

NOTICE OF DECISION

PLANNING DIVISION
555 LIBERTY ST. SE, RM 305
SALEM, OREGON 97301
PHONE: 503-588-6173
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*Si necesita ayuda para comprender esta información, por favor llame
503-588-6173.*

DECISION OF THE PLANNING COMMISSION

**CONDITIONAL USE / CLASS 3 DESIGN REVIEW / CLASS 3 SITE PLAN
REVIEW / CLASS 2 DRIVEWAY APPROACH PERMIT / CLASS 2 ADJUSTMENT
CASE NO. CU-DR-SPR-DAP16-06**

**APPLICATION NOS.: 16-108588-ZO, 16-108589-DR, 16-108590-RP &
16-112282-ZO**

NOTICE OF DECISION DATE: AUGUST 17, 2016

APPLICATION SUMMARY: A consolidated application for a Conditional Use Permit, Design Review, Site Plan Review, Driveway Approach Permit, and Class 2 Adjustment to allow for the development of a proposed 84-unit multiple family development on property located at 3271 Lancaster Drive NE.

REQUEST: A consolidated application for a proposed 84-unit multiple family development. The application includes the following:

- 1) A Conditional Use Permit to allow for a multiple family development within the CR (Commercial Retail) zone on an approximate 3.68 acre portion of the subject property;
- 2) A Class 3 Design Review and Class 3 Site Plan Review for the proposed 84-unit multiple family development;
- 3) A Class 2 Driveway Approach Permit to allow driveway access from the proposed development to the proposed extension of Harold Drive NE; and
- 4) A Class 2 Adjustment to increase the number of driveway approaches allowed to Harold Drive from the eastern and western portions of the proposed development from a maximum of one on each side, pursuant to SRC 804.030(a), to two on each side.

The subject property totals approximately 4.31 acres in size, is zoned CR (Commercial Retail), and is located at 3271 Lancaster Drive NE (Marion County Assessor's Map and Tax Lot Number: 072W18BB06000).

APPLICANT: KILLEN DEVELOPMENT LLC / MULTI-TECH ENGINEERING

LOCATION: 3271 LANCASTER DR NE / 97305

CRITERIA: Salem Revised Code Chapters 240 (Conditional Use Permit), 225 (Design Review), 220 (Site Plan Review), 804 (Driveway Approach Permit), and 250 (Adjustments)

FINDINGS: The findings are in the attached staff report dated August 16, 2016.

DECISION: The Planning Commission adopted the facts and findings of the staff report and took the following action for the subject property located at 3721 Lancaster Drive NE (Marion County Assessor's Map and Tax Lot Number: 072W18BB06000):

- A. **APPROVED** the Conditional Use Permit to allow a multiple family development within the CR (Commercial Retail) zone on an approximate 3.68 acre portion of the subject property.
- B. **APPROVED** the Class 3 Design Review for the proposed 84 unit multiple family development, subject to the following conditions of approval:

- Condition 1:** The proposed design shall be revised to provide a children's play area within the eastern portion of the proposed development. The play area shall be located to the north of Building No. 4 in the area identified as open space. The play area shall be located outside the 10-foot-wide public utility easement (PUE) along Harold Drive NE.
- Condition 2:** A minimum 30-inch tall fence with latching gate shall be installed around the perimeter of the proposed outdoor children's play areas within the eastern and western portions of the subject property.
- Condition 3:** A minimum of 2 plant units, as set forth in SRC Chapter 807, Table 807-2, shall be provided adjacent to the primary entry way of each dwelling unit, or combination of dwelling units.
- Condition 4:** Shrubs shall be planted around the perimeter of the proposed buildings at a minimum density of 1 plant unit per 15 linear feet of exterior building wall.
- Condition 5:** Additional trees shall be planted around the perimeters of Buildings 2, 4, and 7 as follows:
- a) Building No. 2: One additional tree shall be planted adjacent to the southeast corner of the building.
 - b) Building No. 4: One additional tree shall be planted adjacent to the southeast corner of the building and one additional tree shall be planted adjacent to the southwest corner of the building.
 - c) Building No. 7: One additional tree shall be planted adjacent to the northeast corner of the building and one additional tree shall be planted adjacent to the southwest corner of the building.
- Condition 6:** The proposed buildings shall be revised to provide additional contrast and distinction between the ground floor and upper floor facades by incorporating one or more of the following:
- a) Vertically oriented lap siding, or horizontally oriented lap siding that is wider than that provided on the upper floor facades, that is painted a different color than the upper floor facades; or
 - b) A siding material different from that used in the other portions of the building facades.
- Condition 7:** The facades of Buildings 1, 2, and 3 shall be revised to incorporate additional windows as follows:
- a) Building No. 1: Windows shall be provided on the western façade of

- a) Building No. 1: Windows shall be provided on the western façade of the building within the bedrooms on each floor.
- b) Building No. 2: Windows shall be provided on the northern façade of the building within the bathrooms on the third floor.
- c) Building No. 3: Windows shall be provided on the eastern façade of the building within the bedrooms on each floor.

C. **APPROVED** the Class 3 Site Plan Review for the proposed 84-unit multiple family development, subject to the following conditions of approval:

Condition 1: The site plan shall be revised to either:

- a) Remove the first six parking stalls to the north of Building No. 4 that are located off the southern driveway approach on the eastern side of Harold Drive; or.
- b) Reconfigure the first six stalls to meet required setbacks. Compact stalls may be utilized to meet this requirement.

Where the parking spaces are removed, they shall be replaced with landscaping.

Condition 2: Prior to building permit approval, the final plat for Partition Case No. PAR15-06 shall be recorded.

Condition 3: Prior to building permit approval, a parking lease agreement shall be executed between the eastern and western portions of the proposed development. The agreement shall meet the requirements of SRC 806.020 and shall, at a minimum, allow for two spaces on the eastern lot to be utilized by the western lot to satisfy minimum off-street parking requirements.

Condition 4: A minimum of one loading space meeting the requirements of SRC Chapter 806 shall be provided within the development.

Condition 5: All trash/recycling areas shall conform to the solid waste service area standards of SRC 800.055.

Condition 6: Dedicate as public right-of-way the existing reserve strip located at the southerly terminus of Harold Drive NE.

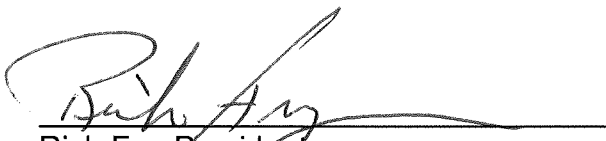
Condition 7: Prior to issuance of certificate of occupancy for any apartment building within the proposed development, construct Harold Drive NE to local street standards with curb line sidewalks within the subject property and connecting to the southerly terminus of the existing Harold Drive improvements.

Condition 8: Design and construct stormwater facilities in accordance with SRC Chapter 71.

- D. **APPROVED** the Class 2 Driveway Approach Permit to allow driveway access from the proposed development to the proposed extension of Harold Drive NE.
- E. **APPROVED** the Class 2 Adjustment to increase the number of driveway approaches allowed to Harold Drive from the eastern and western portions of the proposed development from a maximum of one on each side, pursuant to SRC 804.030(a), to two on each side.

VOTE:

Yes 5 No 2 (Levin, McKinley) Absent 2 (Fox, Guyer)



Rich Fry, President
Salem Planning Commission

The rights granted by the attached decision must be exercised, or an extension granted, by the following dates or this approval shall be null and void:

Conditional Use	<u>September 2, 2018</u>
Class 3 Design Review	<u>September 2, 2018</u>
Class 3 Site Plan Review	<u>September 2, 2020</u>
Class 2 Driveway Approach Permit	<u>September 2, 2018</u>
Class 2 Adjustment	<u>September 2, 2018</u>

Deemed Complete Date:	<u>June 21, 2016</u>
Public Hearing Date:	<u>August 16, 2016</u>
Notice Decision Mailing Date:	<u>August 17, 2016</u>
Decision Effective Date:	<u>September 2, 2016</u>
State Mandate Date:	<u>October 19, 2016</u>

Case Manager: Bryce Bishop, bbishop@cityofsalem.net

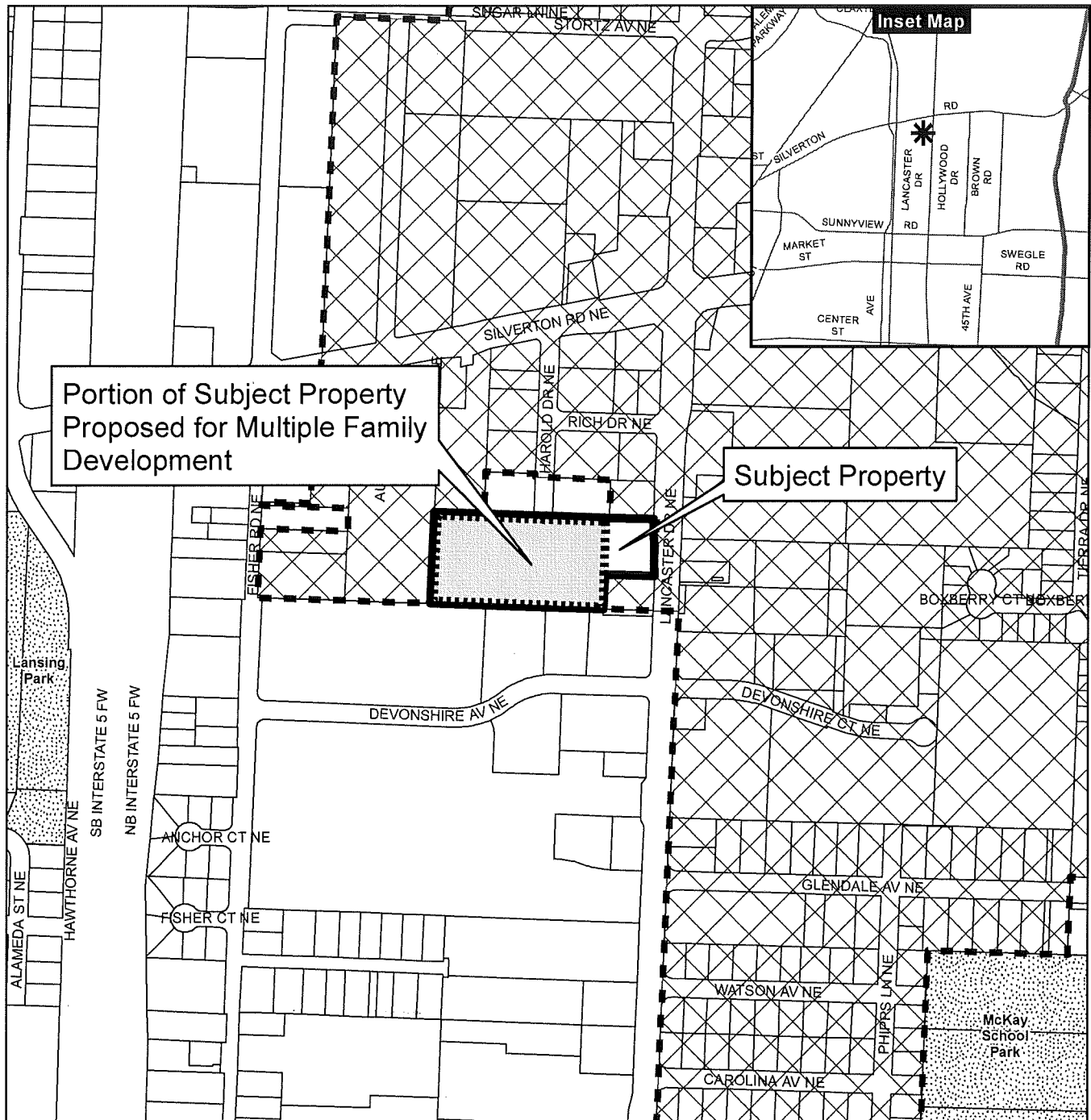
This decision is final unless written appeal from an aggrieved party is filed with the City of Salem Planning Division, Room 305, 555 Liberty Street SE, Salem OR 97301, **no later than 5:00 p.m., SEPTEMBER 1, 2016**. Any person who presented evidence or testimony at the hearing may appeal the decision. The notice of appeal must contain the information required by SRC 300.1020 and must state where the decision failed to conform to the provisions of the applicable

code section, SRC Chapters 240 (Conditional Use Permits), 225 (Design Review), 220 (Site Plan Review), 804 (Driveway Approach Permit) and 250 (Adjustments). The appeal must be filed in duplicate with the City of Salem Planning Division. The appeal fee must be paid at the time of filing. If the appeal is untimely and/or lacks the proper fee, the appeal will be rejected. The City Council will review the appeal at a public hearing. After the hearing, the City Council may amend, rescind, or affirm the action, or refer the matter to staff for additional information.

The complete case file, including findings, conclusions and conditions of approval, if any, is available for review at the Planning Division office, Room 305, City Hall, 555 Liberty Street SE, during regular business hours.

<http://www.cityofsalem.net/planning>

Vicinity Map 3271 Lancaster Drive NE



Legend

Taxlots

Urban Growth Boundary

City Limits

Outside Salem City Limits

Historic District

Schools

Parks

CITY OF Salem
AT YOUR SERVICE
Community Development Dept.



0 100 200 400 Feet

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TO: Planning Commission

FROM: *B.B. For* Lisa Anderson-Ogilvie, AICP, Planning Administrator

STAFF: Bryce Bishop, Planner II *BB.*

HEARING DATE: August 16, 2016

APPLICATION Consolidated Conditional Use, Class 3 Design Review, Class 3 Site Plan Review, Class 2 Driveway Approach Permit, & Class 2 Adjustment Case No. CU-DR-SPR-DAP-ADJ16-06

LOCATION: 3271 Lancaster Drive NE

SIZE: Approximately 4.31 acres – Attachment A

REQUEST: A consolidated application for a proposed 84-unit multiple family development. The application includes the following:

- 1) A Conditional Use Permit to allow for a multiple family development within the CR (Commercial Retail) zone on an approximate 3.68 acre portion of the subject property;
- 2) A Class 3 Design Review and Class 3 Site Plan Review for the proposed 84-unit multiple family development;
- 3) A Class 2 Driveway Approach Permit to allow driveway access from the proposed development to the proposed extension of Harold Drive NE; and
- 4) A Class 2 Adjustment to increase the number of driveway approaches allowed to Harold Drive from the eastern and western portions of the proposed development from a maximum of one on each side, pursuant to SRC 804.030(a), to two on each side.

The subject property totals approximately 4.31 acres in size, is zoned CR (Commercial Retail), and is located at 3271 Lancaster Drive NE (Marion County Assessor's Map and Tax Lot Number: 072W18BB06000).

APPLICANT: Killen Development, LLC

OWNER: Howard Commons, LLC

APPROVAL CRITERIA: - Conditional Use: SRC 240.005(d)
- Class 3 Design Review: SRC 225.005(e)(2)
- Class 3 Site Plan Review: SRC 220.005(f)(3)
- Class 2 Driveway Approach Permit: SRC 804.025(d)
- Class 2 Adjustment: SRC 250.005(d)(2)

RECOMMENDATION: APPROVE Conditional Use, Class 3 Design Review, Class 3 Site Plan Review, Class 2 Driveway Approach Permit, and Class 2 Adjustment Case No. CU-DR-SPR-DAP-ADJ16-06 subject to the following conditions:

CLASS 3 DESIGN REVIEW

- Condition 1:** The proposed design shall be revised to provide a children's play area within the eastern portion of the proposed development. The play area shall be located to the north of Building No. 4 in the area identified as open space. The play area shall be located outside the 10-foot-wide public utility easement (PUE) along Harold Drive NE.
- Condition 2:** A minimum 30-inch tall fence with latching gate shall be installed around the perimeter of the proposed outdoor children's play areas within the eastern and western portions of the subject property.
- Condition 3:** A minimum of 2 plant units, as set forth in SRC Chapter 807, Table 807-2, shall be provided adjacent to the primary entry way of each dwelling unit, or combination of dwelling units.
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- Condition 7:** The facades of Buildings 1, 2, and 3 shall be revised to incorporate additional windows as follows:
- a) Building No. 1: Windows shall be provided on the western façade of the building within the bedrooms on each floor.
 - b) Building No. 2: Windows shall be provided on the northern façade of the building within the bathrooms on the third floor.
 - c) Building No. 3: Windows shall be provided on the eastern façade of the building within the bedrooms on each floor.

CLASS 3 SITE PLAN REVIEW

- Condition 1:** The site plan shall be revised to remove the first six parking stalls to the north of Building No. 4 that are located off the southern driveway approach on the eastern side of Harold Drive. Where the parking spaces are removed, they shall be replaced with landscaping.
- Condition 2:** Prior to building permit approval, the final plat for Partition Case No. PAR15-06 shall be recorded.
- Condition 3:** Prior to building permit approval, a parking lease agreement shall be executed between the eastern and western portions of the proposed development. The agreement shall meet the requirements of SRC 806.020 and shall, at a minimum, allow for two spaces on the eastern lot to be utilized by the western lot to satisfy minimum off-street parking requirements.
- Condition 4:** A minimum of one loading space meeting the requirements of SRC Chapter 806 shall be provided within the development.
- Condition 5:** All trash/recycling areas shall conform to the solid waste service area standards of SRC 800.055.
- Condition 6:** Dedicate as public right-of-way the existing reserve strip located at the southerly terminus of Harold Drive NE.
- Condition 7:** Prior to issuance of certificate of occupancy for any apartment building within the proposed development, construct Harold Drive NE to local street standards with curb line sidewalks within the subject property and connecting to the southerly terminus of the existing Harold Drive improvements.
- Condition 8:** Design and construct stormwater facilities in accordance with SRC Chapter 71.

PROCEDURAL FINDINGS

On April 29, 2016, Mark Grenz, of Multi/Tech Engineering, on behalf of the applicant, Killen Development LLC, filed an application for a Conditional Use Permit, Class 3 Design Review, Class 3 Site Plan Review, Class 2 Driveway Approach Permit, and Class 2 Adjustment to allow for the development of a proposed 84-unit multiple family development on a on an approximate 3.68 acre portion of the property located at 3271 Lancaster Drive NE.

Because multiple land use applications are required in connection with the proposed development, the applicant, pursuant to SRC 300.120(c), chose to consolidate the applications and process them together as one. When multiple applications are consolidated, the review process for the application shall follow the highest numbered procedure type required for the land use applications involved, and the Review Authority for the application shall be the highest applicable Review Authority under the highest numbered procedure type for the land use applications involved.

Based upon these requirements, the proposed consolidated application is required to be processed as a Type III procedure and reviewed by the Planning Commission.

After additional requested information was provided by the applicant, the application was deemed complete for processing on June 21, 2016.

Notice to surrounding property owners was mailed pursuant to SRC requirements on July 27, 2016. Notice was also posted on the subject property by the applicant's representative pursuant to

SRC requirements on August 3, 2016.

The public hearing on the proposed Conditional Use, Class 3 Design Review, Class 3 Site Plan Review, Class 2 Driveway Approach Permit, and Class 2 Adjustment application is scheduled for August 16, 2016. The state-mandated 120-day local decision deadline for the application is October 19, 2016.

BACKGROUND/PROPOSAL

The proposed multiple family development will be located on the western 3.68 acres of an overall 4.31 acre property located at 3271 Lancaster Drive NE (**Attachment A**).

The applicant proposes development of the subject property as an 84-unit multiple family development with 48 dwelling units and parking located on the eastern side of the proposed extension of Harold Drive and 36 dwelling units and parking located on the western side of the proposed extension of Harold Drive (**Attachment B**).

The proposed development requires the following land use approvals:

- 1) A Conditional Use Permit to allow for a multiple family development within the CR (Commercial Retail) zone;
- 2) A Class 3 Design Review and Class 3 Site Plan Review for the proposed 84-unit multiple family development;
- 3) A Class 2 Driveway Approach Permit to allow driveway access from the proposed development to the proposed extension of Harold Drive NE; and
- 4) A Class 2 Adjustment to increase the number of driveway approaches allowed to Harold Drive from the eastern and western portions of the proposed development from a maximum of one on each side, pursuant to SRC 804.030(a), to two on each side.

Prior to filing the proposed application, a Partition and Urban Growth Preliminary Declaration were previously approved for the subject property.

The partition, Case No. PAR15-06, divides the overall 4.31 acre property into 3 parcels (**Attachment C**). The two westernmost parcels of the partition, Parcels 1 and 2, are subject to the proposed 84-unit multiple family development. The easternmost parcel, Parcel 3, will be developed separately from the proposed multiple family development. Partition Case No. PAR15-06 received tentative approval on August 18, 2015, subject to ten conditions of approval, including a requirement to construct an extension of Harold Drive NE through the western portion of the subject property (**Attachment D**). The final plat for the partition has not yet been approved and recorded.

The Urban Growth Preliminary Declaration, Case No. UGA15-01, identifies the public facilities required to serve the subject property. The Urban Growth Preliminary Declaration was approved on October 19, 2015, subject to one condition of approval requiring acquisition of a reserve strip at the southerly terminus of Harold Drive and the construction of a 30-foot-wide linking street improvement within a 60-foot-wide right-of-way from the southerly terminus of Harold Drive NE to the north line of the subject property (**Attachment E**).

The overall 4.31 acre property has frontage on Lancaster Drive NE and the terminus of Harold Drive NE. Harold Drive is designated as a local street under the City's Transportation System Plan (TSP) and Lancaster Drive is designated as a major arterial.

Vehicular access to the proposed multiple family development will be taken from the extension of Harold Drive via two driveway approaches on the east side of Harold Drive and two driveway approaches on the west side of Harold Drive. No vehicular access to Lancaster Drive is proposed for the multiple family development. Vehicular access to Lancaster Drive will, however, be provided to Parcel 3 of Partition Case No. PAR15-06 at the time of future development of that parcel.

SUBSTANTIVE FINDINGS

1. Salem Area Comprehensive Plan (SACP)

Comprehensive Plan Map: The Salem Area Comprehensive Plan Map designates the subject property as "Commercial".

Relationship to Urban Service Area: The subject property lies within the boundaries of the City's Urban Service Area. The Urban Service Area is that territory within the City where all required public facilities (streets, water, sewer, stormwater, and parks) necessary to serve development are already in place or fully committed to be extended.

Pursuant to SRC 200.020(a), an Urban Growth Preliminary Declaration is required when a development is located outside the Urban Service Area boundary, or when a development is located inside the Urban Service Area boundary but development precedes City construction of required facilities. On October 19, 2015, Urban Growth Preliminary Declaration Case No. UGA15-01 was approved for the subject property identifying a requirement for the applicant to construct a 30-foot-wide linking street improvement within a 60-foot-wide right-of-way from the southerly terminus of Harold Drive NE to the north line of the subject property.

2. Zoning

The subject property is zoned CR (Commercial Retail). Zoning of surrounding properties includes:

- North: City of Salem CR (Commercial Retail);
Marion County CG (Commercial General) and CR (Commercial Retail)
- South: City of Salem CR (Commercial Retail);
Marion County CR (Commercial Retail)
- East: Across Lancaster Drive NE, Marion County CR (Commercial Retail)
- West: Marion County RM (Multiple Family Residential)

3. Natural Features

Trees: The City's tree preservation ordinance (SRC Chapter 808) protects Heritage Trees, Significant Trees (including Oregon White Oaks with diameter-at-breast-height of 24 inches or greater), trees and native vegetation in riparian corridors, and trees on lots and parcels greater than 20,000 square feet. The tree preservation ordinance defines "tree" as, "any living woody plant that grows to 15 feet or more in height, typically with one main stem called a trunk, which is 10 inches or more dbh, and possesses an upright arrangement of branches and leaves."

Under the City's tree preservation ordinance, pursuant to SRC 808.035(a), tree conservation plans are required in conjunction with development proposals involving the creation of lots or parcels to be used for the construction of single family or duplex dwelling units, if the development proposal will result in the removal of trees.

There are five trees present on the subject property (two cottonwoods, a walnut, a maple, and

a birch); none of the trees are designated as significant or heritage and none are located within a riparian corridor.

The applicant's development plan identifies all five of the existing trees for removal. Because the proposal does not involve the creation of lots or parcels to be used for the construction of single family or duplex dwelling units, a tree conservation plan is not required in conjunction with the proposed development. The removal of trees from the property cannot occur without first obtaining the necessary approvals established under SRC Chapter 808.

Wetlands: Grading and construction activities within wetlands are regulated by the Oregon Department of State Lands (DSL) and US Army Corps of Engineers. State and Federal wetlands laws are also administered by the DSL and Army Corps, and potential impacts to jurisdictional wetlands are addressed through application and enforcement of appropriate mitigation measures.

SRC Chapter 809 requires notice to DSL for applications for development or land use in areas designated as wetlands on the official wetlands map. The Salem-Keizer Local Wetland Inventory (LWI) does not identify any mapped wetlands or waterways on the subject property. There is, however, a band of hydric/wetland type soils running north-south through the eastern portion of the proposed multiple family development.

Because of the presence of hydric soils on the subject property, notice of the prior partition was sent to DSL. Comments received from DSL indicated that a wetland delineation by a professional wetland consultant is recommended prior to development. The DSL indicates that the wetland delineation report should be submitted to DSL for review and concurrence.

Because no mapped wetlands are currently identified on the subject property, SRC Chapter 809 does not specify any additional requirements as a condition of development. The applicant is advised to coordinate any required wetland delineations with DSL.

Landslide Susceptibility: The City's landslide hazard ordinance (SRC Chapter 810) establishes standards and requirements for the development of land within areas of identified landslide hazard susceptibility. According to the City's adopted landslide hazard susceptibility maps, the subject property is mapped with a small area of 2 landslide hazard susceptibility points. There are 2 activity points associated with the multiple family building permits. Pursuant to the requirements of SRC 810.025, the cumulative total of 4 points between those associated with the land and those associated with the proposed development activity indicates a low landslide risk and therefore a geologic assessment or geotechnical report is not required in conjunction with the proposed development.

4. Neighborhood Association Comments

The subject property is located within the North Lancaster Neighborhood Association. As of the date of completing this staff report, no comments have been received from the neighborhood association regarding the proposal.

5. Public Comments

Notice of the proposal was mailed to property owners within 250 feet of the subject property on July 27, 2016. As of the date of completing this staff report, one comment from an abutting property owner was received inquiring whether the proposed development will encroach on, change, or effect the use of their abutting property at 3695 Devonshire Avenue NE.

Staff Response: The proposed multiple family development will be located on the western

portion of the property at 3271 Lancaster Drive NE. The proposed development maintains required setbacks from the property lines of abutting properties and will not encroach onto the property at 3695 Devonshire Avenue NE.

As required under the tentative approval for Partition Case No. PAR15-06, and as shown on the applicant's proposed site plan, the development will, however, extend Harold Drive NE through the subject property to its southern boundary in order to accommodate its future connection to Devonshire Avenue NE. The extension of Harold Drive to Devonshire Avenue is required under the City's Transportation System Plan (TSP). As indicated in the TSP, the extension of Harold Drive will likely occur along with commercial development. In order to comply with the TSP, the proposed development is required to extend Harold Drive through the subject property. The final connection of Harold Drive to Devonshire Avenue will occur either through future development or a City-initiated project.

6. City Department Comments

- A. The Salem Fire Department reviewed the proposal and indicated that they have no comments regarding the proposal, but will have requirements when plans are submitted for building permits regarding fire department access and water supply, fire lanes, fire sprinklers and alarms, etc.
- B. The Public Works Department reviewed the proposal and provided comments regarding street and City utility improvements required to serve the development and recommended conditions of approval to ensure conformance with the applicable requirements of the SRC. Comments from the Public Works Department are included as **Attachment F**.

7. Public Agency and Public & Private Service Provider Comments

Notice of the proposal was provided to public agencies and to public & private service providers. As of the date of completing this staff report, the following comments were received:

- A. Portland General Electric (PGE) reviewed the proposal and indicated that development costs will be determined by current tariff and service requirements and that a 10-foot PUE is required on all front street lots.
- B. The Salem-Keizer School District reviewed the proposal and provided comments that are included as **Attachment G**. In summary, the School District indicates the subject property is served by Chavez Elementary School, Stephens Middle School, and McKay High School.

Future students residing at the location of the proposed development will be eligible for school transportation services to the elementary school and middle school, and will be within the walk zone of the high school.

The School District explains that enrollment at Chavez Elementary School is estimated to be at 104 percent of capacity when the estimated cumulative impacts of 2015-2016 land use actions are considered. Enrollment at Stephens Middle School is estimated to be at 98 percent of capacity when the estimated cumulative impacts of 2015-2016 land use actions are considered. Enrollment at McKay High School is estimated to be at 99 percent of capacity when the estimated impacts of 2015-2016 land use actions are considered.

The School District also indicates that the developer should provide paved walking routes to allow pedestrian and bicycle access to schools from all residences within the new development and should provide all improvements required by the City where new

transportation routes are established or existing transportation routes change, such as school flashers, crosswalks, and signage.

Staff Response: As indicated in the comments from the Public Works Department, and recommended in the conditions of approval, the proposed development will be required to provide sidewalks along both sides of the extension of Harold Drive. In addition, the proposal includes sidewalks throughout the development to ensure convenient pedestrian access from the individual apartment units to the proposed sidewalks on Harold Drive. The sidewalks provided along the Harold Drive frontages of the subject property and throughout the interior of the proposed development ensure walking routes will be provided to allow pedestrian and bicycle access from the residences within the development to surrounding schools.

8. Applicant Submittal Information

Land use applications must include a written statement addressing the applicable approval criteria and be supported by proof they conform to all applicable standards and criteria of the Salem Revised Code. The written statement provided by the applicant addressing the applicable application approval criteria is include as **Attachment H** to this staff report. Staff utilized the information from the applicant's written statement to help evaluate the proposal and formulate the facts and findings within the staff report.

FINDINGS ADDRESSING APPLICABLE SALEM REVISED CODE APPROVAL CRITERIA FOR CONDITIONAL USE PERMIT

9. CONDITIONAL USE APPROVAL CRITERIA

Salem Revised Code (SRC) 240.005(d) establishes the following criteria that must be met before approval can be granted to a conditional use permit. The following subsections are organized with approval criteria shown in ***bold italic***, followed by findings evaluating the proposed development for conformance with the criteria. Lack of compliance with the following criteria is grounds for denial of the conditional use permit, or for the issuance of certain conditions to ensure the criteria are met.

(1) The proposed use is allowed as a conditional use in the zone;

Finding: The written statement provided by the applicant (**Attachment H**) indicates that under SRC 522.005(a), Table 522-1, multiple family dwellings are allowed within the CR zone with a Conditional Use permit.

Staff concurs with the findings included in the applicant's written statement. Pursuant to SRC 522.005(a), Table 522-1, Multiple Family is allowed as a conditional use in the CR zone. This criterion is met.

(2) The reasonably likely adverse impacts of the use on the immediate neighborhood can be minimized through the imposition of conditions; and

Finding: The written statement provided by the applicant (**Attachment H**) indicates that the proposed apartments will have little to no impact on the neighborhood. The applicant explains that the subject property is surrounded by commercial uses to the north, east, and south, along with apartments to the west. It is indicated that the proposed apartments will have less of an impact on the area then the commercial uses allowed within the CR zone. The applicant's Traffic Engineer has provided an analysis of the site (**Attachment I**). The

analysis indicates that the proposed apartments will have less of an impact on the neighborhood than other allowed uses within the CR zone. The analysis dated April 28, 2016, states, "The proposed 84 apartment units will generate less traffic each day than many allowed uses in the CR zoned." Therefore, the development of apartments on the site will not impact the neighborhood.

