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503-588-6173

REVISED DECISION OF THE HEARINGS OFFICER

CLASS 2 WILLAMETTE GREENWAY DEVELOPMENT PERMIT / CLASS 2 SITE PLAN REVIEW / TREE AND VEGETATION REMOVAL PERMIT CASE NO.: WGP-SPR-TRP25-01

APPLICATION NO.: 25-107171-PLN

NOTICE OF DECISION DATE: July 30, 2025

REQUEST: A consolidated application for site alterations and tree and vegetation removal associated with the establishment of temporary construction and staging areas required for proposed seismic improvements to the Center Street Bridge; and the future repaving of a portion of the north parking lot of Riverfront Park following its use as a temporary construction staging area.

The application includes:

- 1) A Class 2 Willamette Greenway Development Permit for the required site alterations and tree and vegetation removal associated with the establishment of the temporary construction and staging areas needed to support the proposed seismic improvements to the Center Street Bridge;
- 2) A Class 2 Site Plan review for the future repaving of a portion of the north parking lot of Riverfront Park following its use as a temporary construction staging area; and
- 3) A Tree and Vegetation Removal Permit for the removal of vegetation within the riparian corridor of the Willamette River necessary to establish the temporary construction and staging areas for the proposed bridge work.

The subject properties are zoned PA (Public Amusement), CB (Central Business District), and WSCB (West Salem Central Business District), are within the Willamette Greenway Overlay Zone, and located at 314 Water Street NE, 116 Marion Street NE, and the 200 to 300 Blocks of Musgrave Lane NW (Marion County Assessor Map and Tax Lot Numbers: 073W22CD00400, 073W22CD00401, 073W22CD00500, 073W22CD00600, 073W22CD00700, & 073W22DB07400; and Polk County Assessor’s Map and Tax Lot Numbers: 073W22C00100 & 073W22C00101).

APPLICANT: Andrew Walker, ODOT

LOCATION: 314 Water St NE and 116 Marion St NE, Salem OR 97301; and 200 to 300 Blocks of Musgrave Ln NW, Salem OR 97304

CRITERIA: Salem Revised Code (SRC) Chapters 600.015(e)(2) – Willamette Greenway Development Permit; 220.005(f)(2) – Class 2 Site Plan Review; 808.030(d) – Tree and Vegetation Removal Permit

FINDINGS: The findings are in the attached Decision dated July 30, 2025

DECISION: The **Hearings Officer APPROVED** Class 2 Willamette Greenway Development Permit, Class 2 Site Plan Review and Tree and Vegetation Removal

NOTICE OF DECISION

PLANNING DIVISION
planning@cityofsalem.net



Permit Case No. WGP-SPR-TRP25-01 subject to the following conditions of approval:

- Condition 1:** Prior to establishing the proposed temporary construction staging areas for the bridge, the applicable temporary use permit approvals under SRC Chapter 701 shall be obtained.
- Condition 2:** Prior to construction, provide a final Geotechnical Report signed and stamped by the project's Certified Engineering Geologist (CEG) and Engineer (PE).
- Condition 3:** The Salem Fire and Public Works Department shall be given the opportunity to review and comment on the emergency response plan required by Section 311 of the Clean Water Act prior to its adoption.
- Condition 4:** Design and construct a storm drainage system at the time of development in compliance with Salem Revised Code (SRC) Chapter 71 and Public Works Design Standards (PWDS).
- Condition 5:** Prior to any excavation, grading, or construction, a survey map, certified by a licensed Professional Land Surveyor, shall be submitted to the Director showing the Willamette Greenway Boundary and its relationship to the site and survey monuments thereon.
- Condition 6:** Prior to any excavation, grading, or construction, plans for removal and replacement of any native vegetation shall be submitted to and approved by the Director.

The rights granted by the attached decision must be exercised, or an extension granted, by the following dates, or this approval shall be null and void:

Class 2 Willamette Greenway Development Permit:	<u>August 15, 2025 2027</u>
Class 2 Site Plan Review:	<u>August 15, 2029</u>
Tree and Vegetation Removal:	<u>No expiration date</u>
Application Deemed Complete:	<u>July 1, 2025</u>
Public Hearing Date:	<u>July 23, 2025</u>
Notice of Decision Mailing Date:	<u>July 30, 2025</u>
Decision Effective Date:	<u>August 15, 2025</u>
State Mandate Date:	<u>October 29, 2025</u>

