (General Industrial), and located at 4350 Ridge Drive NE (Marion County Map and Tax Lot Number 073W01C02000). The proposal consists of a towing company building, office and garage, a caretaker's dwelling, a vehicle storage yard, and parking.

RECOMMENDED CONDITIONS OF APPROVAL

1. To meet boundary and linking street requirements, the applicant shall either:
 - a. Construct a 30-foot-wide linking street along Ridge Drive NE and Claxter Road NE from the south line of the subject property to the intersection of Claxter Road NE and Salem Industrial Drive NE, and construct a three-quarter street improvement along the entire frontage of Ridge Drive NE; or
 - b. Pursuant to SRC 200.080(a), pay a temporary access fee of \$1,500 per average daily vehicle trip and complete a Temporary Facilities Access Agreement.
2. Convey land for dedication equal to a half-width right-of-way of 30 feet from the centerline on the development side of Ridge Drive NE.
3. Provide a radius along the north line of the driveways pursuant to PWDS. The southerly driveway approach on Ridge Drive NE shall be closed, as shown on the application materials.

FACTS

Streets

1. Ridge Drive NE

- a. Existing Conditions—This street has an approximate 23-foot improvement within a 30-foot-wide right-of-way abutting the subject property.
- b. Standard—This street is designated as a Local street in the Salem TSP. The standard for this street classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way.

2. 22nd Avenue NE

- a. Existing Conditions—This street has an approximate 23-foot improvement within a 30-foot-wide right-of-way abutting the subject property.
- b. Standard—This street is designated as a Local street in the Salem TSP. The standard for this street classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way.

Storm Drainage

Existing Condition—A 10-inch storm main is located in 22nd Avenue NE.

Water

Existing Conditions

1. The subject property is located in the G-0 water service level.
2. An 18-inch water main is located in 22nd Avenue NE. Mains of this size generally convey flows of 4,800 to 11,100 gallons per minute.

Sanitary Sewer

Existing Condition—An 18-inch sewer line is located in 22nd Avenue NE.

CRITERIA AND FINDINGS

Preliminary Declaration for UGA Findings

SRC 200.055—Standards for Street Improvements

Findings: An adequate linking street is defined as the nearest point on a street that has a minimum 60-foot-wide right-of-way with a minimum 30-foot improvement for Local streets or a minimum 34-foot improvement for Major streets (SRC 200.055(b)). All streets abutting the property boundaries shall be designed to the greater of the standards of SRC Chapter 803 and the standards of linking streets in SRC 200.055(b).

The existing streets adjacent to the subject property do not meet linking street standards. The nearest adequate linking street is located at the intersection of Claxter Road NE and Salem Industrial Drive NE. The applicant shall be required to convey land for dedication of right-of-way to equal 30 feet from centerline along the entire frontage of Ridge Drive NE. Boundary street improvements are required along the entire frontage of Ridge Drive NE, but may be satisfied through a deferral agreement or temporary facilities access agreement. 22nd Avenue NE has a paved width meeting the alley definition and is not subject to boundary street requirements.

Pursuant to SRC 200.080, the proposed development meets the criteria for a temporary facilities access agreement because the existing configuration of Ridge Drive NE provides two travel lanes from the nearest adequate linking street to the subject property and has secondary access via 22nd Avenue NE. The temporary access fee for the subject property is based on approximately \$4.4 million in street improvements needed to improve Ridge Drive NE to Local street standards. The Assistant City Traffic Engineer estimates approximately 2,933 average daily vehicle trips are anticipated from future developments located north of Claxter Road NE between Salem Parkway and Interstate 5 that will benefit from the required Ridge Drive NE improvements. The temporary access fee established in the Ridge Drive NE and 22nd Avenue NE area is \$1,500 per average daily vehicle trip based on the 2,933 total trips and 4.4 million dollars in required improvements. The methodology for vehicle trip calculation will be based on the current methodology for calculating vehicle trips to establish Transportation Systems Development Charges.