Staff concurs with the findings included in the applicant's written statement. The development standards of the CR zone included under SRC Chapter 522 and the design review requirements for multiple family developments included under SRC Chapter 702 are intended to minimize the potential impacts of development on surrounding properties. The proposed development has been reviewed for conformance with these requirements and, as recommended to be conditioned under Sections 10 and 11 of this report, conforms to such requirements; thereby ensuring that any adverse impacts of the proposed development on the immediate neighborhood will be minimized.

As indicated in the applicant's written statement, the proposed development is located in an area characterized largely by commercial zoning and a variety of non-residential uses. There is also an apartment complex located immediately to the west of the subject property. Due to the property's CR (Commercial Retail) zoning, the site could be developed with commercial uses that are more intensive and generate more traffic than the proposed 84-unit multiple family development. As indicated in the traffic analysis provided by the applicant (**Attachment I**), it is anticipated the proposed development will generate approximately 560 average daily trips, which represent fewer average daily trips and less of an impact to the immediate neighborhood compared to if the property were instead developed with other uses allowed in the CR zone, such as banks, restaurants, and specialty retail, which are outright permitted. This criterion is met.

(3) The proposed use will be reasonably compatible with and have minimal impact on the livability or appropriate development of surrounding property.

Finding: The written statement provided by the applicant (**Attachment H**) explains the subject property is surrounded by the following uses:

- North: CR zoned property: existing commercial uses
- East: CR zoned property: vacant commercial zoned property
- South: CR zoned property: existing commercial uses
- West: Marion County RM zoned property: existing multi-family dwellings

The applicant indicates the apartments will be compatible with the residential uses to the west, and the surrounding commercial uses. The existing commercial uses are in a convenient location for the residents of the site. Development of residential uses are most compatibility in areas that provide services and amenities like commercial uses for the residents.

The applicant explains that the apartments will be required to go through Site Plan/Design Review, which requires open space and landscaping at a higher percent than what a commercial use would be required to provide. Amenities like landscaped open space will help with the visual appeal of this area and reduce impacts on the neighborhood. The design standards are in place to help ensure compatibly with adjacent uses.

Staff concurs with the findings included within the applicant's written statement. The subject property is located in an area characterized predominantly by commercial zoning and a variety of non-residential uses including, but not limited to, retail stores, supermarkets, restaurants, offices, storage, and shopping centers. There is also an

existing multiple family development immediately to the west of the subject property.

The residential development policies of the Salem Comprehensive Policies Plan pertaining to multi-family housing (Section IV.E.6) provide that multi-family housing shall be located in areas proximate to existing or planned transportation corridors and public facilities and services, and that multiple family developments should be located in areas that provide walking, auto, or transit connections to employment centers, shopping areas, transit service, parks, and public buildings.

The subject property is zoned CR (Commercial Retail). Multiple Family is an allowed use within the CR zone subject to receiving conditional use permit approval. Due to the location of the subject property and the zoning of surrounding properties, the proposed development is located in an area within close proximity to shopping, employment, and other services anticipated by the applicable policies of the City's Comprehensive Policies Plan for the siting of multiple family development.

In addition, due to the proximity of the proposed development to Lancaster Drive, a major arterial street under the City's Transportation System Plan, the proposed development will be located in proximity to a major transportation corridor where transit service is available via Cherriots Route 11 – Lancaster/Keizer every 15 minutes.

Development of the property is subject to the development standards of the CR zone included under SRC Chapter 522 and the design review requirements for multiple family developments included under SRC Chapter 702. The requirements established under these chapters are intended to minimize the potential impacts of development on surrounding properties. As indicated in Sections 10 and 11 of this report, the proposed development has been reviewed for conformance with these requirements and, as recommended to be conditioned, conforms to such requirements; thereby ensuring the proposed development will have minimal impact on the livability and development of surrounding property.

Because the proposed multiple family development will be located in an area where compatible commercial services benefiting the future residents of the development will be available, and because the development will have minimal impact on surrounding uses, the proposed development meets this approval criterion.

FINDINGS ADDRESSING APPLICABLE SALEM REVISED CODE APPROVAL CRITERIA FOR CLASS 3 DESIGN REVIEW

10. CLASS 3 DESIGN REVIEW APPROVAL CRITERIA

Salem Revised Code (SRC) 225.005(e)(2) sets forth the criteria that must be met before approval can be granted to an application for Class 3 Design Review. Pursuant to SRC 225.005(e)(2) an application for a Class 3 Design Review shall be approved if all of the applicable design review guidelines are met.

The applicable design review guidelines for multiple family developments are included under SRC Chapter 702 (Multiple Family Design Review Guidelines and Standards), specifically SRC 702.015 through SRC 702.040. The following subsections are organized with the multiple family design review guideline shown in ***bold italic***, followed by findings evaluating the proposal for conformance with the design review guideline. Lack of compliance with the following design review guidelines is grounds for denial of the Class 3 Design Review application, or for the issuance of certain conditions to ensure the multiple family design review guidelines are met.

Open Space (SRC 702.015)

▪ **Common Open Space (SRC 702.015(b)(1)):**

- (A) A variety of open space areas of sufficient size shall be provided for use by all residents.**

Finding: SRC 111.015(k) defines common open space as:

"Open area intended for shared use and enjoyment in a development. Common open space includes landscaping, walkways, play areas, swimming pools, roof gardens, or other open areas which provide active or passive recreational or visual amenities for residents. Common open space does not include parking areas, streets, or other areas designed for motor vehicle circulation or storage."

The applicant's written statement (**Attachment H**) and site plan indicates that the minimum open space area (landscaped area) for the proposed development is 30.9% common open space on the site. The subject property is 160,203 (3.68 acres/net area) square feet in size with 43,318 square feet of landscaped open space. Therefore, totaling 30.9% open space.

As indicated in the written statement provided by the applicant, approximately 30.9 percent of the development site will be open space. Open space is provided around and between buildings and along the perimeter of the development within required setbacks. The proposed development conforms to this design guideline.

- (B) Common open space shall be distributed around buildings and throughout the site.**

Finding: As shown on the proposed site plan, the proposed development includes an approximate 1,541 square-foot fenced play area within the western portion of the development located to the east of Building No. 6. As indicated in the applicant's written statement (**Attachment H**), landscaped open space areas are located throughout the site. The proposed development conforms to this design guideline.

- (C) The amount of perimeter setbacks used for common open space shall be minimized.**

Finding: As indicated in the applicant's written statement (**Attachment H**) and shown on the site plan, out of the total of 61,570 square feet of common open space included within the development, approximately 27,883 square feet (45.3 percent) of the common open space is located within the perimeter setback areas of the development and approximately 33,686 square feet of open space is located outside of required perimeter setbacks. The majority of the common open area included within the development is located between buildings and throughout the site. The proposed development conforms to this design guideline.

▪ **Children's Play Areas and Adult Recreation Areas (SRC 702.015(c)(1)):**

- (A) A variety of common open area opportunities shall be provided for enjoyment by all residents.**

Finding: The applicant's written statement (**Attachment H**) indicates that the proposed development provides 1,828 square feet of play area in the east portion of the development and 1,721 square feet of play area in the western portion of the

proposed development. The 1,828 square-foot play area within the western portion of the development mentioned in the applicant's written statement refers to a proposed play area that was included in the original site plan for the development, but which was subsequently removed and instead changed to common open space in response to comments made by staff during the initial review of the application.

The design review guideline under SRC 702.015(c)(1)(C) requires children play areas to be located in a manner to incorporate safety into the design by including such things as locating play areas to be visible from dwelling units, locating play areas away from physical barriers such as driveways and parking areas, and selection of play equipment with safe designs.

During staff's initial review of the application it was indicated that the fenced play area on the eastern portion of the proposed development located to the north of Building No. 4, which is now identified on the current plan as open space, was not preferable location for a play area due to its proximity to Harold drive and the parking lot and driveway into the site. However, without a designated play area within the eastern portion of the proposed development, there will be no area for children to play without crossing Harold Drive to reach the play area on the western portion of the site next to Building No. 6. Without a play area within the eastern portion of the site, the proposed development will not include a variety of common open area opportunities as required by this design guideline.

Due to a lack of alternative locations better suited within the eastern portion of the proposed development to serve as a play area, and to help ensure that a variety of common open area opportunities will be provided within the development on both sides of Harold Drive for enjoyment by all residents, including children, the proposed condition of approval is recommended:

Condition 1: The proposed design shall be revised to provide a children's play area within the eastern portion of the proposed development. The play area shall be located to the north of Building No. 4 in the area identified as open space. The play area shall be located outside the 10-foot-wide public utility easement (PUE) along Harold Drive NE.

The proposed development, as recommended to be conditioned, conforms to this design guideline.

(B) *Children's play and/or adult recreation areas shall be located centrally within the development.*

Finding: The proposed development currently includes one children's play area within the western portion of the development located to the east of Building No. 6. No children's play area is proposed within the eastern portion of the development.

The one proposed children's play area to the east of Building No. 6 does not meet the requirements of this design guideline which requires play areas to be located centrally within the development. However, as recommended under Condition No. 1, the proposed development would be also required to provide an additional children's play area within the eastern portion of the proposed development in order to conform to the design guideline requiring a variety of common open area opportunities throughout the development. In addition to meeting that design review guideline, the recommended condition of approval will also ensure that two play

areas are provided within the development, one to serve residents on each side of Harold Drive. Due to a lack of better suited alternative locations, the two play areas will be generally centrally located to the overall development and accessible to residents via the proposed internal network of pedestrian pathways. The proposed development, as recommended to be conditioned under Condition No. 1, conforms to this design guideline.

- (C) ***Children's play areas, if provided, shall be located in a manner to incorporate safety into the design by including such things as locating play areas to be visible from dwelling units, locating play areas away from physical barriers such as driveways and parking areas, and selection of play equipment with safe designs.***

Finding: The proposed development currently includes one children's play area within the western portion of the development located to the east of Building No. 6. No children's play area is proposed within the eastern portion of the development.

However, as recommended under Condition No. 1, the proposed development will also include an additional children's play area within the eastern portion of the proposed development located to the north of Building No. 4.

Due to a lack of better alternative locations, both of these play areas are located in proximity to physical barriers including parking, driveways, and a street. The corresponding design standard to this design guideline requires that a minimum 30-inch tall fence shall be installed to separate outdoor children's play areas from any parking lot, drive aisle, or street. Due to the proximity of parking, driveways, and a street to the proposed play areas, the following condition of approval is recommended to ensure that safety is incorporated into their design as required by this design guideline:

Condition 2: A minimum 30-inch tall fence with latching gate shall be installed around the perimeter of the proposed outdoor children's play areas within the eastern and western portions of the subject property.

The development, as proposed to be conditioned, conforms to this design guideline.

▪ **Private Open Space (SRC 702.015(d)(1)):**

- (A) ***Individual private open space shall be provided for each dwelling unit in all newly constructed multiple family developments.***

Finding: As indicated in the applicant's written statement (**Attachment H**), each dwelling unit will have private open space as required by code. Ground floor units will have patio areas that are 96 square feet in size, with no dimension less than 6 feet. All second and third story units will have balconies/decks that are a minimum 48 square feet in size.

Staff concurs with the findings included within the applicant's written statement. Each dwelling unit within the development includes private open space in the form of either a patio for ground floor units or a balcony for upper-story units. The private open space provided within the proposed development conforms to this design guideline.

- (B) ***Private open space shall be easily accessible from the dwelling unit.***

Finding: As indicated in the applicant's statement (**Attachment H**), proposed private open space areas are located contiguous to the dwelling units. Because private open space areas in the form of patios and balconies are provided for each dwelling unit within the development, and because the patios and balconies are directly accessible from the interiors of the dwelling units via a doorway, the proposed private open space areas conform to this design guideline.

- (C) *If private open space is located adjacent to common open space, a buffer between the two open space areas shall be provided.***

Finding: As indicated in the applicant's written statement (**Attachment H**) and shown on the elevations for the proposed apartment buildings, private open space areas for ground floor dwelling units are proposed to be separated from adjacent common open space through use of 4-foot to 6-foot tall wood fencing.

Because fencing is utilized to screen proposed private open space areas from common open space areas, a visual buffer between the two types of open space areas is provided. The proposed development conforms to this design guideline.

Landscaping (SRC 702.020)

- **General Landscaping (SRC 702.020(b)(1)):**

- (A) *A variety of tree types shall be distributed throughout the site to maximize tree canopy.***

Finding: As indicated on the landscape plans for the proposed development, a variety of deciduous canopy and columnar trees will be distributed throughout the site to maximize tree canopy as required by this design guideline. Trees proposed to be planted include red maple, blaze maple, flowering cherry, and flowering pear.

Trees included within the development are proposed to be planted throughout the site around the perimeter of the proposed buildings and within the interior and around the perimeter of the proposed parking areas. The proposed development conforms to this design guideline.

- (B) *Landscaping shall be used to shield the site from winter winds and summer sun.***

Finding: The overall number of trees proposed to be provided, their distribution throughout the site, their placement around the perimeter of buildings, and their placement within and around the perimeter of parking areas will help to shield the property from winter winds and summer sun. The proposed development conforms to this design guideline.

- (C) *Existing trees shall be preserved to the maximum extent possible.***

Finding: There are five trees present on the subject property (two cottonwoods, a walnut, a maple, and a birch). None of the existing trees are designated as significant trees or heritage trees and none of the trees are located within a riparian corridor.

As indicated in the applicant's written statement (**Attachment H**), none of the existing trees are proposed to be preserved due to their proximity to required

grading and site improvements. The proposed development conforms to this design guideline.

- (D) *Where a development site abuts property zoned Residential Agricultural (RA) or Single Family Residential (RS), an appropriate combination of landscaping and screening shall be provided that is sufficient to buffer between the multiple family development and the abutting RA or RS zoned property.***

Finding: The subject property does not abut RA (Residential Agriculture) or RS (Single Family Residential) zoned land. Because the subject property does not abut RA or RS zoned property, this design guideline is not applicable to the proposed development.

▪ **Street Frontage (SRC 702.020(c)(1)):**

- (A) *The residential character of the site shall be enhanced with trees planted within the public right-of-way.***

Finding: As shown on the landscape plans for the proposed development, trees are proposed to be planted along the perimeter of the subject property within and along the right-of-way of Harold Drive. The trees proposed to be planted within and along the Harold Drive right-of-way will enhance the residential character of the site and ensure conformance with this requirement. The proposed development conforms to this design guideline.

▪ **Building Exteriors (SRC 702.020(d)(1)):**

- (A) *Landscaping shall be planted to define and accentuate the primary entry way of each dwelling unit, or combination of dwelling units.***

Finding: The corresponding design standards to this design guideline require plant units to be provided adjacent to the primary entryways of buildings and around the perimeter of buildings. In order to evaluate conformance with these design review requirements, specific numbers of plant units need to be identified on landscape plans to know how many and what types of plant units will be provided.

The landscape plan provided by the applicant shows specific numbers of trees and their species, as well as the proposed species of shrubs and ground covers to be included within the development, but does not, however, identify the specific numbers of plant units to be distributed around the proposed buildings. Because specific numbers and locations of plant units are not shown on the proposed landscape plan, the following condition of approval, based on the corresponding design standards, is recommended in order to ensure that the landscaping provided for the development will conform to this design guideline:

Condition 3: A minimum of 2 plant units, as set forth in SRC Chapter 807, Table 807-2, shall be provided adjacent to the primary entry way of each dwelling unit, or combination of dwelling units.

The proposed development, as recommended to be conditioned, conforms to this design guideline.

- (B) *Vertical and horizontal landscape elements shall be provided along all exterior walls to soften the visual impact of buildings and create residential character.***

Finding: As shown on the landscape plans for the proposed development, trees and planting beds are proposed around the perimeters of the proposed buildings.

The corresponding design standards to this design guideline require:

- ❖ *New trees to be planted, or existing trees to be preserved, at a minimum density of 10 plant units per 60 linear feet of exterior building wall. Such trees shall be located not more than 25 feet from the edge of the building footprint; and*
- ❖ *Shrubs to be distributed around the perimeter of buildings at a minimum density of 1 plant unit per 15 linear feet of exterior building wall.*

Because the landscape plan provided for the proposed development does not identify specific numbers and locations of plant units to be include within the landscape beds around the perimeters of the proposed buildings, the following condition of approval shall be required, based on the corresponding design standard, to ensure that sufficient horizontal landscape elements will be provided within the development along the exterior walls of the buildings to soften their visual impact and create residential character as required by this design guideline:

Condition 4: Shrubs shall be planted around the perimeter of the proposed buildings at a minimum density of 1 plant unit per 15 linear feet of exterior building wall.

In review of the proposed landscape plan for the required numbers of trees to be planted around the perimeters of the proposed buildings, four of the proposed buildings, Buildings 1, 3, 5, and 6, conform to the minimum number of trees that would otherwise be required to be planted around their perimeters under the corresponding design standard. Buildings 2, 4, and 7, however, do not conform to the corresponding design standard.

Buildings 2 and 4 include only two trees where five would be required and Building 7 includes only three trees where five would be required.

However, because the proposal is being reviewed for conformance with the design guidelines, it must instead be demonstrated how the proposed development meets the design guideline of providing vertical landscape elements along exterior building walls to soften their visual impacts and create residential character.

Because Buildings 1, 3, 5, and 6 conform to the corresponding design standard in regards to the number of trees required around their perimeter, these buildings correspondingly conform to the design guideline.

It is not clear, however, how Buildings 2, 4, and 7 conform to the design guideline. The purpose of this design guideline is to require vertical landscape elements in the form of trees to soften the appearance of buildings and help obscure overall building mass. This can only be achieved, however, when required landscaping is provide around the entire perimeter of the building. Buildings 1, 3, 5, and 6 generally include perimeter trees adjacent to each of the buildings' front, side and rear facades; thereby utilizing vertical landscaping to soften the appearance of the building on all sides.

Buildings 2, 4, and 7, however, only include vertical landscaping along some of their

facades and therefore do not achieve the softened appearance and residential character required under this guideline. In order to ensure proposed Buildings 2, 4, and 7 incorporate vertical landscape elements sufficient to soften the visual impacts of the buildings and create residential character from all sides, the following condition of approval is recommended:

Condition 5: Additional trees shall be planted around the perimeters of Buildings 2, 4, and 7 as follows:

- d) Building No. 2: One additional tree shall be planted adjacent to the southeast corner of the building.
- e) Building No. 4: One additional tree shall be planted adjacent to the southeast corner of the building and one additional tree shall be planted adjacent to the southwest corner of the building.
- f) Building No. 7: One additional tree shall be planted adjacent to the northeast corner of the building and one additional tree shall be planted adjacent to the southwest corner of the building.

The proposed development, as recommended to be conditioned, conforms to this design guideline.

▪ **Privacy (SRC 702.020(e)(1)):**

(A) *Landscaping, or a combination of landscaping and fencing, shall be used to buffer the multiple family development from abutting properties.*

Finding: Pursuant to the requirements of the CR zone under SRC 522.010(b), Table 522-3, a minimum 10-foot-wide landscaped setback, together with a minimum 6-foot-tall sight-obscuring fence or wall, is required between the proposed development and abutting properties. The proposed development includes the required minimum 10-foot landscaped setback and a 6-foot-tall sight-obscuring chain link fence.

As identified, the landscaping and screening provided around the perimeter of the proposed development buffers the proposed development from abutting properties and therefore conforms to this design guideline.

(B) *Landscaping shall be used to enhance the privacy of dwelling units. Methods may include fencing in combination with plant units.*

Finding: As shown on the landscape plans, and recommended in the conditions of approval, trees will be dispersed around the perimeter of the buildings in a manner that helps to enhance the privacy of the dwelling units. In addition, ground floor private open space areas within the development will be screened for privacy from common open space areas through the use of perimeter fencing.

The planting of trees around the perimeter of the proposed buildings and the provision of fencing to screen private open space from common space act to enhance the privacy of the dwelling units. The proposed development conforms to this design guideline.

▪ **Parking Areas (SRC 702.020(f)(1)):**

(A) *Canopy trees shall be distributed throughout the interior, and planted along*

the perimeter, of parking areas (see Figure 702-4 and Figure 702-5).

Finding: As shown on the landscape plans for the proposed development, canopy trees are dispersed throughout the interior, and around the perimeter, of the proposed parking areas. The proposed development conforms to this design guideline.

Crime Prevention Through Environmental Design (SRC 702.025)

▪ **Safety Features for Residents (SRC 702.025(a)(1)):**

(A) Multiple family developments shall be designed in a manner that considers crime prevention and resident safety.

Finding: The applicant's written statement (**Attachment H**) explains that in order to ensure the safety of residents within the development there are no fences or plant materials located in areas within the development that obstruct visibility and that all landscaping adjacent to open space areas will not exceed 3 feet in height. The applicant also explains that windows have been provided in all habitable rooms, including those that face parking lots and open space areas. The applicant indicates that lighting on buildings and along sidewalks will be provided as well.

Staff concurs with the findings included within the applicant's written statement. Provision of landscaping and screening which does not obscure visibility, provision of windows in rooms facing parking and open space areas, and provision of lighting for buildings and along sidewalks ensure the proposed development has been designed in a manner that considers crime prevention and resident safety. The proposed development conforms to this design guideline.

(B) Landscaping and fencing shall be provided in a manner that does not obscure visual surveillance of common open space, parking areas, or dwelling unit entryways.

Finding: As indicated in the applicant's written statement (**Attachment H**), no fencing or plant materials are included within the development that obstruct visibility of parking areas, dwelling unit entries, and common/recreational areas. The proposed development conforms to this design guideline.

Parking, Site Access, and Circulation (SRC 702.030)

▪ **General Parking and Site Access (SRC 702.030(b)(1)):**

(A) Parking areas shall be designed to minimize the expanse of continuous parking (see Figure 702-6).

Finding: The applicant's written statement indicates that due to the required extension of Harold Drive and the location of the property, adding additional planter islands and/or bays is not feasible. It is explained, however, that the parking areas are heavily landscaped, as shown on the site plan, and that the landscaped areas, along with the proposed planter islands, help to minimize the expanse of continuous parking.

The proposed development conforms to this design guideline.

(B) Pedestrian pathways shall be provided that connect to and between buildings,

common open space, parking areas, and surrounding uses (see Figure 702-7).

Finding: As indicated in the applicant's written statement (**Attachment H**) and shown on the proposed site plan, the proposed internal pedestrian circulation system consists of 4-foot to 6-foot wide sidewalks that provide easily identifiable and safe connections between the residential units, parking, recreation areas, manager's apartment, and trash disposal areas. The internal pedestrian sidewalk system also connects the buildings to the public sidewalk system within Harold Drive.

As described above, pedestrian pathways are provided within the development which connect to and between buildings, common open space, parking areas, and surrounding uses. The proposed development conforms to this design guideline.

(C) *Parking shall be located to maximize the convenience of residents.*

Finding: As indicated in the applicant's written statement (**Attachment H**) and shown on the proposed site plan, all buildings are connected to the internal parking areas via 4-foot to 6-foot wide sidewalks. The parking areas are located in areas that are convenient for the residents.

As described above, the proposed parking areas within the development are located within close proximity to the dwelling units they serve. An interconnected system of pedestrian pathways/sidewalks provide for convenient access between parking areas and dwelling units. The proposed development conforms to this design guideline.

(D) *Parking areas and circulation systems shall be designed in a manner that considers site topography, natural contours, and any abutting properties zoned Residential Agriculture (RA) or Single Family Residential (RS).*

Finding: The applicant's written statement (**Attachment H**) indicates that all parking areas and the layout of the site took topography and adjacent properties into consideration as shown on the site plan.

Because the topography of the subject property is flat, there are no special parking design considerations required to be considered in relation to the proposed development. There are also no RA (Residential Agriculture) or RS (Single Family Residential) zoned properties which abut the subject property. The proposed development conforms to this design guideline.

▪ **Site Access (SRC 702.030(c)(1)):**

(A) *Accessibility to and from the site shall be provided for both automobiles and pedestrians.*

Finding: The applicant's written statement (**Attachment H**) explains that the development has street frontage on Harold Drive along the north property line via a stubbed connection. The applicant indicates a north/south Harold Drive connection through the development, as shown on the site plan, is required. The applicant explains that all parking areas within the development will be served by 24-foot to 26-foot wide two-way accessways that run through the development.

Staff concurs with the findings included in the applicant's written statement. Automobile access to and from the eastern and western portions of the proposed

development will be provided via two separate driveway approaches on each side of Harold Drive. Pedestrian access to and from the site, and throughout the interior of the proposed development, is provide via an internal system of pedestrian pathways/sidewalks. The proposed design conforms to this design guideline.

(B) Site access shall be provided in a manner that minimizes vehicle and pedestrian conflicts.

Finding: The applicant's written statement (**Attachment H**) indicates that the proposed sidewalks within the development are raised above the surface of the travel lanes and this provides a clear separation between vehicle and pedestrians. The applicant explains that any pedestrian pathways that cross the parking are or driveways will be minimum of five feet in width and marked. In addition, the applicant indicates that pedestrian pathways will also be lighted.

Staff concurs with the findings included in the applicant's written statement. The proposed design of the pedestrian circulation system within the development safely separates pedestrians from vehicles and minimizes the potential for vehicle and pedestrian conflicts. The proposed development conforms to this design guideline.

(C) Where possible, driveway access shall be provided onto collector or local streets rather than arterial streets.

Finding: Subsequent to the recording of the final plat for Partition Case No. PAR15-06, the portion of the subject property proposed to be developed with the 84-unit multiple family development will no longer have frontage on Lancaster Drive, an arterial street, and instead will only have frontage on the extension of Harold Drive NE. Harold Drive is designated as a local street under the City's Transportation System Plan. Driveway access to the eastern and western portions of the development will be taken from this local street. The proposed development conforms to this design guideline.

(D) Where possible, driveway access shall be consolidated with either existing or future driveways serving adjacent developments.

Finding: The subject property abuts developed properties to north, south, east, and west. Due to constraints relating to the configuration of development on abutting properties which physically precludes shared access, or limitations on access to streets with higher classifications imposed under the City's multi-family design standards, shared/consolidated access with adjacent developments is not possible.

The proposed development does, however, extend Harold Drive through the property to its southern boundary for a future connection to Devonshire at some point in the future. The connection of Harold Drive to Devonshire is identified in the City's Transportation System Plan (TSP) and will help to provide additional north-south connectivity in this area. Because shared/consolidated driveway access with adjacent developments is not possible, this design guideline is not applicable to the proposed development.

(E) Parking areas shall be located to minimize their visibility from the public right-of-way and abutting properties (see Figure 702-8).

Finding: The applicant indicates that due to the two fixed locations for the

extension of Harold Drive through the site, driveway access is limited. The applicant explains that the layout of the lot was taken into consideration and the proposed layout was found to be the safest and most feasible for the site.

As indicated by the applicant, the location of the parking areas within the development was predicated on access limitations presented by the fixed locations of the extension of Harold Drive NE.

Though the resulting design includes parking areas located adjacent to both sides of Harold Drive, the proposed design does, however, include landscaped setbacks between the proposed parking areas and the street to help minimize the visibility of the parking areas from the right-of-way as required by this design guideline. In addition, recommended condition of approval No. 1 of the site plan review approval requires six of the parking spaces adjacent to the right-of-way on the east side of Harold Drive to be removed to meet perimeter setback requirements. Removing these parking spaces helps to further minimize the appearance of parking areas from the right-of-way. The proposed development conforms to this design guideline.

Building Mass & Facade Design (SRC 702.035)

▪ **General Siting and Building Mass (SRC 702.035(b)(1)):**

(A) Buildings shall be sited with sensitivity to topography and natural landform (see Figure 702-9).

Finding: Because the topography of the subject property is flat, there are no special building siting considerations relating to site topography and natural land form required to be considered.

The proposed buildings within the development have been sited to respond to the configuration of the lot, the extension of Harold Drive NE through the central portion of the site, points of driveway access to Harold Drive, and required building setbacks. The proposed development conforms to this design guideline.

(B) The development shall be designed to reinforce human scale.

Finding: The applicant's written statement (**Attachment H**) indicates that the designs of the various buildings within the development do not have long flat walls or roof lines. The applicant explains the proposed buildings incorporate offsets and balconies to break up overall building mass and add some visual element to the buildings. The applicant indicates that none of the buildings within the development exceed 150 feet in length and that the heights of all buildings and structures conform to the maximum height limits of the code.

The proposed development conforms to this design guideline.

(C) Buildings with long monotonous exterior walls shall be avoided.

Finding: The applicant's written statement (**Attachment H**) indicates that varied materials and textures are being used on the building facades. The applicant explains that the materials used on the front, rear, and sides of the apartments are the same; shake siding, trim board, lap siding, and stone around the pillars.

In review of the proposed building designs, portions of the front and rear facades of

the buildings do include some changes in siding material. The ends of the buildings, however, do not include the same changes in siding material as is provided on the front and rear facades. In order to ensure the proposed buildings avoid long monotonous exterior walls as required by this design guideline, the following condition of approval is recommended:

Condition 6: The proposed buildings shall be revised to provide additional contrast and distinction between the ground floor and upper floor facades by incorporating one or more of the following:

- c) Vertically oriented lap siding, or horizontally oriented lap siding that is wider than that provided on the upper floor facades, that is painted a different color than the upper floor facades; or
- d) A siding material different from that used in the other portions of the building facades.

The proposed development, as recommended to be conditioned, conforms to this design guideline.

▪ **Compatibility (SRC 702.035(c)(1)):**

(A) *Contrast and compatibility shall be provided throughout the site through building design, size, and location.*

Finding: Contrast and compatibility is provided throughout the proposed development in regards to building design, size, and location. This is achieved through the placement of windows and utilization of consistent and compatible materials throughout the building facades, incorporation of building offsets, and inclusion of patios/balconies. The proposed design conforms to this design guideline.

(B) *Appropriate transitions shall be provided between new buildings and structures onsite and existing buildings and structures on abutting sites.*

Finding: Abutting properties to the north, south, and west of the subject property are fully developed. The remaining undeveloped portion of the subject property fronting on Lancaster Drive to the east of the proposed development will be separated from the proposed development with the recording of the final plat for Partition Case No. PAR15-06. That property will be developed separately from the proposed multi-family development.

The proposed development provides an appropriate transition between the new buildings and structures on-site and the existing buildings and structures on abutting properties through conformance with the setback requirements and development standards of the CR zone. The proposed development conforms to this design guideline.

(C) *Architectural elements and facade materials shall be used to provide continuity throughout the site.*

Finding: As indicated in the applicant's written statement (**Attachment H**) and illustrated by the elevation drawings for the proposed buildings, architectural elements and facade materials are included within the designs of the buildings to

provide continuity throughout the site. These include building offsets, windows within building facades, utilization of different building materials including shake siding, trim board, lap siding, and stone around the pillars, and articulated common building entries. In addition, recommended condition of approval No. 6 of the design review approval requires a different siding material to be utilized on the ground floor facades of the buildings to help reduce the appearance of building mass and bulk. This condition also helps the proposed buildings to conform to better conform to this design guideline by increasing the variety of materials utilized within the building facades. A distinct ground floor façade material utilized around the perimeter of the buildings provides additional visual continuity between the buildings and throughout the site as required by this design guideline. The proposed development, as recommended to be conditioned, conforms to this design guideline.