Case Manager: Bryce Bishop, bbishop@cityofsalem.net, 503-540-2399

This decision is final unless written appeal and associated fee (if applicable) from an aggrieved party is filed with the City of Salem Planning Division, in person at 440 Church St SE, Salem OR 97312, by mail P.O. Box 14300 Salem, OR 97309, or by email at planning@cityofsalem.net, no later than 5:00 p.m., Thursday, August 14, 2025. Any person who presented evidence or testimony at the hearing may appeal the decision. The notice of appeal must contain the information required by SRC 300.1020 and must state where the decision failed to conform to the provisions of the applicable code section, SRC Chapters 220, 600 and 808. The appeal fee must be paid at the time of filing. If the appeal is untimely and/or lacks the proper fee, the appeal will be

rejected. The Planning Commission will review the appeal at a public hearing. After the hearing, the Planning Commission may amend, rescind, or affirm the action, or refer the matter to staff for additional information.

The complete case file, including findings, conclusions and conditions of approval, if any, is available for review by contacting the case manager, or at the Planning Desk in the Permit Application Center, 440 Church St SE, Salem, during regular business hours.

<http://www.cityofsalem.net/planning>

Attachments:

- A. Hearings Officer Decision
- B. Vicinity Map
- C. Proposed Plans

**CITY OF SALEM
BEFORE THE HEARINGS OFFICER**

A CONSOLIDATED APPLICATION FOR A)
CLASS 2 WILLAMETE GREENWAY)
PERMIT, A CLASS 2 SITE PLAN REVIEW,)
AND A TREE AND VEGETATION)
REMOVAL PERMIT FOR SEISMIC)
BRIDGE IMPROVEMENTS LOCATED AT)
314 WATER STREET NE, 116 MARION)
STREET NE, AND THE 200 TO 300)
BLOCKS OF MUSGRAVE LANE NW.)
SALEM, OREGON.)
_____)

WGR-SPR-TRP25-01

FINDINGS OF FACT, CONCLUSIONS,
AND DECISION

DATE AND PLACE OF HEARING:

On July 23, 2025, at 5:30 p.m., a properly noticed hearing was held before the City of Salem Hearings Officer at the Community Room, Salem Police Department, 333 Division Street NE, Salem, Oregon.

APPEARANCES:

Staff: Bryce Bishop, Planner III

Neighborhood Association: Central Area Neighborhood Development Association of Neighbors and the West Salem Neighborhood Association

Proponents: Oregon Department of Transportation

Appellants:

SUMMARY OF THE APPLICATION AND HEARING

BACKGROUND

The City of Salem held a duly authorized and noticed the appeal hearing on July 23, 2025, regarding the Applicant’s request. During the hearing, Bryce Bishop requested that the staff report be entered into the record, and the Hearings Officer granted the request.

On March 1, 2021, a consolidated application for Class 2 Willamette Greenway Development Permit, Class 2 Site Plan Review, and Tree and Vegetation Removal Permit was submitted by Mariah Mitchell, of DOWL, LLC, on behalf of the applicant, the Oregon Department of Transportation. Because multiple land use applications are required in connection with the proposal, they have been

consolidated and are being processed together as one pursuant to SRC 300.120(c). The applications are associated with the establishment of temporary construction and staging areas required for proposed seismic improvements to the Center Street Bridge; and the future repaving of a portion of the north parking lot of Riverfront Park following its use as a temporary construction staging area. The application was deemed complete on July 1, 2025. The 120-day state mandated local decision deadline for the application is October 29, 2025.

The Hearing Notice was provided on July 3, 2025, to surrounding property owners and tenants pursuant to Salem Revised Code (SRC) and stated that the date for the hearing was July 23, 2025. The property was posted on July 10, 2025, consistent with the requirements of SRC 300.620(b)(3) for a Type III application.

FINDINGS OF FACT AND CONCLUSIONS

1. Salem Area Comprehensive Plan (SACP) designation

The subject properties are located inside the Salem Urban Growth Boundary and the corporate city limits. The properties are designated “Parks, Open Space, and Outdoor Recreation”, “River-Oriented Mixed-Use”, and “Central Business District” on the Salem Area Comprehensive Plan (SACP) Map. The comprehensive plan map designations of surrounding properties are as follows:

East Side of the Willamette River

North: Across Marion Street NE, River-Oriented-Mix Use

South: Parks, Open Space, and Outdoor Recreation

East: Across Front Street NE, Central Business District, River-Oriented Mixed Use

West: Willamette River

West Side of the Willamette River

North: Parks, Open Space, and Outdoor Recreation

South: Willamette River

East: Willamette River

West: Central Business District

2. Zoning and Surrounding Land Uses

The subject property is zoned MU-R (Mixed-Use Riverfront) and partially within the Willamette Greenway Overlay Zone. The zoning of surrounding properties is as follows:

East Side of the Willamette River

North: Across Marion Street NE, (CB) Central Business District with Willamette River Greenway Overlay Zone

South: PA (Public Amusement) with Willamette River Greenway Overlay Zone

East: Across Front Street NE, (CB) Central Business District

West: Willamette River

West Side of the Willamette River

North: PA (Public Amusement) with Willamette River Greenway Overlay Zone

South: Willamette River

East: Willamette River

West: WSCB (West Salem Central Business District)

2. Site Analysis

The properties included with the proposal consist of land that is adjacent to and within close proximity to the Center Street Bridge within Wallace Marine Park, Riverfront Park, and a portion of privately owned property located at 297 Musgrave Lane NW.

3. Neighborhood and Citizen Comments

The subject properties are located within the boundaries of both the Central Area Neighborhood Development Organization (CAN-DO) Neighborhood Association and the West Salem Neighborhood Association. Pursuant to SRC 300.310(b)(1), both the CAN-DO Neighborhood Association and the West Salem Neighborhood Association were contacted by the applicant; and the applicant subsequently presented information about the project at meetings of both neighborhood associations, thereby satisfying the requirements of SRC 300.310.

A representative of the West Salem Neighborhood Association testified that the Association supported the approval of the consolidated application but proposed two conditions of approval. The first condition was that the applicant prepare an emergency response plan to address spill prevention, control and countermeasures and that the plan be reviewed and approved by the Salem Fire Department and the Salem Public Works Department prior to any excavation, grading or construction. This request will be addressed under the analysis of SRC 600.025(c) Water Quality, below.

A second request from the Association was that, prior to excavation, grading, or construction, a final signed agreement and dollar amount be approved specifying the dollar amount to reimburse the City of Salem for the value of trees removed by the project. However, it was pointed out that the representatives of the City of Salem Parks Department had been working with representatives of ODOT for about two years on this issue and were very close to establishing a reimbursement value. Further, it was noted that

this issue would be addressed in an intergovernmental agreement that would be brought before the Salem City Council for public review and Council approval. I do not think a condition of approval mandating a process that is currently near fruition is necessary as construction is not scheduled to begin until Spring of 2026.

The subject property is not located within a Homeowners' Association (HOA); therefore, HOA notice required by SRC 300.620(b)(2)(B)(vi) is not applicable.

At the mailing of the staff report on this matter, no public comments had been received.

4. City Department and Public Agency Comments

The Salem Building and Safety Division reviewed the proposal and indicated no objections to the proposal.

The Salem Fire Department reviewed the proposal and recommended that emergency agencies that will need to utilize this route be notified of the project and possible impacts to emergency response.

The Salem Development Services Division reviewed the proposal and provided comments pertaining to City infrastructure required to serve the property.

No comments from public agencies were received as of the date of completion of the staff report.

5. Analysis of Class 2 Willamette Greenway Development Permit Approval Criteria

Pursuant to SRC 600.015(a)(1), no intensification, change of use, or development within the Willamette Greenway Overlay zone shall occur unless a Greenway Development permit has been issued. Any intensification, change of use, or development proposed within the compatibility review boundary of the Willamette Greenway Overlay Zone requires a Class 2 Greenway Development Permit.

Finding 1: Properties included in the application are located within the Willamette Greenway Overlay Zone and the Willamette Greenway compatibility review boundary. Because the proposal includes site alterations and tree and vegetation removal within these portions of the site, a Class 2 Willamette Greenway Development Permit is required pursuant to SRC 600.015(a)(1) & (b)(2). SRC Chapter 600.015(e)(2) sets forth the following criteria that must be met before approval can be granted to a Class 2 Willamette Greenway Development Permit:

SRC 600.015(e)(2)(A): The proposed intensification, development, or change of use is consistent with:

(i) The Willamette River Greenway Plan;

Finding 2: Conformance with all applicable standards in SRC Chapter 600 establishes consistency with the Greenway Plan. As identified in this decision, the proposal conforms to the applicable standards of SRC Chapter 600 and is therefore consistent with the Greenway Plan.

(ii) The Willamette River Riparian Buffer Enhancement Guide;

Finding 3: Due to the necessary establishment of temporary construction and staging areas on both sides of the Willamette River adjacent to the Center Street Bridge, tree and vegetation removal and temporary site alterations are proposed within the riparian buffer of the Willamette River Greenway. Sheets FA01 through FA06 of the applicant's plan set establish how the affected areas will be restored and revegetated with native grasses, shrubs, and trees following their use for construction and staging. As identified in the applicant's proposed plans, the riparian buffer of the Willamette Greenway will be revegetated in conformance with the specifications included in the Willamette Greenway Buffer Enhancement Guide. This approval criterion is met.