SRC 200.060—Standards for Sewer Improvements

Findings: The proposed development shall be linked to adequate facilities by the construction of sewer lines and pumping stations, which are necessary to connect to such existing sewer facilities (SRC 200.060). The nearest available sewer facility is an 18-inch sewer line located in 22nd Avenue NE. No additional sewer mains are needed as a condition of the proposed development.

SRC 200.065—Standards for Storm Drainage Improvements

Findings: The proposed development shall be linked to existing adequate facilities by the construction of storm drain lines, open channels, and detention facilities, which are necessary to connect to such existing drainage facilities. A 10-inch storm main is located in 22nd Avenue NE and is adequate to serve the proposed development.

SRC 200.070—Standards for Water Improvements

Findings: The proposed development shall be linked to adequate facilities by the construction of water distribution lines, reservoirs, and pumping stations that connect to such existing water service facilities (SRC 200.070). Existing water mains abutting the subject property provide sufficient capacity to serve the subject property.

SRC 200.075—Standards for Park Sites

Findings: The subject property is non-residential. No parks facilities are required.

Site Plan Review Findings

Analysis of the development based on relevant criteria in SRC 220.005(f)(3) is as follows:

Criteria: The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately

Finding: The existing street system is adequate to serve the proposed development. However, the existing configuration of Ridge Drive NE does not meet current standards for its classification of street per the Salem TSP. Required street improvements and right-of-way dedication are specified in the conditions of approval consistent with SRC Chapter 803. The applicant shall be required to convey land for dedication of right-of-way to equal 30 feet from centerline along the entire frontage of Ridge Drive NE. The boundary street improvements may be satisfied through a deferral agreement or temporary facilities access agreement.

Criteria: Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians

Finding: The driveway accesses onto Ridge Drive NE and 22nd Avenue NE provide for safe turning movements into and out of the property.

Criteria: The proposed development will be adequately served with City water, sewer, storm drainage, and other utilities appropriate to the nature of the development

Finding: The Public Works Department has reviewed the applicant's preliminary utility plan for this site. The water, sewer, and storm infrastructure are available within surrounding streets / areas and appear to be adequate to serve the proposed development. The applicant shall design and construct all utilities including sewer, water, and storm drainage according to the PWDS and to the satisfaction of the Public Works Director. The applicant is advised that a sewer monitoring manhole may be

required, and the trash area shall be designed in compliance with Public Works Standards.

The proposed plan demonstrates compliance with PWDS Appendix 4E related to green stormwater infrastructure by setting aside at least ten percent of the total new impervious surface area for installation of green stormwater infrastructure. The applicant's engineer shall design and construct a storm drainage system at the time of development in compliance with SRC Chapter 71 and PWDS.

Class 2 Driveway Approach Permit Findings

Criteria—A Class 2 Driveway Approach Permit shall be granted if:

- (1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;**

Finding—The proposed driveways will be modified to meet the standards for SRC 804 and PWDS.

- (2) No site conditions prevent placing the driveway approach in the required location;**

Finding—There are no site conditions prohibiting the location of the proposed driveways.

- (3) The number of driveway approaches onto an Arterial are minimized;**

Finding—The proposed driveways are not accessing an Arterial street.

- (4) The proposed driveway approach, where possible:**

(a) Is shared with an adjacent property; or

(b) Takes access from the lowest classification of street abutting the property;

Finding—The proposed driveways currently access the lowest classification of street abutting the subject property.

- (5) Proposed driveway approach meets vision clearance standards;**

Finding—The proposed driveways meet the PWDS vision clearance standards set forth in SRC Chapter 805.

(6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;

Finding—The proposed driveways will not create a known traffic hazard and will provide for safe turning movements for access to the subject property.

(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Finding—The driveway approach configurations provide sufficient width to accommodate truck turning movements into the subject property. The applicant shall provide turning radii on both sides of the driveway approaches.

(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections;

Finding—The proposed driveway approaches are located on a Local street.

(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets;

Finding—The proposed driveway approaches are not located in the vicinity of a residentially zoned area.

Prepared by: Jennifer Scott, Project Coordinator
cc: File