- (D) *The majority of dwelling units within the development shall be placed as close as possible to the street right-of-way.***

Finding: The applicant indicates that due to the fixed locations for the extension of Harold Drive, the proposed development is limited on driveway access and therefore moving the buildings closer to the right-of-way is not feasible. The applicant explains that the layout of the lot was taken into consideration and the proposed layout was found to be the safest and most feasible for the site due to access location and circulation.

As shown on the proposed site plan, the majority of the apartment buildings within the development are located away from the street on the perimeter of the proposed development and the parking and internal vehicle circulation areas of the development are generally located within the central portions of the development.

As indicated by the applicant, the extension of Harold Drive through the site and needed driveway access to the development create complications regarding building placement on the site. Despite these complications, proposed Buildings 1, 4, 5, and 7 have been sited in proximity to Harold Drive. The proposed development conforms to this design guideline.

- (E) *Architecturally defined and covered entryways shall be incorporated into the design of buildings.***

Finding: The applicant's written statement (**Attachment H**) indicates that all building entrances face the internal street/parking system. The applicant indicates that the primary entrances for each individual unit is provided through a covered entry way and that all building entries are clearly defined and easily accessible. The applicant explains that the designs of the buildings, with the use of roofline offsets and covered entry ways, promote a positive sense of neighborhood.

The proposed development conforms to this design guideline.

▪ **Building Articulation (SRC 702.035(d)(1)):**

- (A) *The appearance of building bulk shall be minimized by:***
(i) Establishing a building offset interval along building facades; and
(ii) Dispersing windows throughout building facades.

Finding: As shown on the site plan and building elevations for the proposed development, though the proposed buildings do not have four-foot building offsets to separate every two attached dwelling units from the next dwelling unit as required by the corresponding design standard to this design guideline, the proposed buildings do include various offsets ranging from 2 feet to 6 feet in depth around the perimeters of the proposed buildings.

In addition to requiring offsets within building facades, this design guideline also requires windows to be dispersed throughout building facades as another means of helping to break up the overall appearance of building mass and building bulk.

As shown on the elevation drawings for the proposed buildings, Buildings 4 through 7 include windows dispersed regularly throughout the facades in conformance with this design guideline. Portions of the facades of Buildings 1, 2, and 3, however, include areas where blank walls exist without the benefit of windows to reduce the appearance of undivided building mass and bulk. In order to ensure Buildings 1, 2, and 3 include windows dispersed throughout their facades as required by this design guideline, the following condition of approval is recommended:

Condition 7: The facades of Buildings 1, 2, and 3 shall be revised to incorporate additional windows as follows:

- d) Building No. 1: Windows shall be provided on the western façade of the building within the bedrooms on each floor.
- e) Building No. 2: Windows shall be provided on the northern façade of the building within the bathrooms on the third floor.
- f) Building No. 3: Windows shall be provided on the eastern façade of the building within the bedrooms on each floor.

The proposed buildings, as recommended to be conditioned, conform to this design guideline.

(B) *Articulation shall be provided at the common entryway to all residential buildings.*

Finding: As indicated in the finding evaluating the proposed development's conformance with the design guideline requiring architecturally defined and covered entryways, all buildings include architecturally defined common entryways.

The development conforms to this design guideline.

(C) *Building roofs shall reinforce the residential character of the neighborhood.*

Finding: As illustrated by the building elevations for the proposed development, the rooflines of the buildings within the development have generally been limited in length. The longest roof dimensions included within the development, measured from outside edge of eave to outside edge of eave, are associated with Buildings 4 through 7. The overall roof dimension of these buildings is approximately 103 feet, which is only 3 feet more than the maximum 100-foot roof length allowed under the corresponding design standard without a minimum 4-foot roof line offset being required. As indicated in the applicant's written statement (**Attachment H**), the proposed buildings also include breaks in their rooflines.

By limiting the overall building roof line lengths and providing offsets to those roof lines, the residential character of the neighborhood is reinforced. The proposed development conforms to this design guideline.

Recycling (SRC 702.040)

▪ ***On-Site Design and Location of Facilities (SRC 702.040(a)(1)):***

- (A) ***Facilities shall be provided to allow recycling opportunities for tenants that are as conveniently located as the trash receptacles, and that are in compliance with any applicable federal, state, or local laws.***

Finding: The proposed development includes two trash enclosure areas, one within the western portion of the development and the other within the eastern portion of the development. The trash enclosure areas include retainers for recycling within them.

The trash/recycling areas have been designed to conform to the solid waste service area standards of the SRC 800.055 and have been distributed throughout the site to allow for convenient access by residents. The proposed development conforms to this design guideline.

- (B) ***The design and materials of recycling areas shall be similar to the design and materials of the buildings within the development.***

Finding: The proposed trash/recycling areas will be enclosed by a concrete masonry unit (CMU) block wall, with a chain link fence gate with slats, and covered by a wood frame structure that is compatible in design and materials to the buildings within the development. The proposed development conforms to this design guideline.

- (C) ***Recycling areas shall be located to provide adequate access for franchised haulers, and shall have containers sufficient to allow collection of all recyclables collected by the haulers.***

Finding: The proposed trash/recycling areas are sited in conformance with the solid waste service area standards of SRC 800.055 in order to allow for adequate access and maneuvering by the franchised haulers. Each of the proposed trash/recycling areas include a three cubic yard box for garbage, as well as three separate containers for recycling. The proposed development conforms to this design guideline.

FINDINGS ADDRESSING APPLICABLE SALEM REVISED CODE APPROVAL CRITERIA FOR CLASS 3 SITE PLAN REVIEW

11. CLASS 3 SITE PLAN REVIEW APPROVAL CRITERIA

Salem Revised Code (SRC) 220.005(f)(3) sets forth the following criteria that must be met before approval can be granted to an application for Class 3 Site Plan Review. The following subsections are organized with approval criteria shown in ***bold italic***, followed by findings evaluating the proposed development's conformance with the criteria. Lack of compliance with the following criteria is grounds for denial of the Class 3 Site Plan Review application, or for the issuance of certain conditions to ensure the criteria are met.

- (A) ***The application meets all applicable standards of the UDC.***

Finding: The proposed development includes 84 multiple-family dwelling units with

associated parking and vehicle circulation areas.

The subject property is designated "Commercial" on the Salem Area Comprehensive Plan Map and zoned CR (Commercial Retail). The allowed uses and applicable development standards of the CR zone are set forth under SRC Chapter 522.

The proposed development will conform to the applicable development standards of the Salem Revised Code as described below:

SRC CHAPTER 522 (CR ZONE)

SRC 522.005 - Allowed Uses:

Allowed uses within the CR zone are identified under SRC 522.005, Table 522-1. The subject property is proposed to be developed as an 84-unit multiple family development. Multiple Family is allowed as a conditional use within the CR zone subject to receiving approval of a conditional use permit. As part of the development proposal for the site the applicant has submitted an application for a conditional use permit. Findings addressing the proposal's conformance with the Conditional Use approval criteria are contained in Section 9 of this report. As identified in that section, the proposed development satisfies the conditional use approval criteria. The proposed multiple family development therefore is an allowed use within the CR zone.

SRC 522.010(a) - Lot Standards:

Lot standards within the CR zone are established under SRC 522.010(a), Table 522-2. Within the CR zone there are no minimum lot area, width, or depth requirements. Lots for uses other than Single Family are, however, required to have a minimum street frontage of 16 feet.

Prior to filing the consolidated application for the proposed 84-unit multiple family development, tentative approval was granted to Partition Case No. PAR15-06 to divide the subject property into three parcels and extend Harold Drive NE to the southern boundary of the subject property. The two westernmost parcels included in the partition, Parcels 1 and 2, will be developed for the proposed 84-unit multiple family development. The easternmost parcel, Parcel 3, will be developed separately from the proposed multiple family development.

Parcel 1 of Partition Case No. PAR15-06 is approximately 60,884 square feet in size and Parcel 2 is approximately 79,369 square feet in size. Both parcels exceed the minimum lot size, lot dimension, and street frontage requirements of the CR zone.

SRC 522.010(b) - Setbacks:

Setbacks for buildings, accessory structures, and parking and vehicle uses areas within the CR zone are generally established under SRC 522.010(b), Tables 522-3 and 522-4.

Because multiple family development within the CR zone is subject to multiple family design review, the proposed development must also conform to the additional building, accessory structure, and parking and vehicle use area setbacks established under SRC Chapter 702 (Multiple Family Design Guidelines and Standards). As identified under Section 10 of this report, the development, as

proposed to be conditioned, conforms to the applicable design review guidelines for multiple family developments.

A summary of the CR zone setbacks applicable to the proposed multiple development is included in the table below:

Summary of Required CR Zone Setbacks	
Abutting Street	
Buildings	Min. 5 ft.
Accessory Structures	Min. 5 ft.
Parking and Vehicle Use Areas	Min. 6 ft. to 10 ft. <i>(Per alternative setback methods under SRC 806.035(c)(2))</i>
Interior Side	
Buildings	Min. 10 ft. with Type C Landscaping & Screening <i>(includes Min. 6-foot sight-obscuring fence or wall)</i>
Accessory Structures	Min. 10 ft. with Type C Landscaping & Screening <i>(includes Min. 6-foot sight-obscuring fence or wall)</i>
Parking and Vehicle Use Areas	Min. 10 ft. with Type C Landscaping & Screening <i>(includes Min. 6-foot sight-obscuring fence or wall)</i>
Interior Rear	
Buildings	Min. 10 ft. with Type C Landscaping & Screening <i>(includes Min. 6-foot sight-obscuring fence or wall)</i>
Accessory Structures	Min. 10 ft. with Type C Landscaping & Screening <i>(includes Min. 6-foot sight-obscuring fence or wall)</i>
Parking and Vehicle Use Areas	Min. 10 ft. with Type C Landscaping & Screening <i>(includes Min. 6-foot sight-obscuring fence or wall)</i>

The proposed apartment buildings conform to the setback requirements of the CR zone. The apartment buildings are setback more than 5 feet from the right-of-way of Harold Drive and 10 feet and greater from interior side and rear property lines.

The proposed parking and vehicle uses areas within the development also conform to the setback requirements of the CR zone with the exception of the first six parking spaces and corresponding parking lot drive aisle to the north of Building No. 4 that are located off the southern driveway approach on the eastern side of Harold Drive.

With the exception of these six parking spaces and corresponding drive aisle, the parking and vehicle use areas within the development are setback at least 10 feet from interior side and rear property lines and the right-of-way of Harold Drive.

As shown on the proposed site plan (**Attachment B**), the six parking spaces and corresponding drive aisle to the north of Building No. 4 are setback less than 10 feet from the right-of-way of Harold Drive, and in some instances do not include any setback which results in a potential safety issue where the backing/maneuvering area for spaces is immediately abutting the driveway approach into the proposed development.

In addition to the perimeter setback required between parking and vehicle use areas and adjacent streets, the multiple family design review guidelines of SRC 702.030(c)(1)(E) require parking areas for multiple family developments to be located to minimize their visibility from the public right-of-way and abutting properties.

The design as currently proposed results in this parking and maneuvering area being located immediately adjacent to Harold Drive without a required setback.

In order to ensure the proposed development conforms to the minimum parking and vehicle use area setbacks adjacent to Harold Drive and the parking spaces in this area are designed to minimize their visibility from the right-of-way, the following condition of approval is recommended:

Condition 1: The site plan shall be revised to remove the first six parking stalls to the north of Building No. 4 that are located off the southern driveway approach on the eastern side of Harold Drive. Where the parking spaces are removed, they shall be replaced with landscaping.

SRC 522.010(c) - Lot Coverage:

Lot coverage requirements within the CR are established under SRC 522.010(c), Table 522-5. Within the CR zone there is no maximum lot coverage for buildings and accessory structures.

SRC 522.010(c) - Height:

Maximum height for buildings and accessory structures within the CR zone is established under SRC 522.010(c), Table 522-5. The maximum height for buildings within the CR zone is 50 feet. The maximum height for multiple family accessory structures within the CR zone is 15 feet.

As illustrated by the elevation drawings for the apartment buildings, none of the proposed apartments exceed the maximum 50-foot CR zone height limit. As indicated on the site plan, the roof structures over the proposed trash enclosures do not exceed the maximum 15-foot CR zone accessory structure height limit.

SRC 522.010(d) - Landscaping:

Landscaping requirements within the CR zone are established under SRC 522.010(d). Within the CR zone, landscaping is required as follows:

- **Setbacks:** Required setbacks must be landscaped as required under SRC Chapter 807 (Landscaping).
- **Parking & Vehicle Use Areas:** Parking and vehicle use areas must be landscaped pursuant to the requirements of SRC Chapter 807 (Landscaping) and SRC Chapter 806 (Off-Street Parking, Loading, & Driveways).
- **Development Site:** A minimum of 15 percent of the development site must also be landscaped. Other required landscaping, such as landscaping required for setbacks and vehicle use areas, may count towards meeting the 15 percent landscaping requirement.

As shown on the landscape plans for the proposed development, and as established by the findings addressing conformance of the proposed development with the multiple family design review landscaping requirements of SRC Chapter 702, required setbacks and parking and vehicle use areas are landscaped in conformance with the requirements of SRC Chapters 702, 806, and 807.

As shown on the applicant's proposed site plan, approximately 15.4 percent, or 12,226 square feet, of the eastern portion of the development is landscaped and approximately 33 percent, or 20,091 square feet, of the western portion of the

development will be landscaped. The landscaping proposed for the eastern and western portions of the proposed development exceeds the minimum 15 percent overall development site landscaping required under the CR zone.

Because the portion of the subject property proposed for the 84-unit multiple family development is still included as part of a larger overall 4.31 acre property with frontage on Lancaster Drive, the proposed multi-family portion of the property will need to be separated from the remainder of the property if the proposed development is to maintain conformance with the applicable landscaping requirements of the CR Zone. As previously indicated, Partition Case No. PAR15-06 received tentative approval on August 18, 2015, to divide the overall property into three parcels. The proposed multi-family development occupies the two westernmost parcels included in the partition. In order to ensure that the portion of the property proposed for the multiple family development is legally separated from the remainder of the subject property, the following condition of approval is recommended:

Condition 2: Prior to building permit approval, the final plat for Partition Case No. PAR15-06 shall be recorded.

SRC CHAPTER 806 (OFF-STREET PARKING, LOADING, & DRIVEWAYS)

SRC Chapter 806 establishes requirements for off-street parking, loading, and driveways. Included in the chapter are standards for minimum and maximum off-street vehicle parking; minimum bicycle parking; minimum loading; and parking, bicycle parking, loading, and driveway development standards.

Off-Street Parking: Minimum off-street vehicle parking requirements are established under SRC Chapter 806, Table 806-1. The minimum off-street parking requirement for Multiple Family is as follows:

Minimum Off-Street Parking		
Multiple Family	1.5 spaces per dwelling units	Applicable to Multiple Family consisting of 4 or more dwelling units

Maximum off-street parking requirements are established under SRC Chapter 806, Table 806-2. The maximum number of allowed parking spaces is based upon the minimum number of spaces required for the proposed development. If the minimum number spaces required equals 20 spaces or less, the maximum allowed parking is 2.5 times the minimum number of spaces required. If the minimum number of spaces required equals more than 20 spaces, the maximum allowed parking is 1.75 times the minimum number of spaces required.

The 84 dwelling units proposed within the development results in the following off-street parking requirement:

Off-Street Parking Summary			
Portion of Development Site	Minimum Spaces Req.	Maximum Spaces	Spaces Provided
East Lot (48 Units)	72	126	86

West Lot (36 Units)	54	95	52
Total (84 Units):	126	221	138

As shown on the site plan for the proposed development, a total of 138 parking spaces are provided within the proposed development, with 86 of the spaces being located on the eastern portion of the property and 52 of the spaces being located on the western portion of the property. The 138 parking spaces proposed for the development meets the overall off-street parking requirement for the development, but when the individual parking requirements for the eastern and western portions of the property are reviewed separately, the western lot is two spaces short of its minimum off-street parking requirement.

Within non-residential zones, SRC 806.010(b) allows required off-street parking to be located within 500 feet of the development site containing the use or activity it serves. Because off-street parking for the eastern portion of the subject property exceeds minimum off-street parking requirements, there are two additional spaces available on the eastern portion of the property to accommodate the minimum required parking for the western portion of the development.

When required off-street parking is provided off-site from the property it serves, SRC 806.020 requires the off-street parking spaces to either be owned by the owner of the property served by the parking or leased. In response to this issue, the applicant's representative indicated that a parking agreement for both sides of the development will be recorded prior to development of the site that will allow both the east and west sides of the development to share parking whether or not both parcels remain under the same ownership.

In order to ensure that parking for the western portion of the development will meet the minimum off-street parking requirements of SRC Chapter 806, the following condition of approval is recommended:

Condition 3: Prior to building permit approval, a parking lease agreement shall be executed between the eastern and western portions of the proposed development. The agreement shall meet the requirements of SRC 806.020 and shall, at a minimum, allow for two spaces on the eastern lot to be utilized by the western lot to satisfy minimum off-street parking requirements.

As previously identified in the off-street parking summary table above, the eastern portion of the proposed development provides a total of 86 off-street parking spaces, which is 14 more spaces than the minimum 72 spaces required based on the number of dwelling units proposed.

As recommended under Condition 1 earlier in this section, the parking area for the eastern portion of the development will need to be revised to remove 6 parking spaces in order to meet perimeter parking setback requirements abutting Harold Drive. In addition, two parking spaces within the eastern portion of the development will also need to be reserved to meet the off-street parking requirements of the western portion of the development. This required reduction of 8 spaces will reduce the total number of parking spaces available on the eastern portion of the property from 86 to 78, which still exceeds the minimum 72 spaces required for the eastern portion of the development.

Compact Parking: SRC 806.015(b) allows for the utilization of compact parking stalls to satisfy up to 75 percent of the required off-street parking spaces.

The proposed development includes a total of 80 compact parking spaces, with 51 of the spaces being located on the eastern portion of the property and 29 of the spaces being located on the western portion of the property.

The 29 compact spaces proposed for the western portion of the development equal approximately 55.8 percent of the overall 52 spaces provided; therefore not exceeding the maximum 75 percent limit.

The 51 compact spaces proposed for the eastern portion of the development equal approximately 59.3 percent of the overall 86 spaces provided. However, because six parking stalls within the eastern portion of the development are required to be removed in order to meet parking setbacks abutting Harold Drive, the percentage of compact spaces provided within the eastern portion of the development corresponding increases from 59.3 percent to 63.8 percent, which still falls below the maximum 75 percent limit.

The proposed compact spaces within the development do not exceed the maximum number of spaces allowed and therefore conform to this standard.

Bicycle Parking: Minimum bicycle parking requirements are established under SRC Chapter 806, Table 806-8. The minimum bicycle parking requirement for Multiple Family is as follows:

Minimum Bicycle Parking	
Multiple Family	The greater of 4 spaces or 0.1 spaces per dwelling unit

The 84 dwelling units proposed within the development result in the following bicycle parking requirement:

Bicycle Parking Summary		
Portion of Development Site	Minimum Spaces Req.	Spaces Provided
East Lot (48 Units)	5	8
West Lot (36 Units)	4	6
Total (84 Units):	9	14

As shown on the site plan for the proposed development and summarized in the table above, a total of 14 bicycle parking spaces are provided for the development, with 8 of the spaces located within the eastern portion of the development and 6 of the spaces located within the western portion of the proposed development.

The number of bicycle parking spaces included within the development conforms to this standard.

Loading: Minimum loading requirements are established under SRC Chapter 806, Table 806-9. The minimum loading requirement for Multiple Family is as follows:

Minimum Loading		
Multiple Family	0 to 49 dwelling units	None
	50 to 99 dwelling units	Min. 1 space 12ft. W x 19ft. L x 12 ft. H

The 84 dwelling units proposed within the development result in a minimum loading requirement of 1 space. In order to ensure the proposed development conforms to the minimum loading requirements of SRC Chapter 806, the following condition of approval is recommended:

Condition 4: A minimum of one loading space meeting the requirements of SRC Chapter 806 shall be provided within the development.

SRC 800.055 (SOLID WASTE SERVICE AREAS)

SRC 800.055 establishes design standards that apply to all new solid waste, recycling, and compostable service areas, where use of a solid waste, recycling, and compostable receptacle of 1 cubic yard or larger is proposed.

As shown on the site plan for the proposed development, two trash/recycling areas accommodating trash receptacles of 3 cubic yards in size are included within the development. The trash/recycling areas are enclosed by a minimum 7-foot-tall CMU block wall, have an interior dimension within the enclosure of approximately 14.7 feet in width by 11.3 feet in depth, are free of vertical obstructions above the receptacles, and include an unobstructed vehicle operation area in front of each receptacle that is a minimum of 12 feet in width by 45 feet in length.

The proposed trash/recycling areas appear to meet the applicable standards of SRC Chapter 800.055. At the time of building permit, the location and features of the proposed trash/recycling areas will be reviewed for conformance with applicable development standards of SRC 800.055. In order to ensure the proposed trash/recycling areas conform to the applicable standards of SRC 800.055, the following condition of approval is recommended:

Condition 5: All trash/recycling areas shall conform to the solid waste service area standards of SRC 800.055.

SRC CHAPTER 808 (PRESERVATION OF TREES & VEGETATION)

The City's tree preservation ordinance (SRC Chapter 808) protects Heritage Trees, Significant Trees (including Oregon White Oaks with diameter-at-breast-height of 24 inches or greater), trees and native vegetation in riparian corridors, and trees on lots and parcels greater than 20,000 square feet. The tree preservation ordinance defines "tree" as, "any living woody plant that grows to 15 feet or more in height, typically with one main stem called a trunk, which is 10 inches or more dbh, and possesses an upright arrangement of branches and leaves."

Under the City's tree preservation ordinance, pursuant to SRC 808.035(a), tree conservation plans are required in conjunction with development proposals involving the creation of lots or parcels to be used for the construction of single family or duplex dwelling units, if the development proposal will result in the removal of trees.

There are five trees present on the subject property (two cottonwoods, a walnut, a maple, and a birch); none of the trees are designated as significant or heritage and none are located within a riparian corridor.

The applicant's development plan identifies all five of the existing trees for removal. Because the proposal does not involve the creation of lots or parcels to be used for the construction of single family or duplex dwelling units, a tree conservation plan is

not required in conjunction with the proposed development. The removal of trees from the property cannot occur without first obtaining the necessary approvals established under SRC Chapter 808.

SRC CHAPTER 809 (WETLANDS):

Grading and construction activities within wetlands are regulated by the Oregon Department of State Lands (DSL) and US Army Corps of Engineers. State and Federal wetlands laws are also administered by the DSL and Army Corps, and potential impacts to jurisdictional wetlands are addressed through application and enforcement of appropriate mitigation measures.

SRC Chapter 809 requires notice to DSL for applications for development or land use in areas designated as wetlands on the official wetlands map. The Salem-Keizer Local Wetland Inventory (LWI) does not identify any mapped wetlands or waterways on the subject property. There is, however, a band of hydric/wetland type soils running north-south through the eastern portion of the proposed multiple family development.

Because of the presence of hydric soils on the subject property, notice of the prior partition was sent to DSL. Comments received from DSL indicated that a wetland delineation by a professional wetland consultant is recommended prior to development. The DSL indicates that the wetland delineation report should be submitted to DSL for review and concurrence.

Because no mapped wetlands are currently identified on the subject property, SRC Chapter 809 does not specify any additional requirements as a condition of development. The applicant is advised to coordinate any required wetland delineations with DSL.

SRC CHAPTER 810 (LANDSLIDE HAZARDS)

The City's landslide hazard ordinance (SRC Chapter 810) establishes standards and requirements for the development of land within areas of identified landslide hazard susceptibility. According to the City's adopted landslide hazard susceptibility maps, the subject property is mapped with a small area of 2 landslide hazard susceptibility points. There are 2 activity points associated with the multiple family building permits. Pursuant to the requirements of SRC 810.025, the cumulative total of 4 points between those associated with the land and those associated with the proposed development activity indicates a low landslide risk and therefore a geologic assessment or geotechnical report is not required in conjunction with the proposed development.

As is described in the findings above, the proposed development, as proposed to be conditioned, conforms to the applicable development standards of the SRC. This criterion is met.

- (B) The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately;**

Finding: The existing configuration of Harold Drive NE does not meet current standards for its classification of street per the Salem Transportation System Plan (TSP). Harold Drive is being extended to the south line of the subject property to provide sufficient access for the proposed development. In order to ensure that

Harold Drive NE is extended through the subject property as required by the City's TSP, the following conditions of approval are recommended:

Condition 6: Dedicate as public right-of-way the existing reserve strip located at the southerly terminus of Harold Drive NE.

Condition 7: Prior to issuance of certificate of occupancy for any apartment building within the proposed development, construct Harold Drive NE to local street standards with curb line sidewalks within the subject property and connecting to the southerly terminus of the existing Harold Drive improvements.

The proposed development, as recommended to be conditioned, conforms to this approval criterion.

(C) Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians; and

Finding: Vehicular access to the proposed development is provided via two driveway approaches onto Harold Drive NE from the eastern portion of the development, two driveway approaches onto Harold Drive NE from the western portion of the development, and in internal system of driveways running throughout the interior of the development.

Pedestrian access to and throughout the interior of the development is provided by a separate network of pedestrian pathways/sidewalks distributed throughout the site. The proposed pedestrian pathways/sidewalks are raised above the surface of vehicle travel lanes and striped crosswalks are provided where pedestrian pathways/sidewalks must cross driveways. These features provide a clear separation between vehicles and pedestrians and ensure that the proposed parking areas within the development minimize vehicle and pedestrian conflicts throughout the site.

As is described in the findings addressing the development's conformance with the applicable Class 2 Driveway Approach Permit and corresponding Class 2 Adjustment approval criteria contained in Sections 12 and 13 of this report, the driveway approaches onto Harold Drive, as proposed to be adjusted, meet applicable driveway approach siting criteria. The proposed development ensures safe and efficient movement of vehicles, bicycles, and pedestrians. This criterion is met.

(D) The proposed development will be adequately served with City water, sewer, stormwater facilities, and other utilities appropriate to the nature of the development.

Finding: The Public Works Department has reviewed the applicant's preliminary utility plan for this site. Water, sewer, and storm infrastructure are available within surrounding streets/areas and appear to be adequate to serve the proposed development. The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director. The applicant is advised that a sewer monitoring manhole may be required, and the trash area shall be designed in compliance with Public Works Standards.

The applicant's engineer submitted a statement demonstrating compliance with

Stormwater PWDS Appendix 004-E(4)(b) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible.

In order to ensure the proposed development conforms to the requirements of SRC Chapter 71, the following condition of approval is recommended:

Condition 8: Design and construct stormwater facilities in accordance with SRC Chapter 71.

FINDINGS ADDRESSING APPLICABLE SALEM REVISED CODE APPROVAL CRITERIA FOR CLASS 2 DRIVEWAY APPROACH PERMIT

12. CLASS 2 DRIVEWAY APPROACH PERMIT APPROVAL CRITERIA

Salem Revised Code (SRC) 804.025(d) sets forth the following criteria that must be met before approval can be granted to an application for a Class 2 Driveway Approach Permit. The following subsections are organized with approval criteria shown in ***bold italic***, followed by findings evaluating the proposed development's conformance with the criteria. Lack of compliance with the following criteria is grounds for denial of the Class 2 Driveway Approach Permit, or for the issuance of certain conditions to ensure the criteria are met.

(1) *The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;*

Finding: The proposed driveways meet the standards for SRC 804 and PWDS with a Class 2 adjustment for multiple driveway approaches onto a local street where only one is allowed SRC 804.030(a). The Class 2 adjustment is warranted because multiple accesses are needed given the limited area for internal parking circulation. Pursuant to Class 2 adjustment criteria in SRC 250.005(d)(2), the multiple driveway accesses provide equal or better access for the proposed development because vehicle turning movements have a negligible effect on local streets.

(2) *No site conditions prevent placing the driveway approach in the required location;*

Finding: There are no site conditions prohibiting the location of the proposed driveways.

(3) *The number of driveway approaches onto an arterial are minimized;*

Finding: The proposed driveways are not accessing onto an arterial street.

(4) *The proposed driveway approach, where possible:*

(A) *Is shared with an adjacent property; or*

(B) *Takes access from the lowest classification of street abutting the property;*

Finding: The proposed driveways are currently located on Harold Drive with access to the lowest classification of street abutting the subject property.

(5) *The proposed driveway approach meets vision clearance standards;*

Finding: The proposed driveways meet the PWDS vision clearance standards set forth in SRC Chapter 805.

- (6) ***The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;***

Finding: The proposed driveways will not create a known traffic hazard and will provide for safe turning movements for access to the subject property.

- (7) ***The proposed driveway approach does not result in significant adverse impacts to the vicinity;***

Finding: The location of the proposed driveways do not appear to have any adverse impacts to the adjacent properties or streets.

- (8) ***The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and***

Finding: The proposed driveway approaches are located on a local street and do not create a significant impact to adjacent streets and intersections.

- (9) ***The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.***

Finding: The proposed driveway approaches are not located in the vicinity of a residentially zoned area. The driveway will not have an effect on the functionality of the adjacent streets.

FINDINGS ADDRESSING APPLICABLE SALEM REVISED CODE APPROVAL CRITERIA FOR CLASS 2 ADJUSTMENT

13. CLASS 2 ADJUSTMENT APPROVAL CRITERIA

Salem Revised Code (SRC) 250.005(d)(2) sets forth the following criteria that must be met before approval can be granted to an application for a Class 2 Adjustment. The following subsections are organized with approval criteria shown in ***bold italic***, followed by findings evaluating the proposed development's conformance with the criteria. Lack of compliance with the following criteria is grounds for denial of the Class 2 Adjustment application, or for the issuance of certain conditions to ensure the criteria are met.

- (A) ***The purpose underlying the specific development standard proposed for adjustment is:***

- (i) ***Clearly inapplicable to the proposed development; or***
(ii) ***Equally or better met by the proposed development.***

Finding: The proposed adjustment included with the application seeks approval for multiple driveway approaches onto a local street where only one is allowed under SRC 804.030(a). The Class 2 adjustment is warranted because multiple accesses are needed given the limited area for internal parking circulation. The multiple driveway accesses provide equal or better access for the proposed development because vehicle turning movements have a negligible effect on local streets.

- (B) ***If located within a residential zone, the proposed development will not detract from the livability or appearance of the residential area.***

Finding: The proposed development is located within a CR (Commercial Retail) zone. There is a multi-family zoned property located within Marion County to the west of the subject property, but the majority of the properties abutting the property are commercially zoned within either the City or Marion County.

Because the subject property is not located within a residential zone and is located in an area characterized predominantly as commercial rather than residential, this approval criterion is not applicable to the proposed development.

- (C) ***If more than one adjustment has been requested, the cumulative effect of all the adjustments result in a project which is still consistent with the overall purpose of the zone.***

Finding: The proposed development includes only one adjustment to allow two driveway approaches to Harold Drive from each side of the proposed development. Pursuant to SRC 804.030(a), a lot or parcel is entitled to one driveway approach onto a local or collector street if the use of the lot or parcel is for other than Single Family or Two Family.