(iii) The applicable standards of this chapter; and

The proposal, as recommended to be conditioned, must comply with the applicable standards of the Willamette Greenway Overlay Zone, as required by this approval criterion, as follows:

SRC 600.020(a) Uses

Uses and activities excepted from a Greenway Development Permit under SRC 600.015(a)(2), riparian restoration and enhancement activities and water-dependent and water-related uses and activities are permitted within the riparian buffer of the Willamette Greenway Overlay Zone.

Finding 4: The proposal includes tree and vegetation removal and temporary site alterations that are required in order to carry out needed seismic improvements to the Center Street Bridge. As identified earlier in this decision, the properties included in the proposal are zoned a combination of PA (Public Amusement), CB (Central Business District), and WSCB (West Salem Central Business District). Because the proposed construction and staging areas associated with this proposal are temporary in nature and only needed during the construction work on the bridge, they are classified as Temporary Uses under SRC Chapter 701 and are allowed within the PA, CB, and WSCB zones with the approval of temporary use permits for construction storage yards and construction modulars. Because construction storage yards and construction modulars are allowed within the underlying zones they are also allowed within the Willamette Greenway Overlay Zone.

Finding 5: SRC 600.015(a)(2)(L) provides that the proposed seismic improvements to the Center Street Bridge are exempt from a Willamette Greenway Development permit because they represent maintenance of a structure in existence prior to June 9, 2004, and are necessary to extend its life and ensure its seismic and structural stability. The proposed seismic improvements to the bridge are exempt from a Willamette Greenway Development permit under SRC 600.015(a)(2)(L). Because the temporary construction and staging areas within the riparian buffer of the Willamette Greenway are incidental, accessory to, and necessary in order to carry out the proposed maintenance to the bridge, which is otherwise exempt from a Willamette Greenway permit and therefore an allowed use within the riparian buffer pursuant to SRC 600.020(1), the temporary construction and staging areas included with this proposal in support of that work are also allowed within the riparian buffer.

Finding 6: SRC 600.015(a)(2)(I) also exempts the improvement of a public park, in accordance with an officially approved master plan and the setbacks requirements of SRC 600, from a Willamette Greenway Development Permit. As a result of the proposed use of a portion of the north parking lot of Riverfront Park as a temporary construction staging area, the parking lot will be damaged and in need of repair following completion of the project. As such, a Class 2 Site Plan Review has been included in the proposal for the necessary future repair of the parking lot. Because the parking lot will be restored to its pre-existing condition in conformance with the Riverfront Park Master Plan, the proposed parking lot improvements are exempt from a Willamette Greenway Development Permit and any portion of the parking lot within the riparian buffer is therefore allowed pursuant to SRC 600.020(a)(1).

In order to ensure that the proposed temporary construction staging areas receive their required temporary use permit approvals under SRC Chapter 701, the following condition of approval is required:

Condition 1: Prior to establishing the proposed temporary construction staging areas for the bridge, the applicable temporary use permit approvals under SRC Chapter 701 shall be obtained.

SRC 600.025(a) General Standards

This provision requires the following:

- (1) ***Existing predominant topographical features of the bank and escarpment shall be preserved and maintained, with the exception of disturbance necessary for:***
 - (A) ***The construction or establishment of a water-related, water-dependent, or river-oriented use or activity; and***
 - (B) ***Measures necessary to reduce existing or potential bank and escarpment erosion, landslides, or flood hazard conditions.***
- (2) ***The slope, soil characteristics, and other physiographic conditions***

existing within the land area between the ordinary low water line and the Willamette Greenway Boundary shall be considered to assure that the proposed intensification, development, or change of use will not adversely affect the stability of the land area.

- (3) *The hydraulic effect of the Willamette River on the bank shall be considered in the design of any proposed intensification, development, or change of use.*
- (4) *The hydraulic and flood carrying capacity of the river shall be considered in the design of any proposed intensification, development, or change of use.*
- (5) *Impact on the riparian buffer resulting from the proposed intensification, development, or change in use shall be minimized.*

Finding 7: SRC 600.015(d)(5) requires a report by a certified engineering geologist or geotechnical engineer demonstrating that the standards specified in SRC 600.025(a)(2) have been met. This report ensures that the proposed development will not adversely affect the stability of the land area between the ordinary low water line and the Willamette Greenway Boundary. A Geotechnical Engineering Report, prepared by GRI, and dated November 27, 2023, was submitted to the City of Salem. However, the report is labeled “draft” and does not contain the stamps of the Certified Engineering Geologists and Professional Engineers who prepared the report. In order to ensure that the geotechnical engineering report submitted for the project complies with the requirements of SRC 600.015(d)(5), the following condition of approval is required:

Condition 2: Prior to construction, provide a final Geotechnical Report signed and stamped by the project’s Certified Engineering Geologist (CEG) and Engineer (PE).