Because only one adjustment has been requested with the application, this approval criterion is not applicable to the proposed development. As indicated in comments from the Public Works Department, the proposed additional driveway approaches onto Harold Drive will have a negligible effect on Harold Drive and therefore will not result in a project that is inconsistent with the overall purpose of the CR (Commercial Retail) zone or SRC chapter 804 (Driveway Approaches).

CONCLUSION

Based on the facts and findings presented herein, staff concludes that the proposed Conditional Use, Class 3 Design Review, Class 3 Site Plan Review, Class 2 Driveway Approach Permit, and Class 2 Adjustment, as conditioned, satisfy the applicable criteria contained under SRC 240.005(d), SRC 225.005(e)(2), SRC 220.005(f)(3), SRC 804.025(d), and SRC 250.005(d)(2) for approval.

RECOMMENDATION

Staff recommends that the Planning Commission adopt the facts and findings of the staff report and take the following action for the subject property located at 3721 Lancaster Drive NE (Marion County Assessor Map and Tax Lot Number: 072W18BB06000):

- A. **APPROVE** the Conditional Use Permit to allow a multiple family development within the CR (Commercial Retail) zone on an approximate 3.68 acre portion of the subject property.

- B. **APPROVE** the Class 3 Design Review for the proposed 84 unit multiple family development, subject to the following conditions of approval:

Condition 1: The proposed design shall be revised to provide a children's play area within the eastern portion of the proposed development. The play area shall be located to the north of Building No. 4 in the area identified as open space. The play area shall be located outside the 10-foot-wide public utility easement (PUE) along Harold Drive NE.

Condition 2: A minimum 30-inch tall fence with latching gate shall be installed around the perimeter of the proposed outdoor children's play areas within the eastern and western portions of the subject property.

Condition 3: A minimum of 2 plant units, as set forth in SRC Chapter 807, Table 807-2, shall be provided adjacent to the primary entry way of each dwelling unit, or combination of dwelling units.

Condition 4: Shrubs shall be planted around the perimeter of the proposed buildings at a minimum density of 1 plant unit per 15 linear feet of exterior building wall.

Condition 5: Additional trees shall be planted around the perimeters of Buildings 2, 4, and 7 as follows:

- a) Building No. 2: One additional tree shall be planted adjacent to the southeast corner of the building.
- b) Building No. 4: One additional tree shall be planted adjacent to the southeast corner of the building and one additional tree shall be planted adjacent to the southwest corner of the building.
- c) Building No. 7: One additional tree shall be planted adjacent to the northeast corner of the building and one additional tree shall be planted adjacent to the southwest corner of the building.

Condition 6: The proposed buildings shall be revised to provide additional contrast and distinction between the ground floor and upper floor facades by incorporating one or more of the following:

- a) Vertically oriented lap siding, or horizontally oriented lap siding that is wider than that provided on the upper floor facades, that is painted a different color than the upper floor facades; or
- b) A siding material different from that used in the other portions of the building facades.

Condition 7: The facades of Buildings 1, 2, and 3 shall be revised to incorporate additional windows as follows:

- a) Building No. 1: Windows shall be provided on the western façade of the building within the bedrooms on each floor.
- b) Building No. 2: Windows shall be provided on the northern façade of the building within the bathrooms on the third floor.
- c) Building No. 3: Windows shall be provided on the eastern façade of the building within the bedrooms on each floor.

C. **APPROVE** the Class 3 Site Plan Review for the proposed 84-unit multiple family development, subject to the following conditions of approval:

Condition 1: The site plan shall be revised to remove the first six parking stalls to the north of Building No. 4 that are located off the southern driveway approach on the eastern side of Harold Drive. Where the parking spaces are removed, they shall be replaced with landscaping.

Condition 2: Prior to building permit approval, the final plat for Partition Case No. PAR15-06 shall be recorded.

Condition 3: Prior to building permit approval, a parking lease agreement shall be executed between the eastern and western portions of the proposed development. The agreement shall meet the requirements of SRC 806.020 and shall, at a minimum, allow for two spaces on the eastern lot to be utilized by the western lot to satisfy minimum off-street parking requirements.

Condition 4: A minimum of one loading space meeting the requirements of SRC Chapter

806 shall be provided within the development.

Condition 5: All trash/recycling areas shall conform to the solid waste service area standards of SRC 800.055.

Condition 6: Dedicate as public right-of-way the existing reserve strip located at the southerly terminus of Harold Drive NE.

Condition 7: Prior to issuance of certificate of occupancy for any apartment building within the proposed development, construct Harold Drive NE to local street standards with curb line sidewalks within the subject property and connecting to the southerly terminus of the existing Harold Drive improvements.

Condition 8: Design and construct stormwater facilities in accordance with SRC Chapter 71.

D. **APPROVE** the Class 2 Driveway Approach Permit to allow driveway access from the proposed development to the proposed extension of Harold Drive NE.

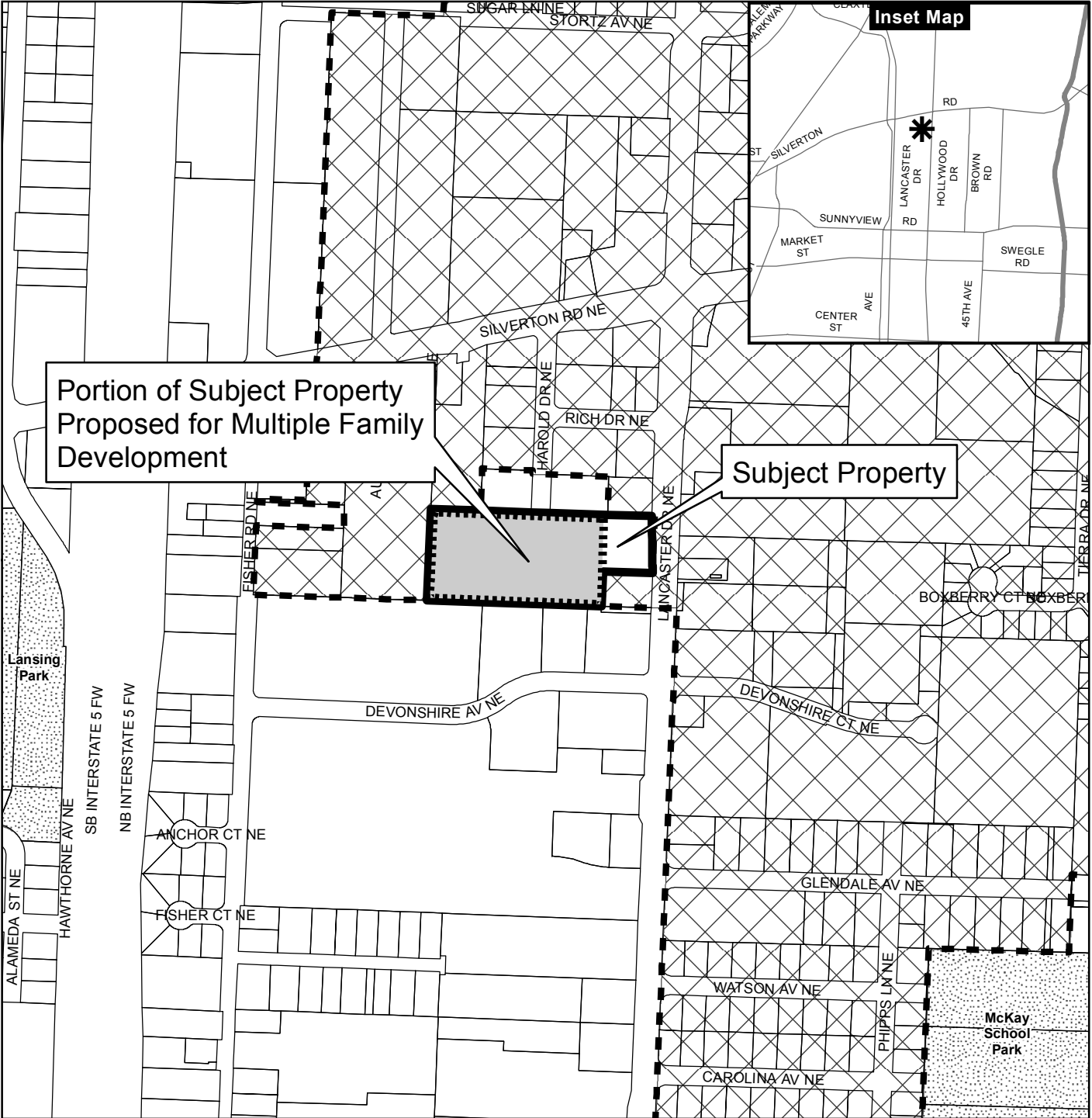
E. **APPROVE** the Class 2 Adjustment to increase the number of driveway approaches allowed to Harold Drive from the eastern and western portions of the proposed development from a maximum of one on each side, pursuant to SRC 804.030(a), to two on each side.

- Attachments:**
- A. Vicinity Map
 - B. Applicant's Proposed Site Development Plans and Building Elevations
 - C. Tentative Plan for Partition Case No. PAR15-06
 - D. Notice of Decision for Partition Case No. PAR15-06
 - E. Notice of Decision for Urban Growth Preliminary Declaration Case No. UGA15-01
 - F. Public Works Department Comments
 - G. Salem-Keizer School District Comments
 - H. Applicant's Written Statement Addressing Approval Criteria
 - I. Applicant's Traffic Impact Analysis

Prepared by Bryce Bishop, Planner II *B.B.*

Vicinity Map

3271 Lancaster Drive NE



Legend

- Taxlots
- Urban Growth Boundary
- City Limits
- Outside Salem City Limits
- Historic District
- Schools

Parks



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0 100 200 400 Feet

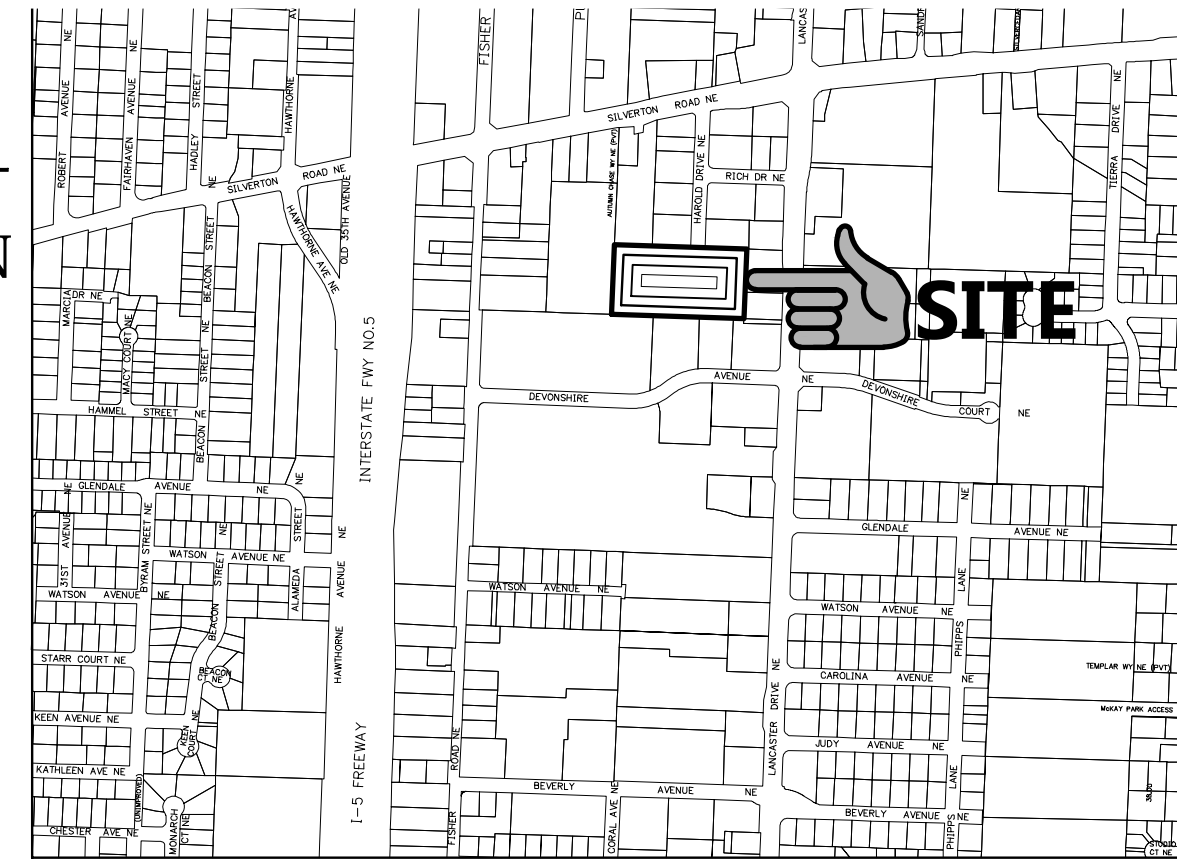


HAROLD DRIVE APARTMENTS

SEC. 18, T. 7 S., R. 2 W., W.M.
CITY OF SALEM
MARION COUNTY, OREGON
TAX LOT No. 6000 - 160,203 S.F.

Owner / Developer:
KILLEN DEVELOPMENT L.L.C.
3842 NW McINTOSH RD.
CAMAS, WASHINGTON 89607

Attachment B



VICINITY MAP

MULTI/TECH
ENGINEERING SERVICES, INC.
1155 19th ST. SE SALEM, OR 97303
PH: (503) 303 - 9227 FAX: (503) 364-1260
www.multitech.ws office@multitech.ws

COVER SHEET

HAROLD
DRIVE
APARTMENTS

ABBREVIATIONS

A.C.	ASPHALTIC CONCRETE	L.P.	LIGHT POLE
ACMP	ALUMINIZED CMP	M	METER, MAIN
ASSY.	ASSEMBLY	M.H.	MANHOLE
B.O.	BLOW OFF	MTL.	METAL
B.F.V.	BUTTERFLY VALVE	O.H.	OVERHEAD
C & G	CURB & GUTTER	P.C.	POINT OF CURVE
CATV	CABLE TELEVISION	PCC	POINT OF CONTINUING CURVE
C.B.	CATCH BASIN	PED.	PEDESTAL
C.B.C.O.	CATCH BASIN CLEANOUT	PRC	POINT OF REVERSE CURVE
C.B.I.	CATCH BASIN INLET	PROP.	PROPOSED
C.L.	CENTERLINE	PT	POINT OF TANGENCY
CMP	CORRUGATED METAL PIPE	PUB.	PUBLIC
C.O.	CLEANOUT	PUE	PUBLIC UTILITY EASMT.
CONC.	CONCRETE	PVC	POLYVINYL CHLORIDE
CONST.	CONSTRUCT	PVT.	PRIVATE
D.I.	DUCTILE IRON	P.P.	POWER POLE
DIA.	DIAMETER	P.L.	PROPERTY LINE
DWG.	DRAWING	R	RADIUS
EASMT.	EASEMENT	R	RIM
E.G.	EXIST. GRADE / GROUND	RD	ROOF DRAIN
EOP, E.P.	EDGE OF PAVEMENT	R.O.W.	RIGHT-OF-WAY
ELEC.	ELECTRIC	SAN S. or S.S.	SANITARY SEWER
ELEV. or EL.	ELEVATION	S	SLOPE
EX. or EXIST.	EXISTING	STA	STATION
FEET	FEET	STD	STANDARD
F.F.	FINISH FLOOR	STL	STEEL
F.G.	FINISH GRADE	STM.DRN. or S.D.	STORM DRAIN
F.H.	FIRE HYDRANT	SVK.	SERVICE
F.M.	FORCE MAIN	SW	SIDEWALK
GUT. or GTR.	GUTTER	T.C.	TOP OF CURB
G.V.	GATE VALVE	TEL.	TELEPHONE
IMP.	IMPROVEMENT	TYP.	TYPICAL
INST.	INSERT	U.G.	UNDERGROUND
INV. or I-	INVERT	ULT.	VAULT
L	LENGTH LINE	W.M.	WATER MAIN

SYMBOLS

EXIST. PROP.	EXIST. PROP.		
○	● BLOW OFF ASSY.	⊗	MANHOLE SAN. SEWER
⊠	■ CATCH BASIN	⊗	MANHOLE STORM DRAIN
⊠	■ CATCH BASIN CLEANOUT	⊗	2" DIA. C.O. / M.H.
⊠	■ CATCH BASIN INLET	⊗	MANHOLE TELEPHONE
⊠	■ CATV PED. / BOX	⊗	MANHOLE WATER
○	● CLEANOUT	⊗	REDUCER / INCREASER
○	● ELEC. PED. / BOX	⊗	TEL. PED. / BOX
○	● FIRE HYDRANT	⊗	TRAFFIC PED. / BOX
○	● GAS LOCATION MARKER	⊗	UTILITY / POWER POLE
○	● GAS VALVE	⊗	WATER METER
⊠	■ MAIL BOX	⊗	WATER VALVE
---	--- CABLE TELEVISION	---	SANITARY SEWER EXIST.
---	--- CENTERLINE	---	SANITARY SEWER PROP.
---	--- DITCH C.L.	---	STORM DRAIN EXIST.
---	--- ELECTRICAL LINE	---	STORM DRAIN PROP.
---	--- GAS MAIN	---	WATER MAIN EXIST.
---	--- TELEPHONE LINE	---	WATER MAIN PROP.

SHEET INDEX

SDR1	COVER SHEET
SDR2	EXISTING CONDITIONS PLAN
SDR3	SITE PLAN
SDR4	OPEN SPACE PLAN
SDR5	GRADING & DRAINAGE PLAN
SDR6	PRIVATE SANITARY SEWER PLAN
SDR7	PRIVATE DOMESTIC WATER PLAN
SDR8	PRIVATE FIRE MAIN PLAN

TYPE 'A' UNITS

1 BED, 1 BATH	- 752 S.F.
A1.1	LOWER FLOOR PLAN
A1.2	MIDDLE & UPPER FLOOR PLAN
A1.3	BUILDING ELEVATIONS

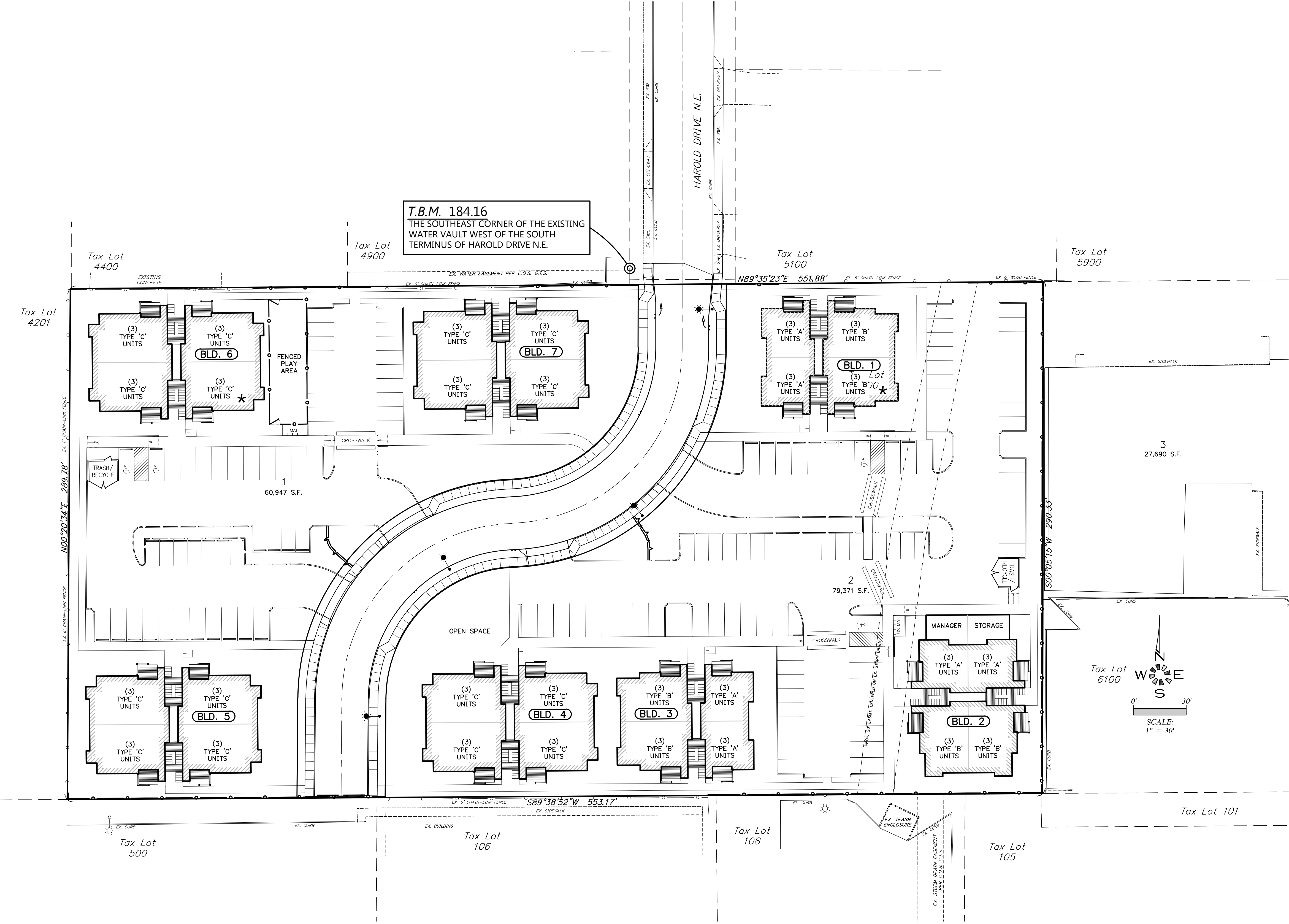
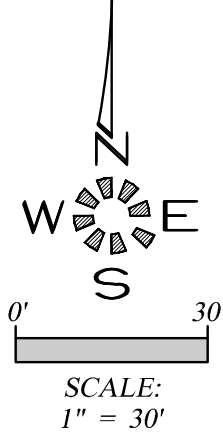
TYPE 'B' UNITS

2 BED, 2 BATH	- 1,060 S.F.
A2.1	LOWER FLOOR PLAN
A2.2	MIDDLE & UPPER FLOOR PLAN
A2.3	BUILDING ELEVATIONS

TYPE 'C' UNITS

3 BED, 2 BATH	- 1,192 S.F.
A3.1	LOWER FLOOR PLAN
A3.2	MIDDLE & UPPER FLOOR PLAN
A3.3	BUILDING ELEVATIONS

1. REVISED PER CITY OF SALEM REVIEW. P.H.S. 06/20/2016



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THESE DRAWINGS WITHOUT WRITTEN
AUTHORIZATION FROM THE DESIGN
ENGINEER.
DIMENSIONS & NOTES TAKE
PRECEDENCE OVER GRAPHICAL
REPRESENTATION.

Design:	M.D.C.	As-Built:
Drawn:	P.H.S.	
Checked:	M.D.C.	
Date:	MAY 2015	
Scale:	AS SHOWN	



EXPIRES: 06-30-2017

JOB # 8059

SDR1

HAROLD DRIVE APARTMENTS

SEC. 18, T. 7 S., R. 2 W., W.M.
CITY OF SALEM
MARION COUNTY, OREGON
TAX LOT No. 6000 - 160,203 S.F.

The premises are free of any 100/500 year return frequency flood hazard, as ZONE X: "Areas determined to be outside 500-year floodplain". Such flood free condition is shown on the Federal Flood Insurance Rate Map: Community panel No. 41047C0375G dated: January 19, 2000.

MULTI/TECH

ENGINEERING SERVICES, INC.
1155 13th ST. SE. SALEM, OR 97302
PH: (503) 363 - 9227 FAX: (503) 364-1260
www.multitech.ws office@multitech.ws

EXISTING
CONDITIONS
PLAN

HAROLD
DRIVE
APARTMENTS

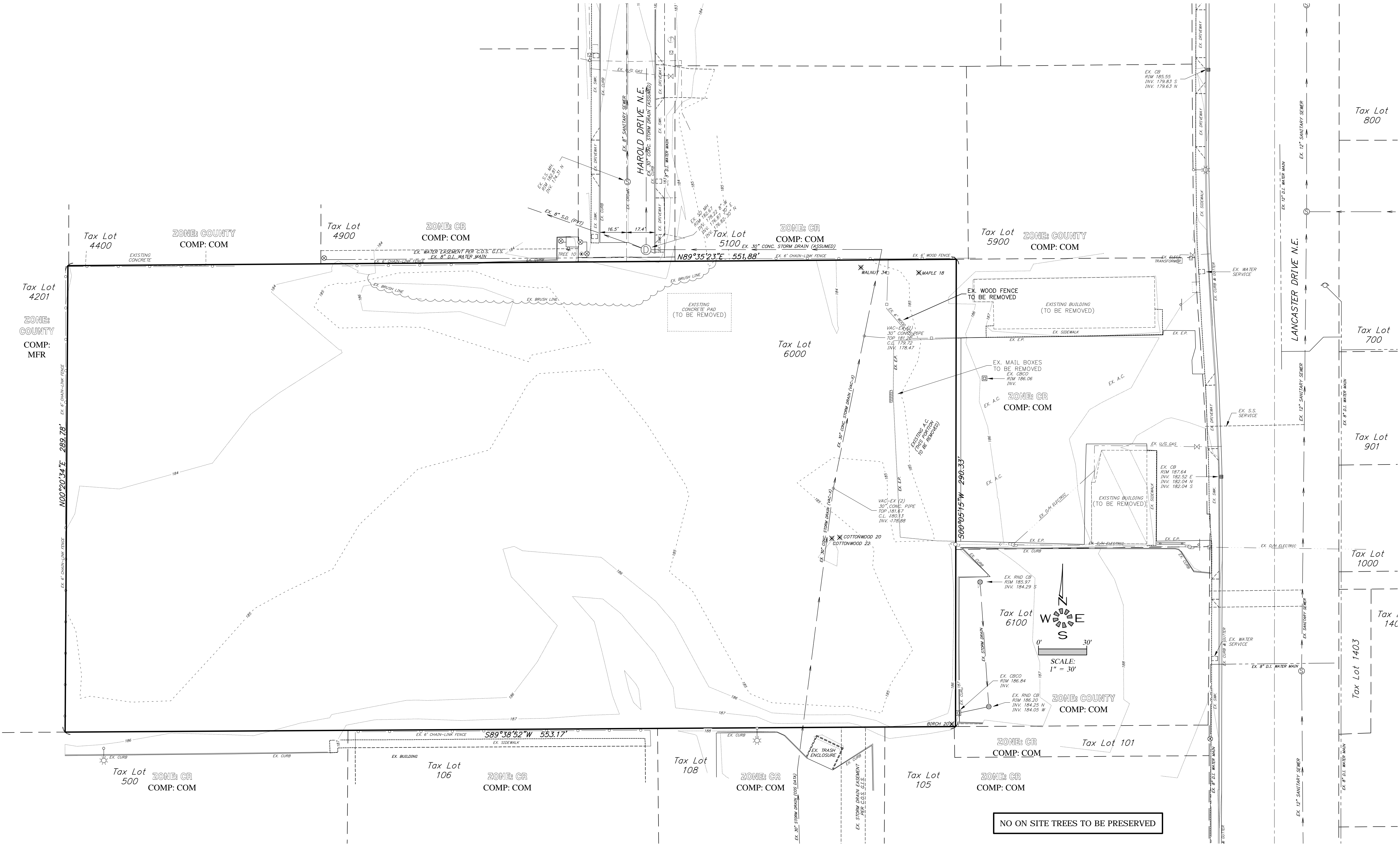
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DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

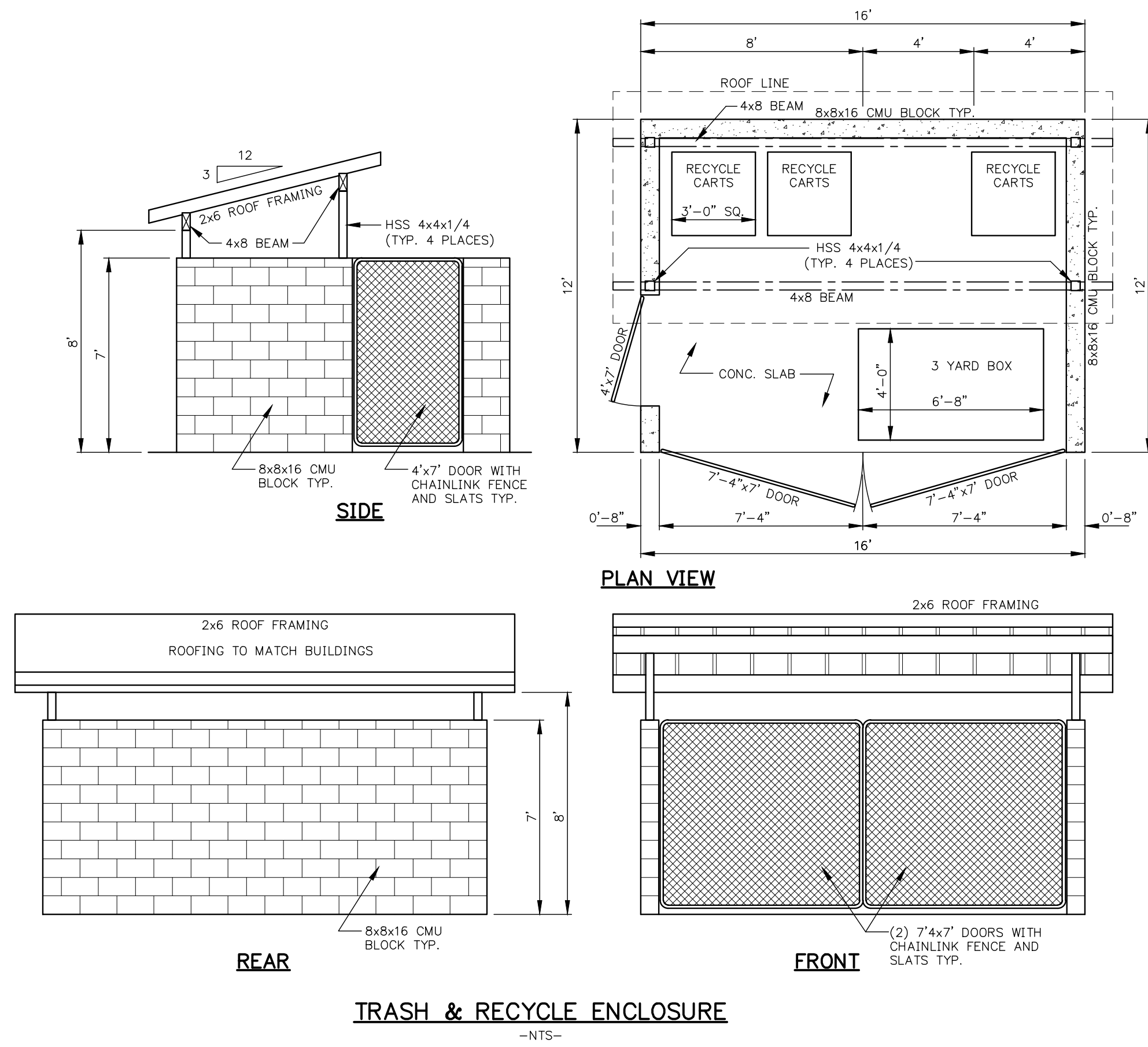
Design: M.D.G.
Drawn: P.H.S.
Checked: M.D.G.
Date: JAN. 2015
Scale: AS SHOWN
As Built:

REGISTERED PROFESSIONAL ENGINEER
986
JULY 14, 1978
PARK D. GREIN
EXPIRES: 06-30-2017

JOB # 6059

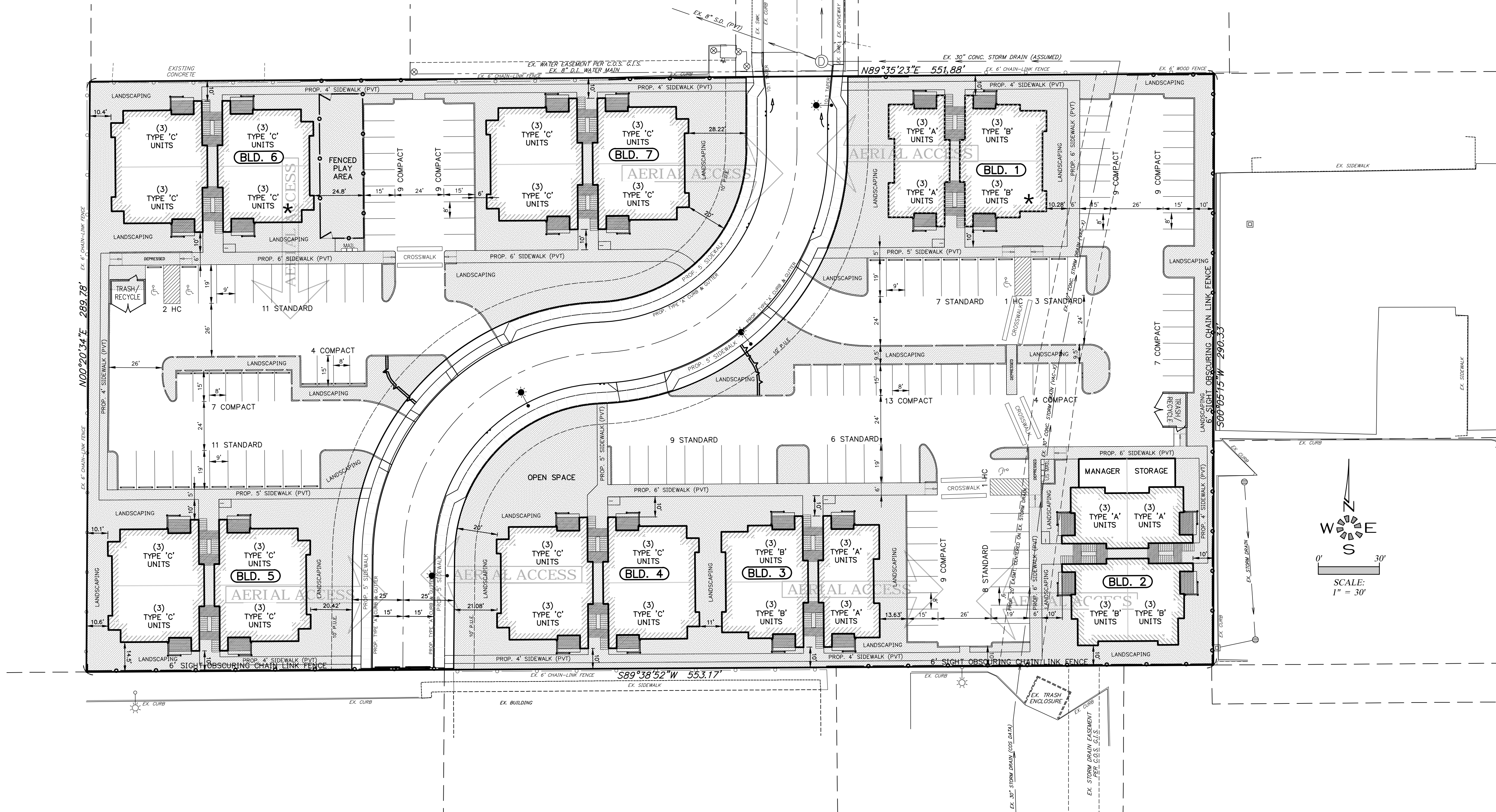
SDR2





TRASH & RECYCLE ENCLOSURE

-NTS-



SYMBOL LEGEND:

- COMMON OPEN SPACE
- 2 BICYCLE SPACES - MIN. 4' WIDE
- POLE LITE MAX. 14' IN HEIGHT
- POST LITE MAX. 5' IN HEIGHT
- MAXIMUM 1:12 SLOPE ON SIDEWALK RAMPS

★ THIS LOWER FLOOR UNIT IN BLD. #1 & BLD. #6 IS TO BE A TYPE A UNIT IN ACCORDANCE WITH THE 2014 OSSC SEC. 1107.6.2.1.1 (NOTED ON FLOOR PLANS). ALL OTHER LOWER FLOOR UNITS TO BE A TYPE B UNIT IN ACCORDANCE WITH THE 2010 OSSC SEC. 1107.6.2.1.2.

The premises are free of any 100/500 year return frequency flood hazard, as ZONE X: "Areas determined to be outside 500-year floodplain". Such flood free condition is shown on the Federal Flood Insurance Rate Map: Community panel No. 41047C0375G dated: January 19, 2000.

SUMMARY TABLE:

TOTAL SITE AREA	160,203 S.F.
R.O.W. DEDICATION	79,374 S.F.
NET AREA	140,268 S.F.
ZONE	CR
COMP PLAN	COM
TOTAL BUILDINGS	7
MANAGERS'S OFFICE	1
STORAGE/MAINTENANCE	1
TYPE 'A' 1-Bd, 1-Ba (752 S.F.) UNITS	18
TYPE 'B' 2-Bd, 2-Ba (1,060 S.F.) UNITS	18
TYPE 'C' 3-Bd, 2-Ba (1,192 S.F.) UNITS	48
TOTAL APARTMENT UNITS	84
GROSS FLOOR AREA	89,832 S.F.
MANAGERS'S OFFICE	1
BUILDING HEIGHT	41'-3 1/2"
BICYCLE PARKING	14
PARKING SPACES:	
STANDARD STALLS	54
COMPACT STALLS	80
HANDICAP STALLS	4
TOTAL PARKING STALLS	138
EAST LOT	79,374 S.F. (100%)
LANDSCAPE COVERAGE	12,226 S.F. (15.4%)
WEST LOT	60,894 S.F. (100%)
LANDSCAPE COVERAGE	20,091 S.F. (33.0%)
TOTAL SITE AREA	140,268 S.F. (100%)
LANDSCAPE COVERAGE	43,318 S.F. (30.9%)
EAST PARKING AREA	33,825 S.F. (100%)
LANDSCAPE COVERAGE	4,330 S.F. (12.8%)
WEST PARKING AREA	20,805 S.F. (100%)
LANDSCAPE COVERAGE	2,091 S.F. (10.1%)
TOTAL PARKING AREA	54,630 S.F. (100%)
LANDSCAPE COVERAGE	6,421 S.F. (11.8%)
(PLANTING OF STREET TREES WILL BE REQUIRED AT TIME OF DEVELOPMENT)	
COMMON OPEN SPACE:	
EAST: IN SETBACK:	14,242 S.F.
EAST: NOT IN SETBACK:	19,328 S.F.
TOTAL EAST:	33,570 S.F.
WEST: IN SETBACK:	13,641 S.F.
WEST: NOT IN SETBACK:	14,358 S.F.
TOTAL WEST:	27,999 S.F.
TOTAL IN SETBACK:	27,883 S.F.
TOTAL NOT IN SETBACK:	33,686 S.F.
TOTAL:	61,570 S.F.
PLAY AREA:	1,509 S.F.

SITE PLAN

HAROLD DRIVE APARTMENTS

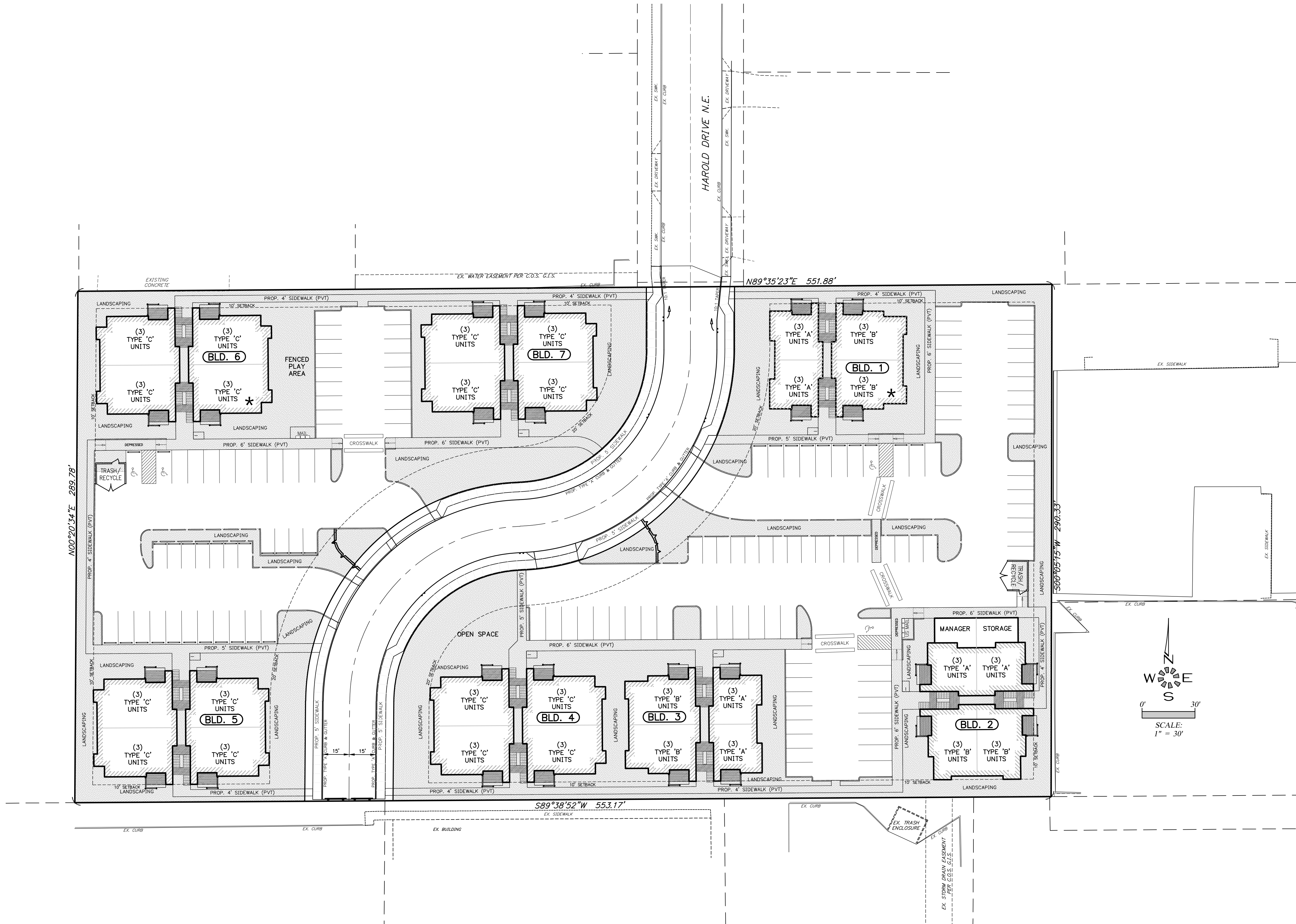
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DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

Design: M.D.G.
Drawn: P.H.S.
Checked: M.D.G.
Date: MAY 2015
Scale: AS SHOWN
As-Built: _____



JOB # 6059

SDR3



1. ADDED PER CITY OF SALEM REVIEW.	COMMON OPEN SPACE:		
	EAST:	IN SETBACK:	14,242 S.F.
	EAST:	NOT IN SETBACK:	19,328 S.F.
	TOTAL EAST:		33,570 S.F.
	WEST:	IN SETBACK:	13,641 S.F.
	WEST:	NOT IN SETBACK:	14,358 S.F.
	TOTAL WEST:		27,999 S.F.
	TOTAL IN SETBACK:		27,883 S.F.
	TOTAL NOT IN SETBACK:		33,686 S.F.
	TOTAL:		61,570 S.F.

OPEN SPACE PLAN

HAROLD DRIVE APARTMENTS

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Design: M.D.G.
Drawn: P.H.S.
Checked: M.D.G.
Date: JUNE 2016
Scale: AS SHOWN
As Built:

REGISTERED PROFESSIONAL ENGINEER
P.H.S.
JULY 14, 1971
EXPIRES: 06-30-2017
JOB # 6059

**SCHEMATIC
LANDSCAPE PLAN**

**HAROLD
DRIVE
APARTMENTS**

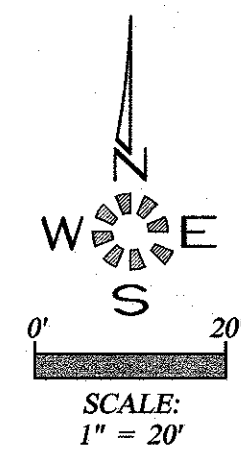
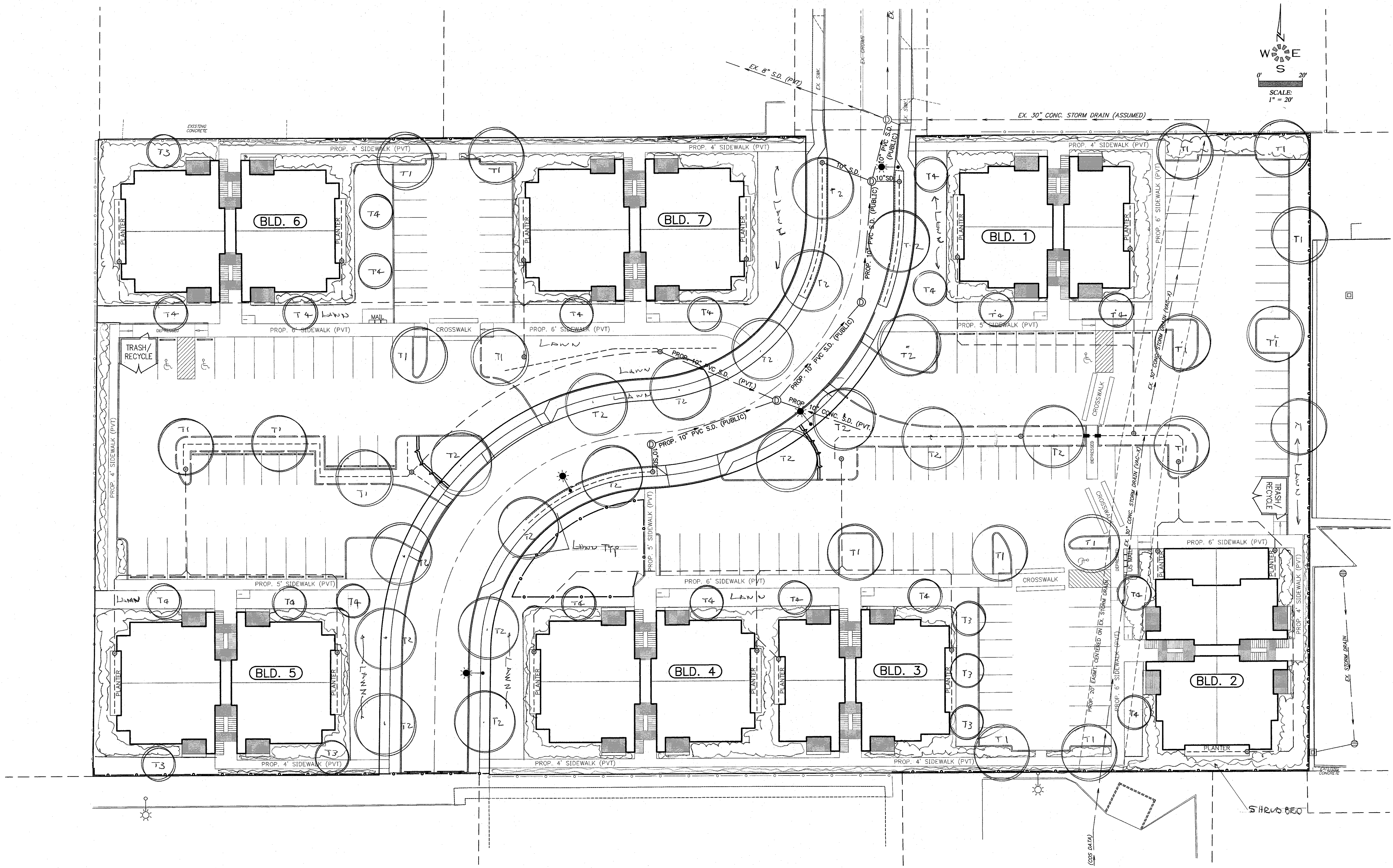
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AUTHORIZATION FROM THE DESIGN
ENGINEER.
DIMENSIONS & NOTES TAKE
PRECEDENCE OVER GRAPHICAL
REPRESENTATION.

Design: TAK
Drawn: TAK
Checked: MAY 2016
Date: AS SHOWN
Scale: AS SHOWN
As-Built:

JOB # 6059

L1.1

LANDSCAPE & IRRIGATION DESIGN BY:
**LANDSCAPE DESIGN
&
CONSULTANTS LLC**
Doing business since 1985
620 WORMWOOD ST. S.E. SALEM, OR 97306 PHONE: (503) 551-8599



LANDSCAPE NOTES:

General:

1. All local, municipal, state, and federal laws regarding uses, regulations, governing or relating to any portion of the work depicted on these plans are hereby incorporated into and made part of these specifications and their provision shall be carried out by the Contractor. The Contractor shall at all times protect the public throughout the construction process.
2. The Contractor shall carefully correlate construction activities with earthwork contractor and other site development.
3. The Contractor shall verify drawing dimensions with actual field conditions and inspect related work and adjacent surfaces. Contractor shall verify the accuracy of all finish grades within the work area. The Contractor shall report to the Landscape Design & Consultants LLC (LDC) or Owner all conditions which prevent proper execution of this work.
4. The exact location of all existing utilities structures and underground utilities, which may not be indicated on the drawings, shall be determined by the Contractor. The Contractor shall protect existing structures and utility services and is responsible for their replacement if damaged.
5. Disturbance and impacts to existing native trees/shrubs shall be minimized to the greatest extent practicable.
6. The Contractor shall keep the premises free from rubbish and debris at all times and shall arrange material storage to not interfere with the operation of the project. All unused material, rubbish, and debris shall be removed from the site.
7. All plant material and planting supplies shall be warranted for a period of not less than one year from the completion date of installation. All replacement stock shall be subjected to the same warranty requirement as the original stock. Any damage due to replacement operations shall be repaired by the Contractor. At the end of the warranty period, inspections shall be made by LDC, Owner/General Contractor. All plant and lawn areas not in a healthy growing condition shall be removed and replaced with plants and turf cover of a like kind and size before the close of the next planting season.

Grading / Erosion Control:

1. The design and placement of the building on the site lends itself to minimal slope conditions with positive drainage being maintained around the entire building. In this case standard landscaping procedures of topsoil, lawn, and a two inch layer of bark mulch on all planting beds will be sufficient to control erosion. In the event site conditions change or there are Slopes / Bio Swales / Detention Ponds on the project with slopes greater than 30% Poly tight Jute Netting shall be installed with anchoring pins as per manufactures recommendations prior to planting. Recommend DeWitt PJN4216 Erosion Control Poly Jute Netting and DeWitt anchor pins or approved equal.
2. Seed recommendation is Pro Time 700 Low Profile or approved equal over the jute netting at a rate of 2 lbs. per 1000 sq. feet. The address of Pro Time is 1712 SE Ankeny, Portland OR 97214. Phone 503-239-7518. There email is info@ordtime.lawnseed.com
3. The work limits shown on this plan shall clearly be marked in the field prior to construction. No disturbance beyond the work limits shall be permitted.
4. Grading shall be performed during optimal weather conditions.
5. Erosion control measures shall be constructed in conjunction with all clearing and grading activities, and in such a manner as to ensure that sediment and sediment-laden water does not enter the drainage system or violate applicable water standards
6. Prior to the commencement of construction activities, Contractor shall place orange construction fencing around perimeters of construction impact areas, and sediment fencing at downhill portions of the site. Contractor is responsible for proper installation, maintenance, replacement, and upgrading of all erosion and sediment control measures, in accordance with local, state, and federal regulations.

Plant Material:

1. Contractor shall verify all plant & tree quantities with LDC or Owner prior to construction.
2. In the event of a discrepancy between plants materials listed on the drawings, the drawings shall govern the plant species and quantities required.
3. Plant material shall be first quality stock and shall conform to the code of standards set forth in the current edition of the American Standards for Nursery Stock sponsored by the American Association of Nurserymen, Inc. (AAN)
4. Species and variety as specified on the drawings and delivered to the site shall be certified true to there genus, species and variety and as defined within the current edition International Code of Nomenclature for Cultivated Plants.
5. Obtain freshly dug, healthy, vigorous plants nursery-grown under climatic conditions similar to those in the locality for the project for a minimum of two years. Plants shall have been lined out in rows, annually cultivated, sprayed, pruned, and fertilized in accordance with good horticultural practice. All container plants shall have been transplanted or root pruned at least once in the past three years. Balled-and-burlapped (B&B) plants must come from soil which will hold a firm root ball. Heeled in plants and plants from cold storage are not acceptable.
6. Planting stock shall be well-branched and well-formed, sound, vigorous, healthy, free from disease, sun-scaled, windburn, abrasion, and harmful insects or insect eggs; and shall have healthy, normal, unbroken root systems. Deciduous trees and shrubs shall be symmetrically developed, uniform habit of growth, with straight trunks or stems, and free from objectionable disfigurements. Evergreen trees and shrubs shall have well-developed symmetrical tops with typical spread of branches for each particular species or variety. Only vines and ground cover plants well-established shall be used. Plants budding into leaf or having soft growth shall be sprayed with an anti-desiccant at the nursery before digging.
7. Contractor shall not make substitutions of plant materials. If required landscape material is not obtainable, submit proof of non-availability and proposal for use of equivalent material. When authorized, adjustments of contract amount (if any) will be made by change order.
8. Plant sizes and grading shall conform to the latest edition of American Standard for Nursery Stock as sponsored by the American Association of Nurserymen Inc. (AAN)
9. All vegetation shown on this plan shall be maintained in a healthy and vigorous growing condition throughout the duration of the proposed use. All vegetation not so maintained shall be replaced wit new vegetation at the beginning of the next growing season.

Planting:

1. Planting shall be installed between February 1 and May 1 or between October 1 and November 15. If planting is installed outside these times frames, additional measures maybe needed to ensure survival and shall be pre-approved by the owner.
2. Plant material shall be transported to the sit in a timely manner to minimize on-site storage. Where storage is required, all plants shall be kept moist and shaded.
3. Plant stock shall be handled in a manner that will not break, scrape, or twist any portion of the plant. Protect plants at all times from conditions that can damage the plant (e.g., sun, wind, freezing conditions).
4. Provide the following clearance for planting of trees where applicable:

Maintain 30 feet vision triangles at all intersections and corners
5 feet from all street/parking lot light standards
10 feet from fire hydrants
5 feet from all utility vaults, meter boxes, etc.

5. No trees or shrubs shall be planted on existing or proposed utility lines.
6. All shrub beds shall receive a minimum 2" layer of bark mulch evenly applied immediately after panting is completed. All plant beds shall drain away from buildings.
7. Excavate plant pits for shrubs and trees as follows:
Container stock: width = 2 times the container diameter, depth = container depth.
Bare root stock: width = 2 times the widest diameter of the root, depth = of root system.
B & B: width = 2 times ball diameter, depth = ball depth.
Scarify sides and bottom of plant pits to roughen surfaces.
8. Place plants plumb in the pit. Backfill with native soil or top soil mixture to the original plant soil line, and tap solidly around the ball and roots. Water plants immediately after planting if soil is not saturated to the surface.

Lawn Areas/Hydro-Seeding/Sod:

1. All lawn area shall be seeded unless otherwise directed by Owner to install Sod. Seed or Sod shall be procured through Kuenzi Turf Nursery. Seed or Sod shall be Rhizomatous Tall Fescue (RTF); Seed mix shall be applied at a rate of 10 lbs. per 1000 sq. feet. Contact Kuenzi Turf Nursery at (503)586-6537 or approved equal.
2. All areas shown on the plan shall be seeded between March 1st, and May 1st, or between September 1st and October 15th.
3. Scarify soil surface to a depth of 3 inches, to ensure adequate seed contact with soil.
4. Prior to seeding, clearly delineate seeding limits using flags or non-toxic paint.
5. Hydro-seed application rate shall be 60 lbs. per 100 gallons of hose work or 75 lbs. per 100 gallons of tower work
6. If, at the end of one complete growing season, the planted and seeded area fail to exhibit well-established plant communities, or exhibit patchiness in the patterns of vegetative cover, supplemental seeding and planting shall occur.
7. Seeding of slopes greater than 30% shall be done by Hydro Seeding using a seed mix of Pro Time 700 Low Profile or approved equal at a rate of 2 lbs. per 1000 sq. feet.

Top Soil Mixtures:

1. A 2" to 4" layer of garden care compost, mushroom compost or similar material sterilized at 105 degrees Fahrenheit shall be incorporated into the existing soil prior to planting and seedings/sod lawns.
Incorporate into existing soil prior to planting the following fertilizers at a rate specified per 1000 sq. ft. of planting area.
20 lbs. 10-6-4 50% Slow Release
30 lbs. 38-0-0 Nitroform
10 lbs. Iron Sulfate 21%
20 lbs. 0-18-0 Super Phosphate
25 lbs. Dolomite Lime
10 lbs. 13-0-11 Potassium Nitrate

Bark Mulch:

1. All shrub beds shall receive a minimum 2" layer of fine hemlock or fir bark mulch evenly applied immediately after panting is completed. All plant beds shall drain away from buildings.

Poly Jute Netting:

1. Tight net Poly Jute Netting shall be installed on Bio Swales/Detention Ponds/Vegetated Swale and Rain Gardens as a soil stabilizer and erosion control agent. Jute Netting shall be installed with anchoring pins as per manufactures recommendations prior to planting. Recommend DeWitt PJN4216 Erosion Control Poly Jute Netting and DeWitt anchor pins or approved equal.

Rain Gardens Bark Mulch:

1. After Poly Jute Netting and Plants are installed a 2" layer of bark mulch shall be installed in rain gardens.

Bio Swales/Detention Ponds/Vegetated Swales:

1. Bark Mulch shall not be applied to Bio Swales/Detention Ponds or Vegetated Swales.

Weed Control Agent:

1. Apply caseeron as a weed control agent after planting as per manufactures specified recommendations around building or approved equal.

Non-Native Plant Species:

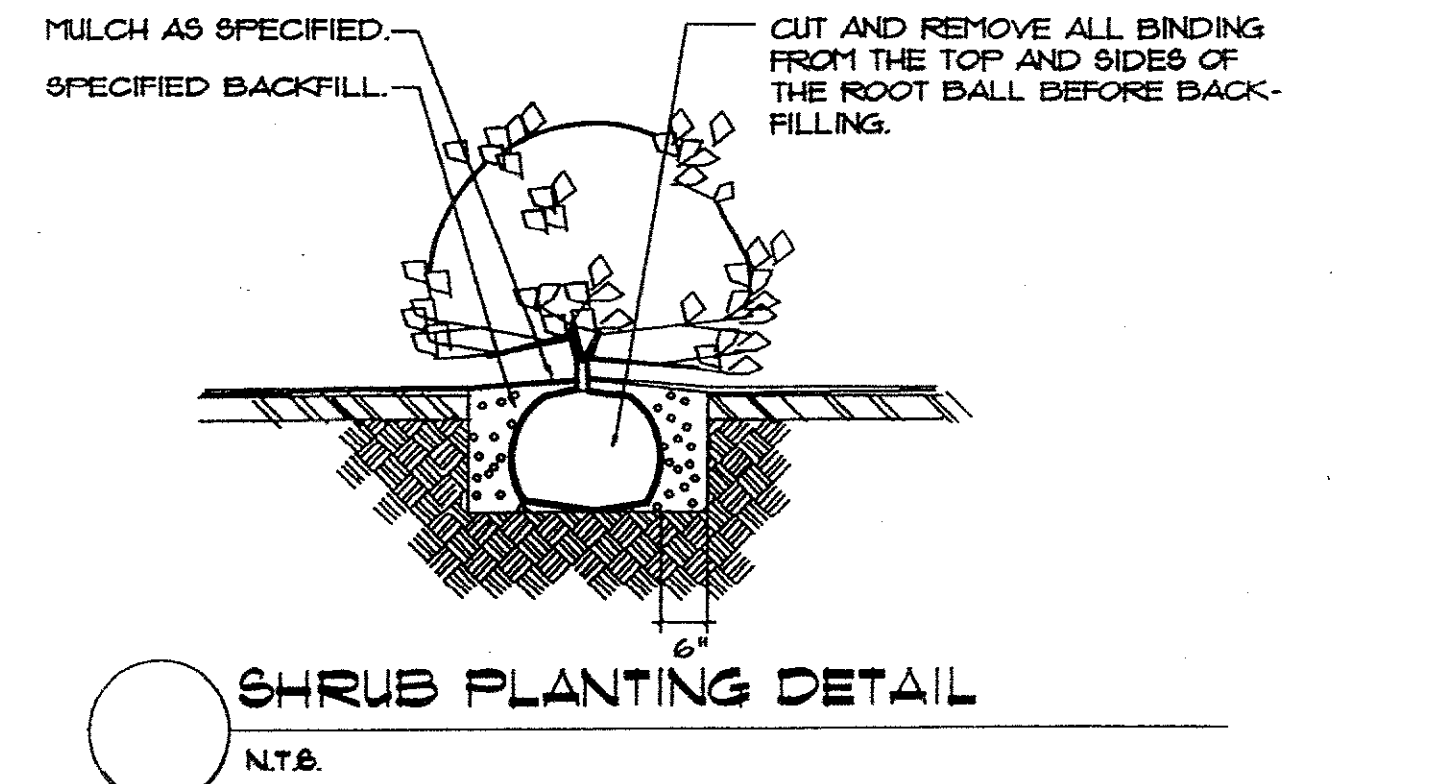
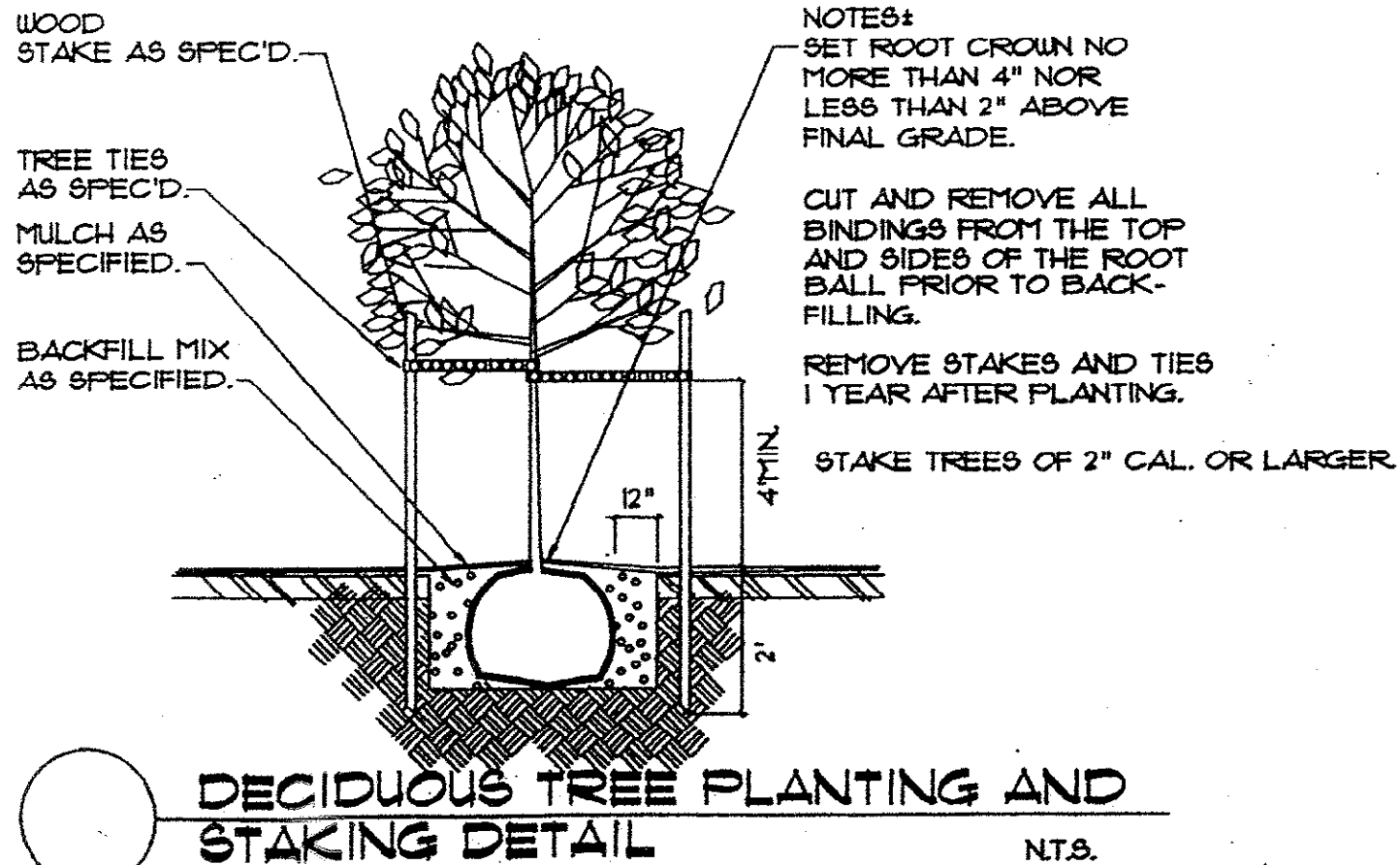
1. All non-native, invasive plant species shall be removed from the site.
2. Growing Medium for Stormwater Bio Swales/Vegetated Swales/Rain Gardens/Planter Boxes:
Furnish imported growing medium for vegetated stormwater facilities conforming to the following:
1. Standard Blend for Public and Private Facilities: Use this blend for all vegetated stormwater management facilities.
3. General Composition: The medium shall be a blend of loamy soil, sand, and compost that is 30 to 40 percent compost (by volume) and sterilized to 105 degrees Fahrenheit.
4. Analysis Requirements for the Blended Material:
a. Particle Gradation: A particle gradation analysis of the blended material, including compost, shall be conducted in conformance with ASTM C1 17/C136 (AASHTO T1 1/127). The analysis shall include the following sieve sizes: 1 inch, 3/8 inch, #4, #10, #20, #40, #60, #100, and #200. The gradation of the blend shall meet the following gradation criteria:

Sieve Size	Percent Passing
1 inch	100
# 4	60-100
# 10	40-100
# 40	15-50
# 100	5-25
# 200	3-5

b. The blend shall have a Coefficient of Uniformity (D60/D10) equal to or greater than 6 to ensure that it is well graded (has a broad range of particle sizes). The coefficient is the ratio of two particle diameters on a grain-size distribution curve; it is the particle diameter at 60 percent passing divided by the particle diameter at 10 percent passing.

- a. Organic Matter Content: An analysis of soil organic matter content shall be conducted in conformance with ASTM D2974 (loss on ignition test). The soil organic matter content shall be a minimum of 10 percent, as reported by that test.
 - d. pH: The blended material shall be tested and have a pH of 5.5 to 7.
 - e. Depth of growing medium shall be 18 inches. Growing medium shall be placed in 4 inch lifts and lightly compacted.
 - f. Once planting is completed Bark Mulch shall not be applied.
5. General Requirements for the Blended Material:
- a. The material shall be loose and friable.
 - b. It shall be well mixed and homogenous.
 - c. It shall be free of wood pieces, plastic, screened and free of stones 1 inch (25 mm) or larger in any dimension; free of roots, plants, sod, clods, clay lumps, pockets of coarse sand, paint, paint washout, concrete slurry, concrete layers or chunks, cement, plaster, building debris, oils, gasoline, diesel fuel, paint thinner, turpentine, tar, roofing compound, acid, and other extraneous materials harmful to plant growth; and free of weeds and invasive plants including but not limited to:
 1. *Cirsium arvense* (Canadian Thistle)
 2. *Convolvulus* spp. (Morning Glory)
 3. *Cytisus scoparius* (Scotch Broom)
 4. *Dipsacus sylvestris* (Common Teasel)
 5. *Festuca arundinacea* (Tall Fescue)
 6. *Hedera helix* (English Ivy)
 7. *Holcus canatus* (Velvet Grass)
 8. *Lolium* spp. (Rye Grasses)
 9. *Lolus conciliatus* (Bird's Foot Trefoil)
 10. *Lythrum salicaria* (Purple Loose Strife)
 11. *Meillolus* spp. (Sweet Clover)
 12. *Myriophyllum spicatum* (Eurasian Milfoil)
 13. *Phalaris arundinacea* (Reed Canary Grass)
 14. *Rubus discolor* (Himalayan Blackberry)
 15. *Solanum* spp. (Nightshade)
 16. *Trifolium* spp. (Clovers)
 17. Not infested with nematodes, grubs, other pests, pest eggs, or other undesirable organisms and disease-causing plant pathogens; and with sufficient structure to give good tilth and aeration, continuous, air-filled, pore-space content on a volume/volume basis shall be at least 15 percent when moisture is present at field capacity, and soil shall have a field capacity of at least 15 percent on a dry weight basis.

6. Compost: The compost shall be derived from plant material and provided by a member of the U.S. Composting Council Seal of Testing Assurance (STA) program. See www.compostingcouncil.org for a list of providers in Portland and sounding area.
 7. The compost shall be the result of the biological degradation and transformation of plant-derived materials under conditions designed to promote aerobic decomposition. The material shall be well composted, free of viable weed seeds, and stable with regard to oxygen consumption and carbon dioxide generation. The compost shall have no visible free water and produce no dust when handled. It shall meet the following criteria, as reported by the U.S. Composting Council STA Compost Technical Data Sheet provided by the vendor:
 - a. 100 percent of the material must pass through a 1/2-inch screen. The pH of the material shall be between 6 and 8.
 - b. Manufactured inert material (plastic, concrete, ceramics, metal, etc.) shall be less than 1.0 percent by weight. The organic matter content shall be between 35 and 65 percent.
 - c. The soluble salt content shall be less than 6.0 mmhos/cm. Germination (an indicator of maturity) shall be greater than 80 percent.
 - d. The stability shall be between classes 5-7.
 - e. The carbon/nitrogen ratio shall be less than 25:1.
 - f. The trace metals test result = "pass".
 9. The compost shall be delivered to the site in a timely manner to minimize on-site storage. Where storage is required, all plants shall be kept moist and shaded.
 10. Submittals: At least 14 working days in advance of construction, submit the following:
 - a. Two 5-gallon buckets of the blended material.
 - b. Documentation for the three analyses described in Section (4) of this specification (particle gradation with calculated coefficient of uniformity; organic matter content; pH). The analyses shall be performed by an accredited laboratory with certification maintained current. The date of the analyses shall be no more than 90 calendar days prior to the date of the submittal.
- The report shall include the following information:
Name and address of the laboratory
Phone contact and e-mail address for the laboratory
Test data, including the date and name of the test procedure



PLANT PALETTE

		APARTMENTS				
QTY.	SYM	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	REMARKS
TREES						
19	T-1	Acer rubrum 'Armstrong'	Armstrong Red Maple	1 1/2"-2" cal.	B & B	6' Standard
20	T-2	Acer rubrum 'Autumn Blaze'	Autumn Blaze Maple	1 1/2"-2" cal.	B & B	6' Standard
6	T-3	Prunus serrulata Amanagawa	Amanagawa Flowering Cherry	1 1/2"-2" cal.	B & B	6' Standard
17	T-4	Pyrus calleryana 'Capital'	Capital Flowering Pear	1 1/2"-2" cal.	B & B	6' Standard
SHRUBS						
	S-1	Abelia grandiflora 'Ed Goucher'	Edward Goucher Abelia	2 gal.	cont.	Full
	S-2	Berberis thunbergii 'Crimson Pygmy'	Crimson Pygmy Japanese Barberry	2 gal.	cont.	Full
	S-3	Calamagrostis x acutiflora 'Avalanche'	Avalanche Feather Reed Grass	2 gal.	cont.	Full
	S-4	Cornus stolonifera 'Kelsey'	Dwarf Redwig Dogwood	2 gal.	cont.	Full
	S-11	Cornus sericea Arctic Fire 'Farrow'	Arctic Fire Redwig Dogwood	2 gal.	cont.	Full
	S-6	Daphne ordora	Winter Daphne	2 gal.	cont.	Full
	S-7	Festuca glauca Elijah Blue	Elijah Blue Festuca Grass	2 gal.	cont.	Full
	S-8	Euonymus alatus 'Compacta'	Dwarf Burning Bush	2 gal.	cont.	Full
	S-9	Mahonia aquifolium 'Compacta'	Compact Oregon Grape	2 gal.	cont.	Full
	S-10	Pennisetum alopecuroides Hameln	Dwarf Fountain Grass	2 gal.	cont.	Full
	S-11	Rubus discolor (Himalayan Blackberry)	Mt. Fire Japanese Andromeda	5 gal.	cont.	Full
	S-12	Rosa 'prostrata red'	Red Prostrata Rose	2 gal.	cont.	Full
	S-13	Rhododendron 'Yaku Princess'	Yaku Princess Rhododendron	18"-24"	w/buds	Full
	S-14	Rhododendron 'Daphnoides'	Daphnoides Rhododendron	18"-24"	w/buds	Full
	S-15	Rhododendron 'PJM'	PJM Rhododendron	18"-24"	w/buds	Full
	S-16	Prunus laurocerasus 'Otto Lukens'	Otto Lukens Laurel	2 gal.	cont.	Full
	S-17	Spiraea japonica 'Gold Flame'	Gold Flame Spiraea	2 gal.	cont.	Full
	S-18	Spiraea japonica 'Magic Carpet'	Magic Carpet Spiraea	2 gal.	cont.	Full
	S-19	Ilex crenata 'Convexa'	Convexleaf Japanese Holly	1 gal.	cont.	Full
	S-20	Viburnum tinus 'Spring Bouquet'	Laurestinus 'Spring Bouquet'	5 gal.	cont.	Full

CONTRACTOR TO VERIFY ALL QUANTITIES OF PLANT MATERIALS WITH LANDSCAPE DESIGN & CONSULTANTS PRIOR TO INSTALLATION
PLANT MATERIAL SUBSTITUTIONS MAY BE MADE BY THE OWNER FOR PLANT MATERIALS OF SIMILAR HABIT, FLOWERING CHARACTERISTIC AND/OR STRUCTURE OF GROWTH DUE TO AVAILABILITY

PLANT PALETTE

COMMON BIO SWALE

		SHRUBS				
QTY.	SYM	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	REMARKS
Bottom						
	W-1	Deschampsia cespitosa	Tufted Hairgrass	Plugs.	Planted 1' o/c	Full
	W-2	Juncus tenuis	Slender Rush	Plugs.	Planted 1' o/c	Full
	W-3	Carex densa	Dense Sedge	4" pots	Planted 1' o/c	Full
Side Slopes						
	W-4	Juncus patens	Spreading Rush	1 gal.	Planted 2' o/c	Full
	W-5	Carex obnupta	Slough sedge	1 gal.	Planted 2' o/c	Full
	W-6	Juncus effusus var. pacificus	Soft Rush	1 gal.	Planted 2' o/c	Full
Top of Swale						
	W-7	Cornus stolonifera 'Kelsey'	Dwarf Redwig Dogwood	2 gal.	Planted 3'-o/c	Full

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SEE LANDSCAPE NOTES BEFORE PLANTING THE BIO-SWALE:

Tight Poly Jute Netting shall be installed on the slopes and bottom of the Bio Swale as a soil stabilizer and erosion control agent. Use ground covering anchoring pins for installation as per manufactures directions. Do not install plant material until the Poly Jute Netting is installed. Holes are to be cut within the Poly Jute Netting for plant material installation. Once plants are installed bark mulch or any other cover material shall not be applied on the bottom or sides of the Bio Swale. For the swale bottom group 6 to 8 species of each plant together and very the plantings species. Do the same on the side slopes.

PLANT PALETTE

STORMWATER PLANTERS

		SHRUBS				
QTY.	SYM	BOTANICAL NAME	COMMON NAME	SIZE	CONDITION	REMARKS
Bottom						
	P-1	Juncus tenuis	Slender Rush	1 gal.	Planted 12" o/c	Full
	P-2	Carex testacea	Orange sedge	1 gal.	Planted 12" o/c	Full
	P-3	Juncus patens	Spreading Rush	1 gal.	Planted 12" o/c	Full
	P-4	Juncus effusus var. pacificus	Soft Rush	1 gal.	Planted 12" o/c	Full

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SEE LANDSCAPE NOTES BEFORE PLANTING THE STORMWATER PLANTERS:

LANDSCAPE & IRRIGATION DESIGN BY:
LANDSCAPE DESIGN
&
CONSULTANTS LLC
Doing business since 1985
620 WORMWOOD ST. S.E. SALEM, OR 97306 PHONE: (503) 531-9590

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Checked: TAK
Date: MAY 2016
Scale: AS SHOWN
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JOB # 6059

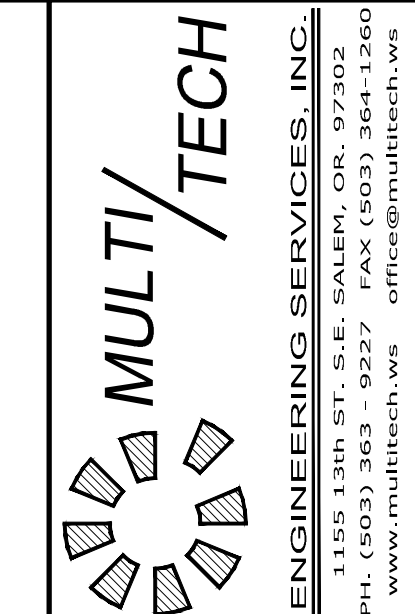
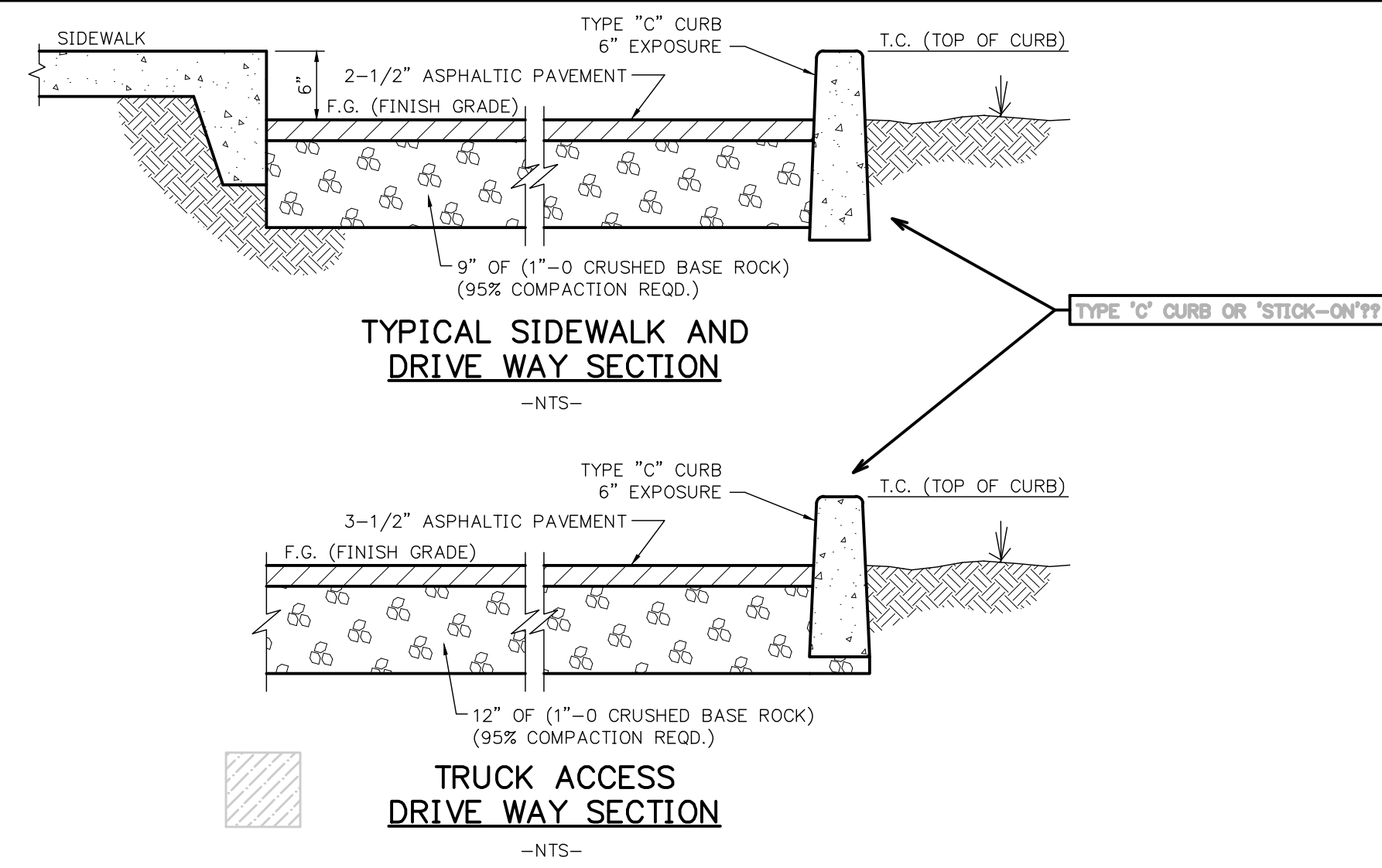
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MULTI/TECH
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11503 SW 37th St. Suite 200
Portland, OR 97225
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www.multitech-wr.com
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SCHEMATIC
LANDSCAPE PLAN

HAROLD
DRIVE
APARTMENTS

6059.dwg/PL011 L1.2



PRIVATE GRADING & DRAINAGE PLAN

HAROLD
DRIVE
APARTMENTS

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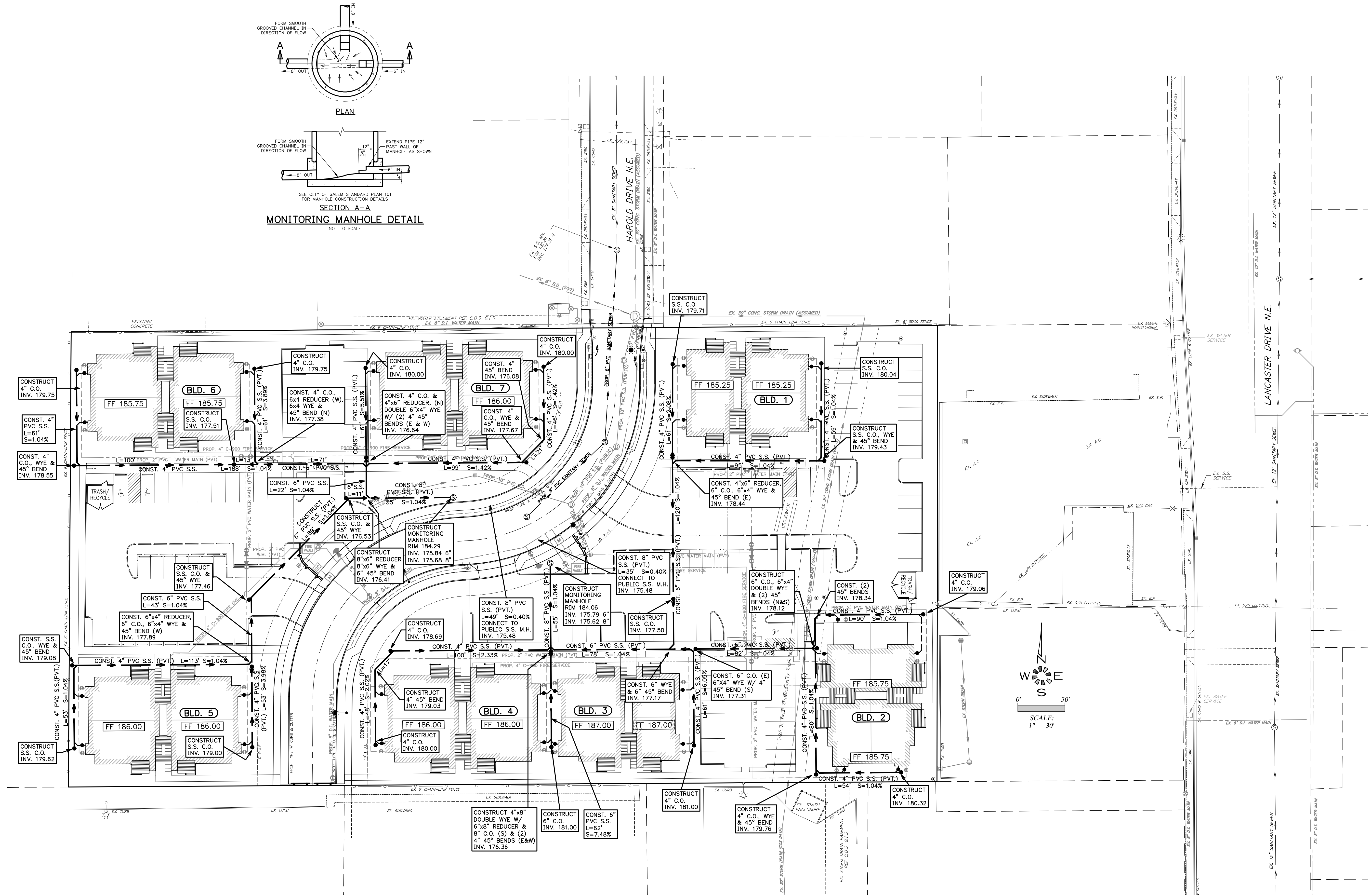
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Drawn:	P.H.S.
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Date:	JAN. 2015
Scale:	AS SHOWN



EXPIRES: 06-30-2017

SDR5



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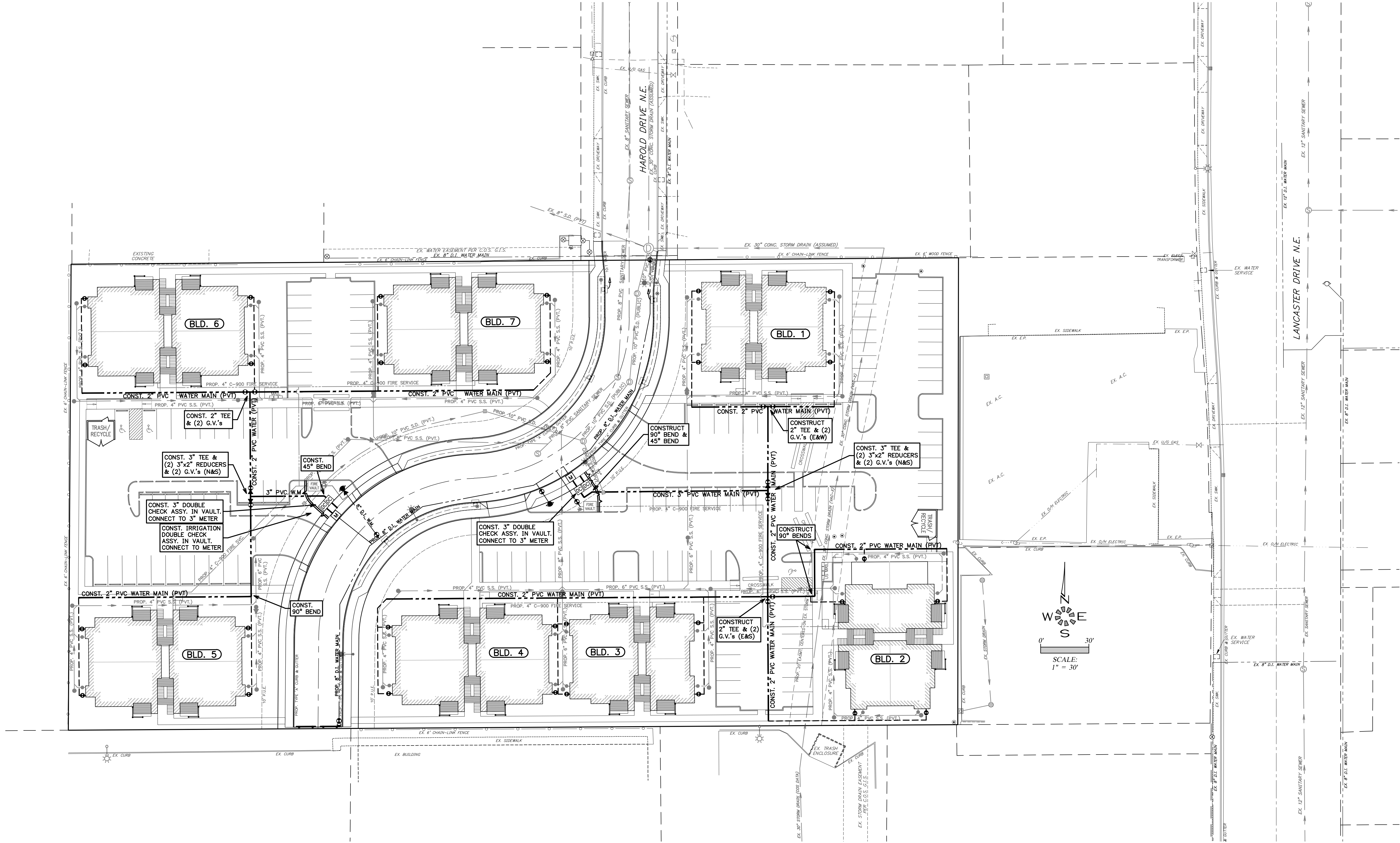
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CHECKED: M.D.G.
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AS-BUILT: _____
REGISTERED PROFESSIONAL ENGINEER
JULY 1, 1978
MARK D. GREGG
EXPIRES: 06-30-2017
JOB # 6059

SDR6

PRIVATE
SANITARY SEWER
PLAN

HAROLD
DRIVE
APARTMENTS

MULTI/TECH
ENGINEERING SERVICES, INC.
1155 13TH ST. S.E. SALEM, OR 97302
PH. (503) 363-9227 FAX (503) 364-1260
www.multitech.ws office@multitech.ws



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P.H.S. 06/20/2016
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Date: JAN. 2015
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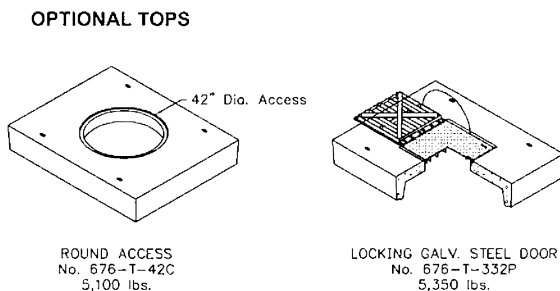
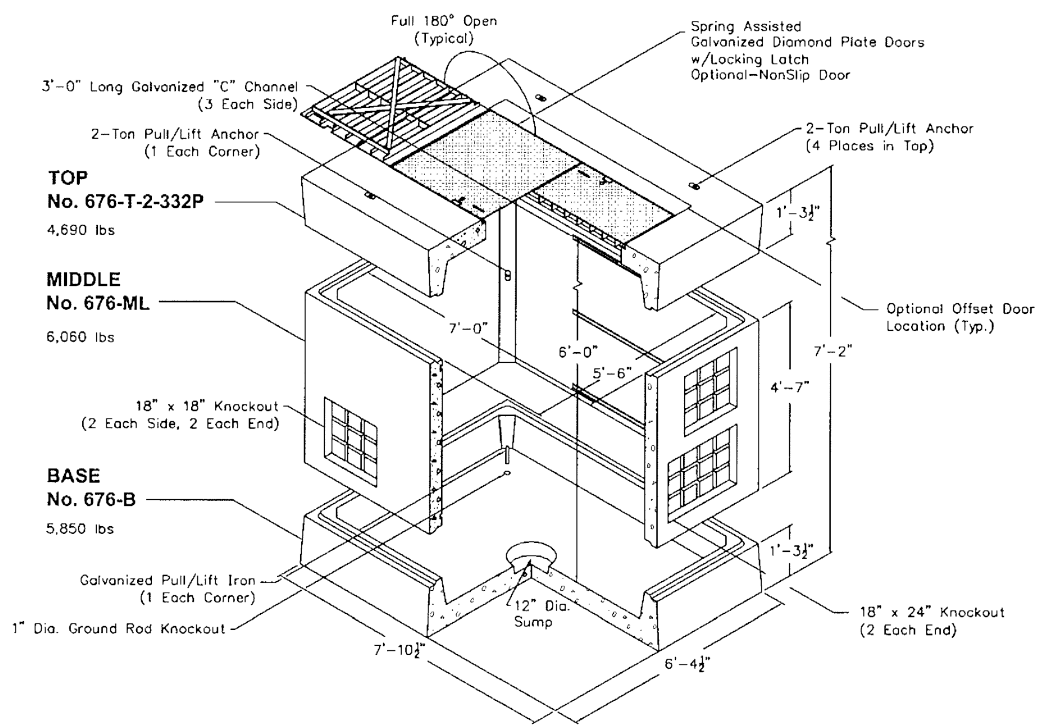
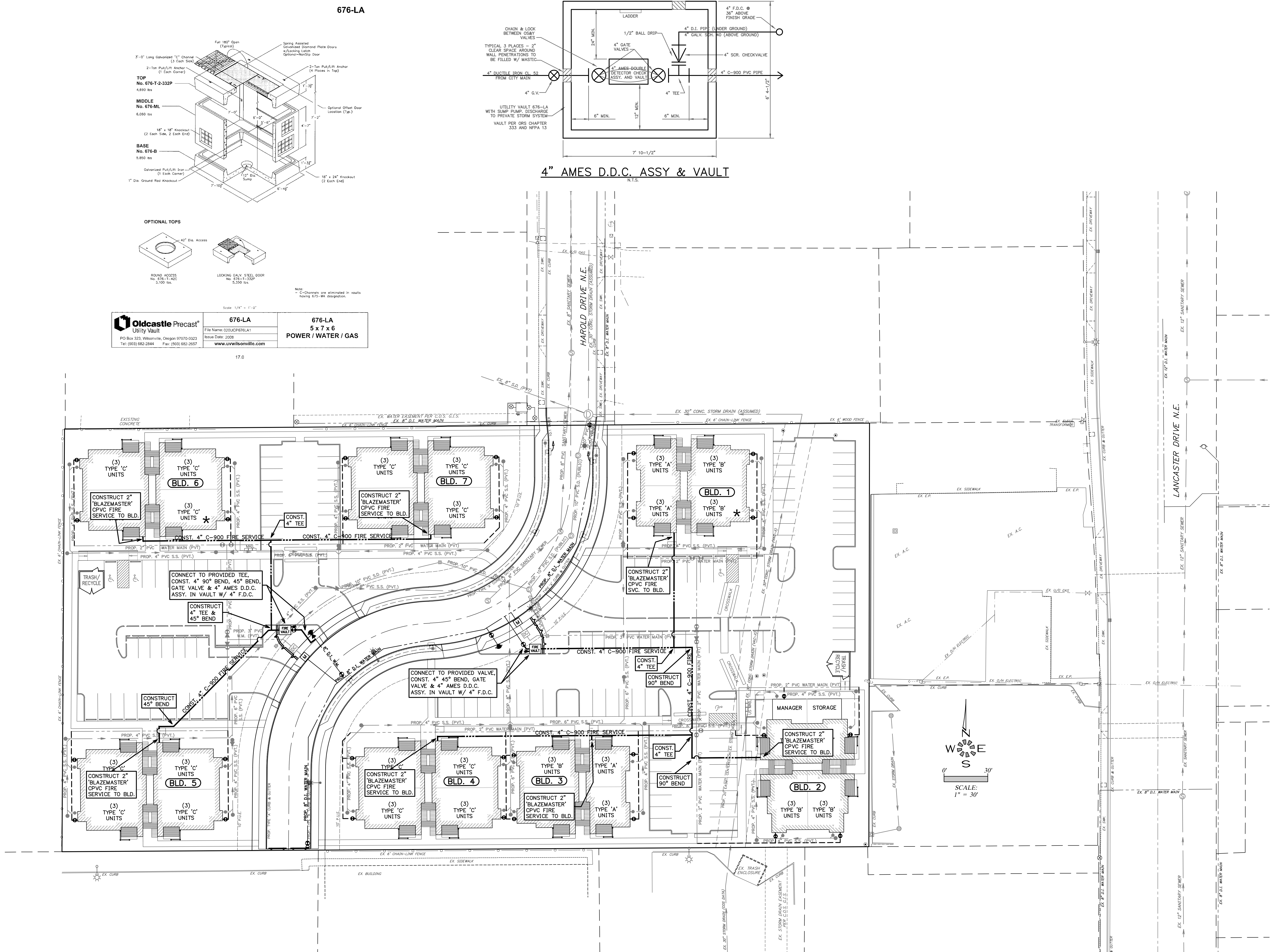
REGISTERED PROFESSIONAL ENGINEER
JULY 14, 1971
MARK D. GREW
EXPIRES: 06-30-2017
JOB # 6059

SDR7

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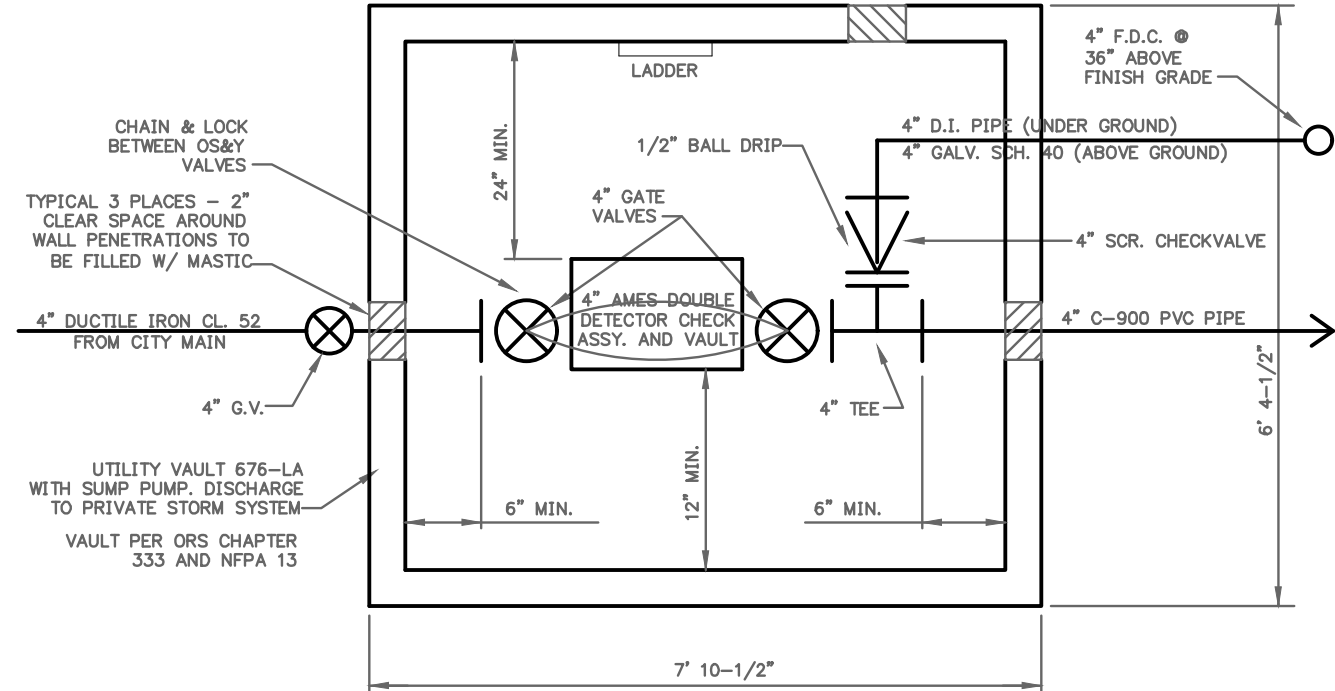
HAROLD DRIVE APARTMENTS

PRIVATE DOMESTIC WATER PLAN



	676-LA	676-LA
	Utility Vault	5 x 7 x 6
	File Name: 020UCP676LA1	POWER / WATER / GAS
	PO Box 323, Wilsonville, Oregon 97070-0323 Tel: (503) 682-2844 Fax: (503) 682-2857	Issue Date: 2008 www.uv.wilsonville.com

17.0



4" AMES D.D.C. ASSY & VAULT
N.T.S.

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P.H.S. 06/20/2016
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DRAWN: P.H.S.
CHECKED: M.D.G.
DATE: JAN. 2015
SCALE: AS SHOWN
AS-BUILT:

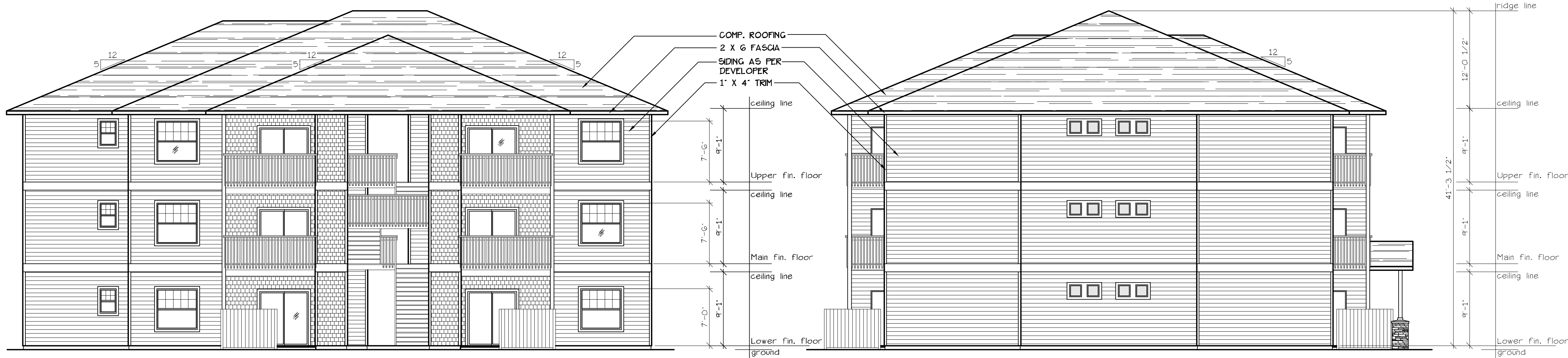
REGISTERED PROFESSIONAL ENGINEER
JULY 14, 1915
MARK D. GREAL
EXPIRES: 06-30-2017
JOB # 6059

SDR8

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www.multitech.com office@multitech.com

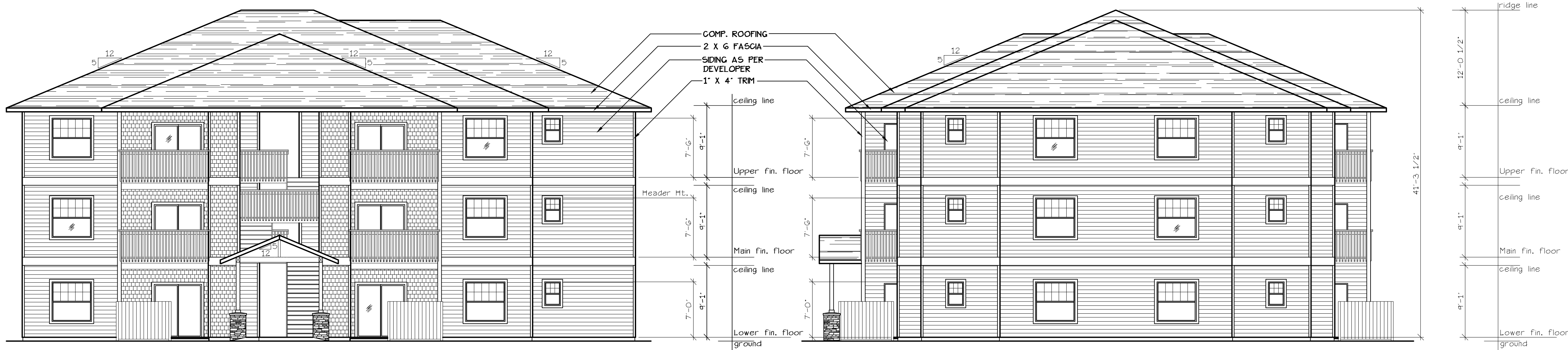
PRIVATE
FIRE MAIN
PLAN

HAROLD
DRIVE
APARTMENTS



REAR ELEVATION (BLD. 1 + 3)
SCALE: 1/8" = 1'-0"

END ELEVATION (BLD. 1 + 3) TYPE A UNITS
SCALE: 1/8" = 1'-0"



FRONT ELEVATION (BLD. 1 + 3)
SCALE: 1/8" = 1'-0"

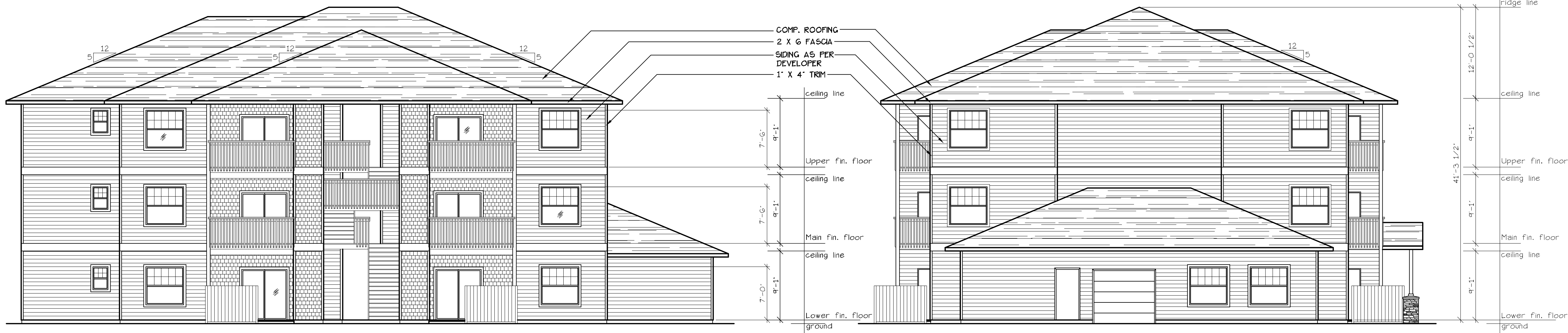
END ELEVATION (BLD. 1 + 3) TYPE B UNITS
SCALE: 1/8" = 1'-0"

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REVISIONS TO BE MADE TO
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ENGINEER.
DIMENSIONS & NOTES TAKE
PRECEDENCE OVER GRAPHICAL
REPRESENTATION.

Design: P.L.M.
Drawn: G.L.D.
Checked: M.D.G.
Date: MAR 2015
Scale: AS SHOWN

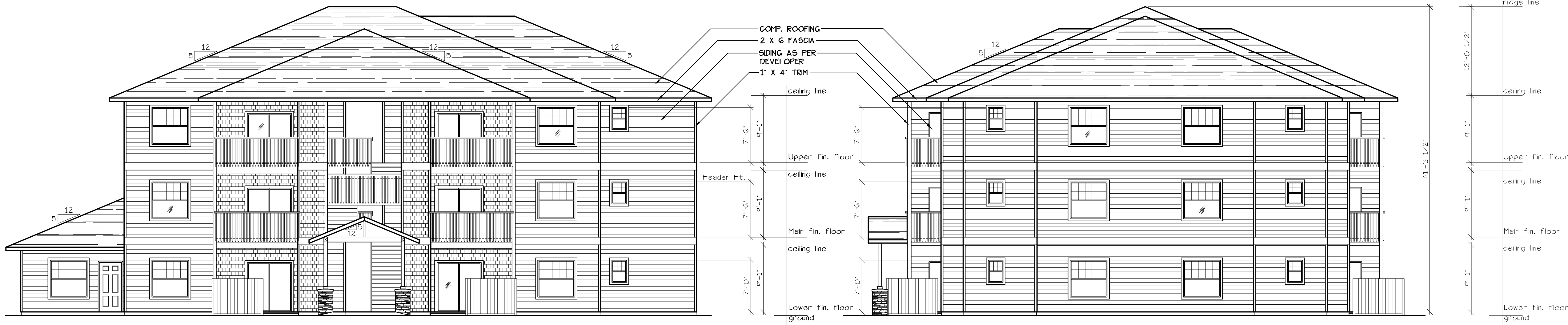
JOB # 6059





EAST ELEVATION (BLD. 2)
SCALE: 1/8" = 1'-0"

NORTH ELEVATION (BLD. 2) (TYPE A UNITS)
SCALE: 1/8" = 1'-0"



WEST ELEVATION (BLD. 2)
SCALE: 1/8" = 1'-0"

SOUTH ELEVATION (BLD. 2) (TYPE B UNITS)
SCALE: 1/8" = 1'-0"



TYPE C UNITS

TYPE C UNITS

REAR ELEVATION (BLD. 4-7)

SCALE: 1/8" = 1'-0"

COMP. ROOFING
2 X 6 FASCI
SIDING AS PER
DEVELOPER
1" X 4" TRIM

ceiling line
7'-6"
9'-1"
Upper fin. floor
ceiling line
7'-6"
9'-1"
Main fin. floor
ceiling line
7'-0"
9'-1"
Lower fin. floor
ground



END ELEVATION (BLD. 4-7)

SCALE: 1/8" = 1'-0"

TYPE C UNITS

ridge line
12'-0 1/2"
ceiling line
9'-1"
41'-3 1/2"
Upper fin. floor
ceiling line
9'-1"
Main fin. floor
ceiling line
9'-1"
Lower fin. floor
ground



TYPE C UNITS

TYPE C UNITS

FRONT ELEVATION (BLD. 4-7)

SCALE: 1/8" = 1'-0"

COMP. ROOFING
2 X 6 FASCI
SIDING AS PER
DEVELOPER
1" X 4" TRIM

ceiling line
7'-6"
9'-1"
Upper fin. floor
ceiling line
7'-6"
9'-1"
Main fin. floor
ceiling line
7'-0"
9'-1"
Lower fin. floor
ground



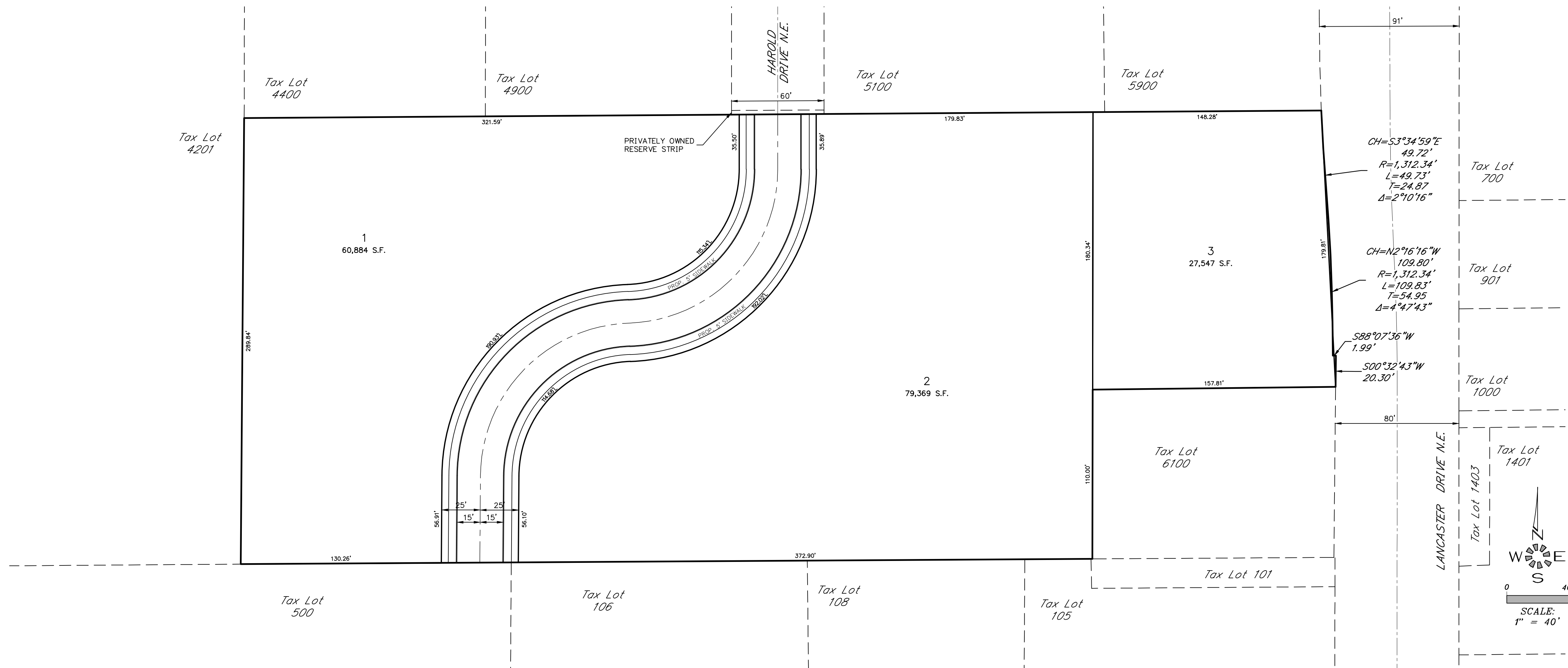
END ELEVATION (BLD. 4-7)

SCALE: 1/8" = 1'-0"

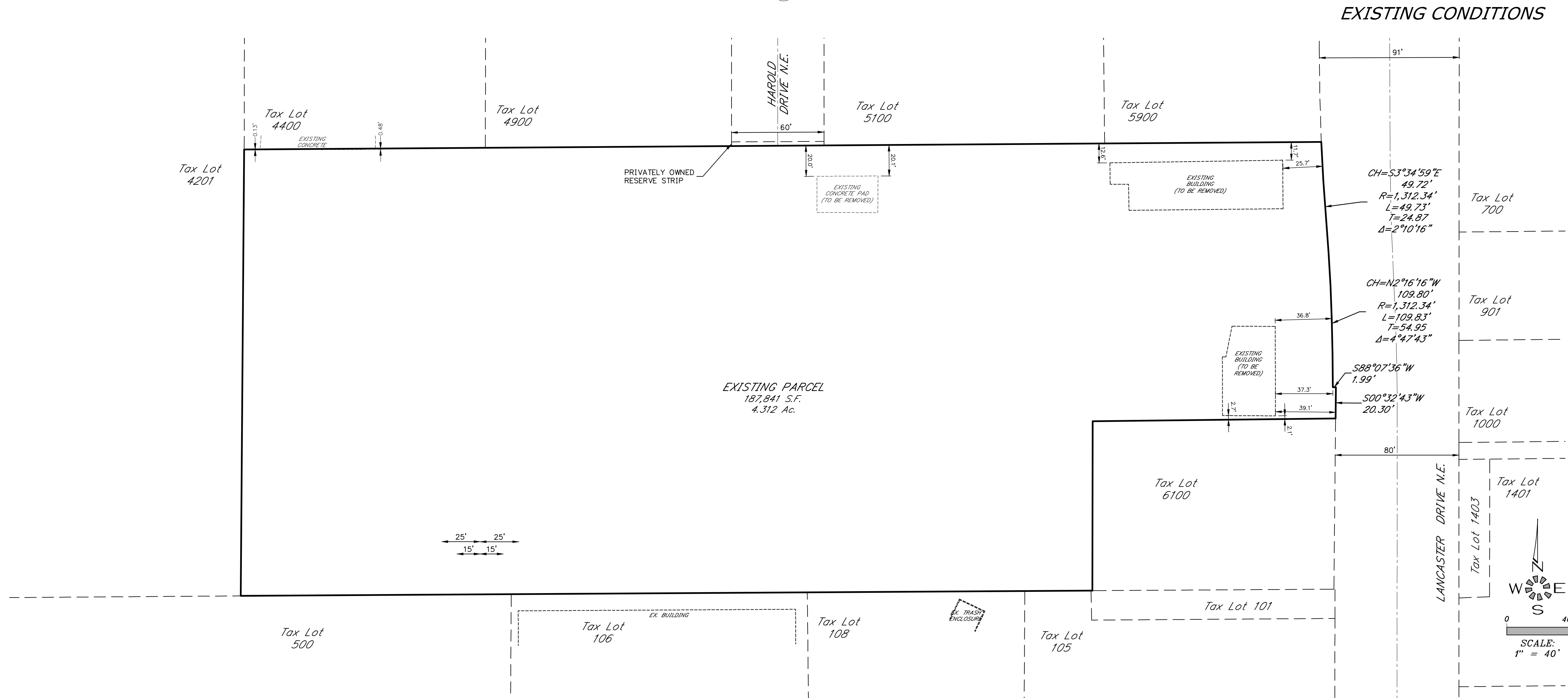
TYPE C UNITS

ridge line
12'-0 1/2"
ceiling line
9'-1"
41'-3 1/2"
Upper fin. floor
ceiling line
9'-1"
Main fin. floor
ceiling line
9'-1"
Lower fin. floor
ground

J:\6000-6059\6059-Harold\new\apartments (new)\6059-PART.DWG, 5/1/2015 11:28:23 AM



PROPOSED CONDITIONS



EXISTING CONDITIONS

Attachment C

Engineer / Surveyor:

Multi/Tech Eng. Inc.

1155 13th St. S.E.
SALEM, OREGON 97302

Owner / Developer:

Killen Development LLC

3842 NW McIntosh Road
CAMAS, WASHINGTON 98607

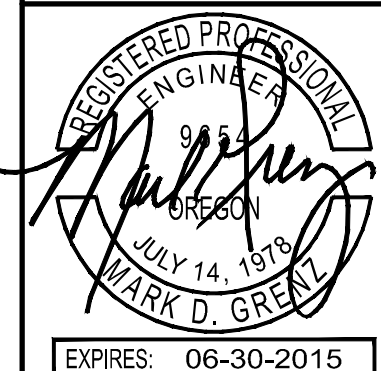


TENTATIVE
PARTITION
PLAN

HAROLD DRIVE

NO CHANGES, MODIFICATIONS OR REPRODUCTIONS TO BE MADE TO THESE DRAWINGS WITHOUT WRITTEN AUTHORIZATION FROM THE DESIGN ENGINEER.
DIMENSIONS & NOTES TAKE PRECEDENCE OVER GRAPHICAL REPRESENTATION.

Design: M.D.G.
Drawn: C.D.S.
Checked: M.D.G.
Date: JAN. 2015
Scale: AS SHOWN
As Built: _____



JOB # 6059

*Si necesita ayuda para comprender esta informacion, por favor llame
503-588-6173*

DECISION OF THE PLANNING ADMINISTRATOR

PARTITION CASE NO. PAR15-06

APPLICATION NO.: 15-104023-LD

NOTICE OF DECISION DATE: AUGUST 18, 2015

APPLICATION SUMMARY: An application to divide property located 3271 Lancaster Drive NE into 3 parcels.

REQUEST: To divide approximately 4.3 acres into 3 parcels, with proposed Parcel 1 consisting of approximately 60,884 square feet, proposed Parcel 2 consisting of approximately 79,369 square feet, and proposed Parcel 3 consisting of approximately 27,547 square feet. The property is zoned CR (Commercial Retail) and located at 3271 Lancaster Drive NE (Marion County Assessor's Map and Tax Lot Number: 072W18BB06000).

APPLICANT: KILLEN DEVELOPMENT LLC (RANDALL KILLEN, JIM GRIGGS)

LOCATION: 3271 LANCASTER DR NE

CRITERIA: Salem Revised Code 205.005(d)

DECISION: The Planning Administrator **GRANTED** Partition Case No. PAR15-06, subject to the following conditions of approval:

- Condition 1.** Prior to final plat approval all existing structures and paved areas on the subject property shall be removed or:
- 1) That portion of the existing paved area on Parcel 3 that extends over the proposed lot line into Parcel 2 shall be removed and brought into conformance with the minimum interior rear setback requirements of the CR zone for vehicle use areas under SRC 522.010(b); and
 - 2) Parcel 3 shall be brought into conformance with the minimum development site landscaping requirements of the CR zone under SRC 522.010(d)(3).
- Condition 2:** Provide a tentative stormwater design sufficient to demonstrate how future impervious surfaces will be conveyed from the Harold Drive extension and each parcel.
- Condition 3:** Provide a storm drainage easement for the existing 27-inch storm main located on the subject property pursuant to Public Works Design Standards.
- Condition 4:** Construct a public sanitary sewer extension as needed to serve Parcels 1 and 2.

NOTICE OF DECISION

PLANNING DIVISION
555 LIBERTY ST. SE, RM 305
SALEM, OREGON 97301
PHONE: 503-588-6173
FAX: 503-588-6005

CITY OF Salem
AT YOUR SERVICE

- Condition 5:** Construct an 8-inch public water main in Harold Drive within the subject property. The new water main shall connect to the existing main at the southerly terminus in Harold Drive or to the existing main in Devonshire.
- Condition 6:** Provide a 10-foot-wide public utility easement (PUE) along the entire street frontage of each parcel.
- Condition 7:** Ensure access to Parcel 1 and Parcel 2 by completing one of the following:
- 1) Dedicate as public right-of-way the existing reserve strip located at the southerly terminus of Harold Drive NE by either the applicant's initiative, or the City's initiative by entering into an acquisition agreement with the City pursuant to SRC 200.050 and depositing monies toward acquisition as specified by the City Attorney; the agreement shall include a performance guarantee pursuant to SRC 110.100(c)(6) restricting issuance of building permits for development of Parcel 1 and Parcel 2; or
 - 2) Secure a 35-foot-wide permanent public access easement across the abutting property to the south from Devonshire Avenue NE to the south line of the subject property either through the applicant's initiative, or the City's initiative by entering into an acquisition agreement with the City pursuant to SRC 200.050 and depositing monies toward acquisition as specified by the City Attorney; the agreement shall include a performance guarantee pursuant to SRC 110.100(c)(6) restricting issuance of building permits for development of Parcel 1 and Parcel 2.
- Condition 8:** Prior to approval of building permits for development of Parcel 1 or Parcel 2, whichever comes first:
- 1) Construct Harold Drive to local street standards within the existing reserve strip located at the southerly terminus of Harold Drive; or
 - 2) Provide, as an interim improvement, two minimum 12-foot-wide vehicular travel lanes and a 5-foot-wide pedestrian access way from Devonshire Avenue NE to the south line of the subject property; or
 - 3) Provide a performance guarantee pursuant to SRC 110.100 ensuring the construction of (1) or (2) above; the improvement shall be completed as a condition of final occupancy for development of Parcel 1 or Parcel 2, whichever comes first.
- Condition 9:** Convey a minimum 50-foot-wide right-of-way through the subject property as shown on the tentative plan from the northern boundary of the subject property adjacent to the existing terminus of Harold Drive NE to the southern boundary of the subject property.
- Condition 10:** Prior to approval of building permits for development of Parcel 1 or Parcel 2, whichever comes first:
- 1) Construct Harold Drive NE to local street standards with curb line sidewalks within the subject property as shown on the tentative plan; or

- 2) Provide a performance guarantee pursuant to SRC 110.100 ensuring the construction of (1) above; the improvement shall be completed as a condition of final occupancy for development of Parcel 1 or Parcel 2, whichever comes first.

The rights granted by the attached decision must be exercised, or an extension granted, by September 3, 2017, or this approval shall be null and void.

A copy of the decision is attached.

Application Deemed Complete: May 13, 2015
Notice of Decision Mailing Date: August 18, 2015
Decision Effective Date: September 3, 2015
State Mandate Date: September 10, 2015

Case Manager: Bryce Bishop, bbishop@cityofsalem.net B.B.

This decision is final unless written appeal from an aggrieved party is filed with the City of Salem Planning Division, Room 305, 555 Liberty Street SE, Salem OR 97301, **no later than 5:00 p.m., September 2, 2015**. The notice of appeal must contain the information required by SRC 300.1020 and must state where the decision failed to conform to the provisions of the applicable code section, SRC Chapter 205.005(d). The appeal must be filed in duplicate with the City of Salem Planning Division. The appeal fee must be paid at the time of filing. If the appeal is untimely and/or lacks the proper fee, the appeal will be rejected. The Planning Commission will review the appeal at a public hearing. After the hearing, the Planning Commission may amend, rescind, or affirm the action, or refer the matter to staff for additional information.

The complete case file, including findings, conclusions and conditions of approval, if any, is available for review at the Planning Division office, Room 305, City Hall, 555 Liberty Street SE, during regular business hours.

<http://www.cityofsalem.net/planning>

*Si necesita ayuda para comprender esta informacion, por favor llame
503-588-6173*

DECISION OF THE PLANNING ADMINISTRATOR

URBAN GROWTH AREA PRELIMINARY DECLARATION NO. UGA15-01

APPLICATION NO.: 15-113927-LD

NOTICE OF DECISION DATE: OCTOBER 9, 2015

REQUEST: An Urban Growth Preliminary Declaration request to determine the public facilities required to develop an apartment complex on property that is approximately 4.31 acres in size, zoned CR (Commercial Retail), and located at 3271 Lancaster Drive NE (Marion County Assessor's Map and Tax Lot Number: 072W18BB06000).

APPLICANT: KILLEN DEVELOPMENT LLC (RANDALL KILLEN)

LOCATION: 3271 LANCASTER DR NE

CRITERIA: Salem Revised Code 200.025(d) & (e)

FINDINGS: The Findings and Order dated October 9, 2015 is attached.

DECISION: The Planning Administrator **GRANTED** Urban Growth Area Preliminary Declaration No. UGA15-01, subject to the applicable standards of the Salem Revised Code, the findings contained herein, and the following condition of approval:

Condition 1: Construct a 30-foot-wide linking street improvement within a 60-foot-wide right-of-way from the southerly terminus of Harold Drive NE to the north line of the subject property. This improvement requires acquisition of the reserve strip at the southerly terminus of Harold Drive NE as right-of-way.

The rights granted by the attached decision must be exercised, or an extension granted, by **October 27, 2019**, or this approval shall be null and void.

A copy of the decision is attached.

Application Deemed Complete:	<u>August 25, 2015</u>
Notice of Decision Mailing Date:	<u>October 9, 2015</u>
Decision Effective Date:	<u>October 27, 2015</u>
State Mandate Date:	<u>December 23, 2015</u>

Case Manager: Bryce Bishop, bbishop@cityofsalem.net

BB.

This decision is final unless written appeal from an aggrieved party is filed with the City of Salem Planning Division, Room 305, 555 Liberty Street SE, Salem OR 97301, **no later than 5:00 p.m., October 26, 2015**. The notice of appeal must contain the

NOTICE OF DECISION

PLANNING DIVISION
555 LIBERTY ST. SE, RM 305
SALEM, OREGON 97301
PHONE: 503-588-6173
FAX: 503-588-6005



information required by SRC 300.1020 and must state where the decision failed to conform to the provisions of the applicable code section of SRC Chapter 200. The appeal must be filed in duplicate with the City of Salem Planning Division. The appeal fee must be paid at the time of filing. If the appeal is untimely and/or lacks the proper fee, the appeal will be rejected. The City Council will review the appeal at a public hearing. After the hearing, the City Council may amend, rescind, or affirm the action, or refer the matter to staff for additional information.

The complete case file, including findings, conclusions and conditions of approval, if any, is available for review at the Planning Division office, Room 305, City Hall, 555 Liberty Street SE, during regular business hours.

<http://www.cityofsalem.net/planning>



MEMO

TO: Bryce Bishop, Planner II
Community Development Department

FROM: Glenn J. Davis, PE, CFM, Chief Development Engineer
Public Works Department

DATE: August 9, 2016

SUBJECT: PUBLIC WORKS RECOMMENDATIONS
CU-DR-SPR-DAP-ADJ 16-06 (16-108590)
3271 LANCASTER DRIVE NE
84-UNIT APARTMENT COMPLEX

RECEIVED

AUG 08 2016

COMMUNITY DEVELOPMENT

PROPOSAL

A consolidated application for a Conditional Use Permit, Design Review, Site Plan Review, Driveway Approach Permit, and Class 2 Adjustment to allow for the development of a proposed 84-unit multiple family development on property located at 3271 Lancaster Drive NE.

The subject property totals approximately 4.31 acres in size, is zoned CR (Commercial Retail), and is located at 3271 Lancaster Drive NE (Marion County Assessor's Map and Tax Lot Number: 072W18BB06000).

RECOMMENDED CONDITIONS OF APPROVAL

1. Record the final plat pursuant to Partition Case PAR15-06.
2. Dedicate as public right-of-way the existing reserve strip located at the southerly terminus of Harold Drive NE (Harold).
3. Construct Harold to local street standards with curb line sidewalks within the subject property and connecting to the southerly terminus of the existing Harold improvements as shown on the tentative plan.
4. Design and construct stormwater facilities in accordance with SRC Chapter 71.

FACTS

Streets

1. Harold Drive NE
 - a. Standard—This street is designated as a Local street in the Salem TSP. The standard for this street classification is a 30-foot-wide improvement within a 60-foot-wide right-of-way.

Storm Drainage

1. Existing Conditions
 - a. A 30-inch storm main is located in Harold.

Water

1. Existing Conditions
 - a. The subject property is located in the G-0 water service level.
 - b. An 8-inch water main is located in Harold. Mains of this size generally convey flows of 500 to 1,100 gallons per minute.

Sanitary Sewer

1. Existing Conditions
 - a. An 8-inch sewer line is located in Harold.

CRITERIA AND FINDINGS

Analysis of the development based on relevant criteria in SRC 220.005(f)(3) is as follows:

Criteria—The transportation system provides for the safe, orderly, and efficient circulation of traffic into and out of the proposed development, and negative impacts to the transportation system are mitigated adequately

Finding—The existing configuration of Harold does not meet current standards for its classification of street per the Salem TSP. Harold is being extended to the south line of the subject property to provide sufficient access for the proposed development.

Criteria—Parking areas and driveways are designed to facilitate safe and efficient movement of vehicles, bicycles, and pedestrians

Finding—The driveway accesses onto Harold provides for safe turning movements into and out of the property.

Criteria—The proposed development will be adequately served with City water, sewer, storm drainage, and other utilities appropriate to the nature of the development

Finding—The Public Works Department has reviewed the applicant's preliminary utility plan for this site. The water, sewer, and storm infrastructure are available within surrounding streets / areas and appear to be adequate to serve the proposed development. The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the PWDS and to the satisfaction of the Public Works Director. The applicant is advised that a sewer monitoring manhole may be required, and the trash area shall be designed in compliance with Public Works Standards.

The applicant's engineer submitted a statement demonstrating compliance with Stormwater PWDS Appendix 004-E(4)(b) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible.

Criteria—A Class 2 Driveway Approach Permit shall be granted if:

1. **The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;**

Finding—The proposed driveways meet the standards for SRC 804 and PWDS with a Class 2 adjustment for multiple driveway approaches onto a local street where only one is allowed SRC 804.030(a). The Class 2 adjustment is warranted because multiple accesses are needed given the limited area for internal parking circulation. Pursuant to Class 2 adjustment criteria in SRC 250.005(d)(2), the multiple driveway accesses provide equal or better access for the proposed development because vehicle turning movements have a negligible effect on local streets.

2. **No site conditions prevent placing the driveway approach in the required location;**

Finding—There are no site conditions prohibiting the location of the proposed driveways.

3. **The number of driveway approaches onto an arterial is minimized;**

Finding—The proposed driveways are not accessing onto an arterial street.

4. The proposed driveway approach, where possible:

- a) Is shared with an adjacent property; or**
- b) Takes access from the lowest classification of street abutting the property;**

Finding—The proposed driveways are currently located on Harold with access to the lowest classification of street abutting the subject property.

5. The proposed driveway approach meets vision clearance standards;

Finding—The proposed driveways meet the PWDS vision clearance standards set forth in SRC Chapter 805.

6. The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;

Finding—The proposed driveways will not create a known traffic hazard and will provide for safe turning movements for access to the subject property.

7. The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Finding—The location of the proposed driveways do not appear to have any adverse impacts to the adjacent properties or streets.

8. The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and

Finding—The proposed driveway approaches are located on a local street and do not create a significant impact to adjacent streets and intersections.

9. The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Finding—The proposed driveway approaches are not located in the vicinity of a residentially zoned area. The driveway will not have an effect on the functionality of the adjacent streets.

Prepared by: Nathan Coapstick, Project Coordinator



DAVID FRIDENMAKER, Manager
Facility Rental, Planning, Property Services
3630 State Street, Bldg. C • Salem, Oregon 97301-5316
503-399-3335 • FAX: 503-375-7847

Christy Perry, Superintendent

July 21, 2016

Bryce Bishop, Planner
Planning Division, City of Salem
555 Liberty Street SE, Room 305
Salem OR 97301

FAX No. 503-588-6005

RE: Land Use Activity
Salem Case No. CU-DR-SPR-DAP-ADJ16-06, 3271 Lancaster Dr. NE

SUMMARY OF COMMENTS

School Assignment: Chavez Elementary School, Stephens Middle School and McKay High School

School Capacity: Sufficient school capacity currently exists at the assigned middle school and high school, but not at the assigned elementary school.

School Transportation Services: Students residing at the subject property location will be eligible for school transportation services to the assigned elementary school and middle school and will be within the walk zone of McKay High School.

Below is data and the District's comments regarding the proposed land use activity identified above. If you have questions, please call at (503) 399-3335.

ELEMENTARY SCHOOL INFORMATION (GRADES K TO 5)

1. School Name: Chavez Elementary School
2. Estimated change in student enrollment due to proposed development: 17
3. Current school capacity: 598
4. Estimate of school enrollment including new development: 609
5. Ratio of estimated school enrollment to total capacity including new development: 102%.
6. Walk Zone Review: Eligible for transportation to Elementary School.
7. Estimate of additional students due to previous 2015 land use applications: 13
8. Estimate of additional students due to previous 2016 land use applications: 0
9. Estimated cumulative impact of 2015-16 land use actions on school capacity: 104% of capacity.

MIDDLE SCHOOL INFORMATION (GRADES 6 TO 8)

1. School Name: Stephens Middle School
2. Estimated change in student enrollment due to proposed development: 7
3. Current school capacity: 1,094
4. Estimate of school enrollment including new development: 1,069
5. Ratio of estimated school enrollment to total capacity including new development: 98%
6. Walk Zone Review: Eligible for transportation to Middle School
7. Estimate of additional students due to previous 2015 land use applications: 5

8. Estimate of additional students due to previous 2016 land use applications: 1
9. Estimated cumulative impact of 2015-16 land use actions on school capacity: 98% of capacity.

HIGH SCHOOL INFORMATION (GRADES 9 TO 12)

1. School Name: McKay High School
2. Estimated change in student enrollment due to proposed development: 7
3. Current school capacity: 2,367
4. Estimate of school enrollment including new development: 2,341
5. Ratio of estimated school enrollment to total capacity including new development: 99%
6. Walk Zone Review: Within walk zone of High School.
7. Estimate of additional students due to previous 2015 land use applications: 11
8. Estimate of additional students due to previous 2016 land use applications: 1
9. Estimated cumulative impact of 2015-16 land use actions on school capacity: 99% of capacity.

ESTIMATE SUMMARY (GRADES K TO 12):

1. Total estimated change in student enrollment: 31
2. Total estimated student enrollment over capacity: 11

Developer should provide paved walk route(s) to allow pedestrian access and bicycle access to school(s) from all residences within the new development and should provide all improvements required by the City of Salem where new transportation routes are established or existing transportation routes change, such as school flashers, crosswalks, and signage. As per ORS 195.115, when the walk zone review indicates "eligible for transportation due to hazard" the District requests that the City initiate a planning process with the District to identify the barriers and hazards to children walking or bicycling to and from school, determine if the hazards can be eliminated by physical or policy changes and include the hazard elimination in the City's planning and budgeting process.

ASSUMPTIONS:

1. When land use request is granted, 84 new residence(s) will be built.
2. Estimates are computed using the Student Rate per Dwelling Method described in the District's Facility Study for years 2001-2020.
3. If current capacity exists at the schools currently serving the parcel then an estimate of zero cost, or no significant impact, is made.
4. If current capacity does not exist at the schools currently serving the parcel then an estimate of cost for one-time capital improvements is made.
5. Income from the proposed land use for capital improvement is assumed to be zero since capital improvement funds come from voter approved bond measures that can be an unpredictable and irregular source of income.

Sincerely,



David Fridenmaker, Manager
Planning and Property Services

c: Mike Wolfe, Chief Operations Officer
David Hughes, Manager – Custodial and Property Services
William White, Manager - Risk Management
Michael Shields, Director – Auxiliary Services

Harold Drive Apartments **Revised-Conditional Use**

PROPOSAL:

Under SRC 522.005(a)-Table 522-1, multiple family dwellings are allowed within the CR zone with a Conditional Use permit.

The subject property is 4.31 acres in size and zoned CR (Commercial Retail). The applicant is applying for a Conditional Use for the construction of an 84 unit apartment complex.

CONDITIONAL USE CRITERIA:

SRC 240.005(d) - An application for Conditional Use permit shall be granted if all of the following criteria are met:

- (1) The proposed use is allowed as a conditional use in the zone;*
- (2) The reasonably likely adverse impacts of the use on the immediate neighborhood can be minimized through the imposition of conditions; and*
- (3) The proposed use will be reasonably compatible with and have minimal impact on the livability or appropriate development of surrounding property.*

APPLICANT'S REASONS ADDRESSING CHAPTER 240.005(d):

- (1) Under SRC 522.005(a)-Table 522-1, multiple family dwellings are allowed within the CR zone with a Conditional Use permit.
- (2) The proposed apartments will have little to no impact on the neighborhood. The subject property is surrounded by commercial uses to the north, east, and south, along with apartments to the west.

The proposed apartments will have less an impact on the area than a commercial use that is allowed within the CR zone. The applicant's Traffic Engineer has provided an analysis of the site. The analysis indicates that the proposed apartments will have less of an impact on the neighborhood than the allowed uses within the CR zone. The analysis dated April 28, 2016, states, "The proposed 84 apartment units will generate less traffic each day than many allowed uses in the CR zoned." Therefore, the development of apartments on the site will not impact the neighborhood. See attached memo dated April 28, 2016.

The proposal will be required to go through Site Plan/Design Review to ensure that all setbacks and design standards are met. Any conditions placed on the site will require Code compliance, which will help ensure minimal to no impacts on the neighborhood.

(3) The subject property is surrounded by the following uses:

North: CR zoned property: existing commercial uses

East: CR zoned property: vacant commercial zoned property

South: CR zoned property: existing commercial uses

West: Marion County RM zoned property: existing multi-family dwellings

The apartments will be compatible with the residential uses to the west, and the surrounding commercial uses. The existing commercial uses are in a convenient location for the residents of the site. Development of residential uses are most compatibility in areas that provide services amenities like commercial uses for the residents.

The apartments will be required to go through Site Plan/Design Review, which requires open space and landscaping at a higher percent than what a commercial use would be required to provide. Amenities like landscaped open space will help to with the visual appeal of this area and reduce impacts on the neighborhood. The design standards are in place to help ensure compatibly with adjacent uses.

Harold Drive Apartments

The following statement addresses the applicable Design Review approval guidelines in the SRC Chapter 702 (Multiple Family Design Review Guidelines and Design Review Standards) and the requirements under the CR zone Chapter 522 (Commercial Retail). Information provided on the site plans for the Design Review application further address applicable code requirements.

On August 18, 2015, Partition Plat No. 15-06 was approved. The Partition approval allowed the applicant to divide the subject property into three parcels. The Harold Drive Apartments will be located on Parcels 1 and 2 of Partition Plat No. 15-06.

On October 9, 2015, UGA 15-01 was approved for the subject property with Conditions of Approval.

On February 22, 2016, a pre-application conference was held with the applicant and City staff to discuss the development of the subject property.

Proposal: The subject property is located at 3271 Lancaster Drive NE (072W18BB/6000).

The applicant is proposing the development of 3.68 acres of CR zoned property. The applicant is proposing to develop the site with 84 apartment units as shown on the site plans.

The applicant is requesting to meet the guidelines and go before the Design Review Board. All guidelines have been addressed and met as outlined within this narrative and on the site plans.

The applicant is also requesting a Class 2-Driveway Approach Permit (DAP) to allow driveways approach onto Harold Drive.

Commercial Retail (CR) SRC Chapter 522

Setbacks: All minimum setbacks to property lines, between buildings and distances to the entrances are met as shown on the tentative plan. Setbacks are shown on the tentative plan.

North:	10-foot setback (City of Salem CR (Commercial Retail) and Marion County CG and CR); existing Office Building, grocery store, and industrial uses
East:	10-foot setback (City of Salem CR (Commercial Retail); vacant land
South:	10-foot setback (City of Salem CR (Commercial Retail); Apartments, restaurants and grocery store
West:	10-foot setback (Marion County-RM); existing apartment development

Maximum Height: Maximum building height allowed in the CR zone is 50'. The proposed buildings will be 41'-3" measured to the highest point of the roof and 33'-3" measured to the middle of the gable. Therefore, the buildings are in compliance with the building height requirement.

Multiple Family Design Review Guidelines - Chapter 702

702.015 (b)(1) Common Open Space Guidelines and 702.015(c)(1): In multi-family developments, a portion of the land not covered by buildings and parking shall be of adequate size and shape and in the proper location to be functional for outdoor recreation and relaxation. The guidelines are also intended to ensure that open space is an integral part of the overall development design.

The minimum open space area (landscaped area) provided for this development is 30.9% common open space on the site. The subject property is 160,203 (3.68 acres/net area) square feet in size with 43,318 square feet of landscaped open space. Therefore, totaling 30.9% open space.

Open Space

Common Open Space _____ 61,570 S.F. (38%)

Common Open Space

Within Setbacks _____ 27,883 S.F.

Landscape _____ 43,318 S.F. (30%)

The proposed development provides 1,828 square feet of play area in the east portion of the development, 1,721 square feet of play area in the west portion of the development, and landscaped open space areas throughout the site. The play area is available to all 84 dwelling units and accessible via the proposed pedestrian pathways (6-foot wide paved sidewalks). The play area and open space areas are located in convenient and safe areas for the residents. The proposed development provides approximately 61,570 square feet of total common open space throughout the site. Common open space and a play area have been provided on both the eastern and western portion of the site. Therefore, this guideline has been met. See attached site plans.

702.015 (d)(1) Private Open Space Guidelines: Each unit will have private open space as required by code. Ground floor units will have patio areas that are 96 square feet in size, with no dimension less than 6 feet. All second and third story units will have balconies/decks that are a minimum 48 square feet in size. All private open space areas are located contiguous to the dwelling unit and will be screened with landscaping and/or fencing. This private open space includes the patios and balconies/decks. Therefore, this guideline has been met.

702.020 (b)(1) Landscaping Guidelines: The subject property does not abut RS or RA zoned properties. There is an existing multi-family development located to the west of the site. Landscaping is being provided adjacent all property lines. Landscaping has been provided throughout the site as identified on the landscape plans. A minimum of 1 tree will be planted for every 2,000 square feet of the site. Trees and vegetation have been provided throughout the development as shown on the landscape plans. There is 43,318 square feet of landscaped area throughout the site. Therefore, 30.9% of the site is landscaped. Landscape plans have been provided and demonstrate how the landscape guidelines have been met.

A permanent underground irrigation system will be provided when development plans are final.

There are a few existing trees on the site. There are no significant trees or heritage trees on the property. All of the trees on the site are designated for removal. Any trees on the site that are within building envelopes, access ways, parking areas, right-of-way, or will be effected by grading have to be removed.

New trees will be provided through the site as shown on the landscape plans.

702.010 (c)(1) Street Frontage Guidelines: The landscape plans identify how this guideline is met. Trees will be provided along the street frontage with one canopy tree per 50 linear feet. See attached landscaped plans. Therefore, this guideline has been met.

702.020 (d)(1) Building Exterior Guidelines: The exterior of the buildings will be landscaped to provide a visually appealing development. Trees and shrubs will be planted in front of and around all buildings as shown on the landscape plans. This will help to provide shading and privacy for residents. Therefore, this guideline has been met.

702.020 (e)(1) Privacy Guidelines: All ground level private open space areas (patios) will be screened and separated with landscaping and/or fencing. This will help to provide privacy for ground level residents. Therefore, this guideline has been met.

702.020 (f)(1) Landscape Parking Guidelines: In order to take into consideration circulation, pedestrian access, landscaping, and the requirements of the code, the parking areas have been carefully designed. All parking areas are landscaped as required, and separated by landscaped bays. The parking areas and landscaped areas provide for visually appealing apartment grounds. Therefore, this guideline has been met. See attached site plans.

702.025 (a)(1) Crime Prevention Guidelines: Safety of the residents is very important and all requirements are met to assure safety and compliance with code. There are no fences or plant materials located in areas within the development that obstruct visibility. All landscaping adjacent to open space areas will not exceed 3 feet in height.

All buildings have windows provided in habitable rooms and windows that face the parking lots and open space areas. This helps provide an eye on the development. Lighting on the buildings and along the sidewalks will be provided as well. Therefore, this guideline has been met. See attached site plans.

702.030(b)(1) Parking, Site Access, and Circulation Guidelines: The subject property has street frontage on Harold Drive along the north property line, via a stubbed connection.

Harold Drive is designated as 'local' street within the City of Salem Transportation Plan. The applicant is required to provide a north/south Harold Drive connection through the development as shown on the site plan. Therefore, this guideline has been met.

A Traffic Impact Analysis (TIA) is not required for this development.

Parking: The development is for an 84 unit apartment complex. Code requires 1.5 vehicle parking spaces per every 1 dwelling unit. Therefore, the applicant is required to provide 126 on-site vehicle parking spaces. However, as shown on the site plan, 138 on-site parking spaces are being provided.

Adequate parking has been provided on both the eastern and western portions of the development.

The site plan indicates the ratios provided for standard, compact, handicap spaces and location of bike spaces. Fifty-four (54) standard size parking stalls, eighty (80) compact parking stalls, and four (4) handicap stalls are provided through-out the site.

All parking areas will be served by 24 to 26-foot wide two-way accessways that run through the development.

Bicycle parking is also required on site. The Code requires 0.1 bicycle parking space per dwelling unit. Eight bike parking spaces are required for this development. Bike racks have been provided on the site and located in a convenient location for the residents. Providing fourteen (14) on-site bicycle parking spaces.

(A) Parking areas shall be designed to minimize the expanse of continuous parking (see Figure 702-6).

Due to the required Harold Drive extension and the location of the property, adding additional planter islands and/or bays is not feasible. However, the parking areas are heavily landscaped

as shown on the site plan. The landscaped areas help to minimize the continuous parking areas, along with the proposed planter islands.

(B) Pedestrian pathways shall be provided that connect to and between buildings, common open space, parking areas, and surrounding uses (see Figure 702-7).

The internal pedestrian circulation system consists of hard 5 to 6-foot wide surfaced sidewalks that provide easily identifiable and safe connections between the residential units, parking, recreation areas, manager's apartment, and the trash disposal area. The pedestrian system connects the buildings to the public sidewalk system within Harold Drive via the proposed internal sidewalk system. The sidewalks are raised above the surface of the travel lanes. This provides a clear separation between vehicles and pedestrians. Any pedestrian pathways that cross the parking area or driveways will be marked and a minimum of five feet wide. The pedestrian pathways will be lighted. Proposed pedestrian sidewalk connections are illustrated on the tentative site plan.

(C) Parking shall be located to maximize the convenience of residents.

All buildings are connected to the internal parking areas via 5 to 6-foot wide sidewalks. The parking areas are located in areas that are convenient for the residents.

(D) Parking areas and circulation systems shall be designed in a manner that considers site topography, natural contours, and any abutting properties zoned Residential Agriculture (RA) or Single Family Residential (RS).

All parking areas and the layout of the site took topography, and adjacent properties into consideration as shown on the site plan.

Therefore, this guideline has been met.

702.030(c)(1) Pedestrian Site Access Guidelines: The internal pedestrian circulation system consists of hard 5 to 6-foot wide surfaced sidewalks that provide easily identifiable and safe connections between the residential units, parking, recreation areas, manager's apartment, and the trash disposal area. The pedestrian system connects the buildings to the public sidewalk system within Harold Drive via the proposed internal sidewalk system. The sidewalks are raised above the surface of the travel lanes. This provides a clear separation between vehicles and pedestrians. Any pedestrian pathways that cross the parking area or driveways will be marked and a minimum of five feet wide. The pedestrian pathways will be lighted. Proposed pedestrian sidewalk connections are illustrated on the tentative site plan.

The design of pedestrian circulation systems shall provide clear and identifiable connections

within the multiple family development and to adjacent uses and public streets/sidewalks. The proposed development provides safe and convenient bicycle and pedestrian access from within the development to adjacent residential areas.

Therefore, this guideline has been met.

702.035(b)(1) Building Mass and Façade Design Guidelines: These guidelines are intended to promote building and site design that contributes positively to a sense of neighborhood and to the overall streetscape by carefully relating building mass, entries and yards to public streets.

The building design does not have long flat walls or roof lines. The buildings will have an offset that breaks up the front of the buildings and the roof lines. Balconies (decks) and dormers are incorporated in the building design to add some visual element to the buildings. There are no buildings within the development that will exceed 150 feet in length. The buildings on the site are about 77 to 99 feet in length. The height and length of the buildings and structures conform to the measuring requirements in code.

Buildings 1, 4, 5 and 7 all face the interior of the lot, with the side of the buildings facing Harold Drive. Due location of the site and the Harold Drive extension the side of these buildings face Harold Drive, as shown on the site plan. The street side of Buildings 1, 4, 5, and 7 will be designed to be visually appealing, by providing similar design as is being provided for the front building façade for all buildings. In order to be consistent with the front facade of the building; windows, offsets, and architectural features will be incorporated in the portions of the building facing the right-of-way (Harold Drive).

Varied materials and textures are being used on the building facade. The applicant has provided building elevations to show how this is being complied with. The materials used on the front, rear, and sides of the apartments are the same; shake siding, trim board, lap siding, and stone around the pillars. See attached building elevations. Therefore, this guideline has been met.

702.035(c)(1) Compatibility Guidelines: The subject property does not abut any RA or RS zoned property. All properties surrounding the site are zoned CR (Commercial Retail).

The primary entrances for each individual unit is provided through a covered entry way. All building entries are clearly defined and easily accessible. The design of the building with the use of roofline offsets and covered entry ways, promote a positive sense of neighborhood. All building entrances face the internal street/parking system.

All the buildings are located on the 20-foot setback line. See attached site plans.

All roof-mounted equipment will be screened and integrated into the building design. Further review of this requirement will take place at the time of building permits.

Therefore, this guideline has been met.

702.035(d)(1) Building Articulation Guidelines: All buildings have entrances physically and visually connected to the internal public sidewalk system and the parking lots. All external stairways are recessed into the buildings. Therefore, physically and visually incorporating them into the buildings architecture design.

The primary entrances for each individual unit is provided through a covered entry way. All building entries are clearly defined and easily accessible. The design of the building with the use of roofline offsets and covered entry ways, promote a positive sense of neighborhood.

The building design does not have long flat walls or roof lines. The buildings will have an offset that breaks up the front of the buildings and the roof lines. All buildings will have a minimum of 2-foot offsets, balconies, patios, eaves, and windows incorporated into the design of each of the buildings. Therefore, this guideline has been met. See building elevations.

702.040(a)(1) Recycling: There are two trash/recycle areas provided within the western and eastern portions of the development. The trash receptacles are accessible for all residents via the paved internal sidewalk system. The trash/recycle areas will all be screened and enclosed with a sight-obscuring fence or wall.

Class 2-Driveway Approach Permit

SRC 804.025 (d) Criteria. A Class 2 Driveway Approach Permit shall be granted if:

(1) The proposed driveway approach meets the standards of this Chapter and the Public Works Design Standards;

Applicant Response: The applicant has been working with Public Works staff regarding the location and design of the driveway. Due to the Harold Drive connection points, there are few driveway location options. Therefore, as shown on the site plan the driveway approach is required for access to the site and is in compliance with Public Works design standards.

(2) No site conditions prevent placing the driveway approach in the required location;

Applicant Response: The location of the driveway approach was taken into consideration prior to laying the site out. The location of the proposed driveways take into consideration the

location of the buildings, required parking, and the required Harold Drive street extension. Therefore, there are no conditions on the site that prevent the driveway approach.

(3) The number of driveway approaches onto an arterial are minimized;

Applicant Response: The proposed driveways are onto a local street. Therefore, this criterion is not applicable.

(4) The proposed driveway approach, where possible:

(A) Is shared with an adjacent property; or

(B) Takes access from the lowest classification of street abutting the property;

Applicant Response to (4)(B): The proposed driveways will take access from Harold Drive, which is designated as a 'local' street in the Salem Transportation Plan. Therefore, this criteria has been met.

(5) The proposed driveway approach meets vision clearance standards;

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approaches are in the required locations and meet vision clearance standards. As shown on the site plan, this criterion has been met.

(6) The proposed driveway approach does not create traffic hazards and provides for safe turning movements and access;

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approaches are in the required locations and do not create traffic hazards. As shown on the site plan, this criterion has been met.

(7) The proposed driveway approach does not result in significant adverse impacts to the vicinity;

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approaches are in the required locations and do not result in adverse impacts to the vicinity. As shown on the site plan, this criterion has been met.

(8) The proposed driveway approach minimizes impact to the functionality of adjacent streets and intersections; and

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approaches are in the required locations to minimize impacts to adjacent streets and intersections. As shown on the site plan, this criterion has been met.

(9) The proposed driveway approach balances the adverse impacts to residentially zoned property and the functionality of adjacent streets.

Applicant Response: The applicant has been working with Public Works to ensure that the driveway approaches are in the required locations to help balance the adverse impacts to residentially zoned property. As shown on the site plan, this criterion has been met.

Date: April 28, 2016
 To: Brandie Dalton, Planner, MultiTech Engineering
 From: Karl Birky, PE, PTOE
 Re: Harold Apartment Traffic Considerations



The proposed Harold Drive Apartments will add 84 apartment units to the City of Salem on a 3.68 Ac site west of Lancaster Dr and between Silverton Rd and Devonshire Ave NE. The site is zoned Commercial Retail (CR) and allows apartments as a conditional use. You've asked for an estimate of the traffic apartments would generate compared to traffic generated by other Commercial Retail uses.




Traffic engineers estimate 84 apartments will add 560 trips each day (ADT) to the City's transportation system. Planners often estimate that a site can be developed with 30% coverage for commercial developments. In other words, the floor space of the stores would be 30% of the site area and the remaining 70% would be parking, landscaping and roadways. The estimated floor space for commercial uses on this site is 48,061 sq. ft. (48 ksf). The following table estimates the traffic generated by commercial development of the site compared to the daily traffic generated by 84 apartments. While these estimates are for the entire site being developed with one use, the probability is high that some combination of uses would develop on the site. Most commercial uses will generate more trips (traffic) than the proposed apartments.

Use	ITE Trip ADT Gen. Rate	Estimated Units	Estimated Trip Gen.
			Entire Site
Apt - ITE 220	6.65 Trips/Apartment	84 Apts	560 ADT
Gen Office - ITE 710	11.02 Trips/ksf*	48 ksf	529 ADT
Med/Dental Office - ITE 720	36.13 Trips/ksf*	48 ksf	1,734 ADT
Specialty Retail - ITE 826	44.32 Trips/ksf*	48 ksf	2,127 ADT
Supermarket - ITE 850	102.24 Trips/ksf*	48 ksf	4,907 ADT
Drive In Bank - ITE 912	148.15 Trips/ksf*	48 ksf	7,111 ADT
Restaurant - ITE 932	127.15 Trips/ksf*	48 ksf	6,103 ADT

* Trips/ksf is average daily trips (ADT) per 1000 sq. ft. of floor space.

The proposed 84 apartment units will generate less traffic each day than many allowed uses in the CR zone. There does not appear to be a reason, from a traffic engineering perspective, the City should not approve this conditional use request. Thank you for asking ATEP, Inc to provide this analysis. I can be reached at 503-364-5066 if you have questions or would like additional information.


 Karl Birky, PE, PTOE