Finding 8: SRC 600.015(d)(6) requires a report by a registered professional engineer detailing the hydraulic and flood carrying capacity of the river. This ensures that development standards of SRC 600.025(a)(3) and SRC 600.025(a)(4) are met. Specifically, the report shall ensure that the hydraulic effect of the Willamette River on the bank are considered and that the flood carrying capacity of the river is not negatively impacted. The applicant has submitted a Hydraulic and Hydrologic Report and No–Rise Certification which demonstrates that the proposed development will not have a hydrologic impact on the Willamette River.

Conclusion: The proposed development is in conformance with the standards in SRC 600.025(a).

SRC 600.025(b) Landscaping

- (1) Landscaping shall conserve, or if disturbed by the development activity restore to the greatest extent possible, vegetative cover within the Willamette Greenway Boundary. Landscaping is not required where it would significantly interfere with a water-dependent or water-related use or activity.***

Finding 9: Existing vegetation will be removed within the Willamette Greenway Boundary in order to accommodate the proposed temporary construction staging areas for the bridge. Vegetation proposed for removal will be replaced at a 1:1 ratio; therefore, conforming to the landscaping requirements of SRC 600.025. The proposal conforms to this standard.

- (2) Native vegetation removed from the riparian buffer shall be replaced with native vegetation which is compatible with and enhances the functions of the riparian buffer.***

Finding 10: The proposed revegetation plan requires that native vegetation removed from the riparian buffer will be replanted with native species selected from the City's Tree and Vegetation Technical Manual. The proposal conforms to this standard.

- (3) Trees and shrubs shall be provided as follows:***
 - (A) A minimum of one tree shall be provided for every 20 feet of river frontage.***
 - (B) A minimum of one shrub shall be provided for every two feet of river frontage.***
 - (C) All trees and shrubs shall be planted within and generally riverward of the Willamette Greenway Boundary.***
 - (D) The planting standards included under subsections (b)(3)(A) and (B) of this section are for calculation purposes only, and do not require linear planting. Groupings of trees, shrubs, or both are encouraged, particularly along the riverbank.***

Finding 11: One tree will be planted for every 20 feet of river frontage in conformance with this standard. On the west side of the river 54 trees are proposed for removal (*19 within the riparian corridor and 35 outside the riparian corridor*). On the west side of the river, 66 trees are proposed for removal (*16 in the riparian corridor and 50 outside the riparian corridor*). The proposed revegetation plans identify a total of 263 to be replanted on the western side of the river and 168 trees to be replanted on the eastern side of the river.

The proposal also includes the replanting of shrubs, as shown on the revegetation plans, interspersed throughout the planting areas on the eastern and western sides of the river in conformance with the planting density established by this standard.

All of the trees and shrubs proposed to be replanted in the project area are located riverward of the Willamette Greenway Boundary and are arranged in a manner so as to be grouped together, where possible, in density quantities along the riverbank. The proposal conforms to these standards.

(4) Areas which are not paved or revetted shall be planted with living ground cover

Finding 12: Living ground cover is proposed in all areas of the project area that are not paved or revetted. The proposal conforms to this standard.

SRC 600.025(c) Water Quality

SRC 600.025(c)(1) requires the establishment of a riparian buffer and SRC 600.025(c)(2) provides two methods of establishing a riparian buffer boundary. SRC 600.025(c)(3) identifies various mitigation measures that may be used to mitigate any intensification, development or change of use within the riparian buffer.

Finding 13: The riparian buffers on the eastern and western sides of the Willamette River were established based on the relative slopes of the embankments, the extent of the floodway boundary, and Riparian Buffer Method 1 established under SRC 600.025(c)(2) – Table 600–1. Riparian Method 1 allows for the establishment of a uniform riparian buffer boundary based on three bank slope measurements; one taken along the river, one at each property line, and one located at the center of the property. The bank slopes on both sides of the river exceed 25 percent; thereby resulting in a minimum required riparian buffer width of 75 feet. Because the floodway boundary on the east side of the river doesn't exceed the minimum 75-foot-wide riparian buffer width, the required riparian buffer on the east side of the river is 75 feet, as shown on the plans. On the west side of the river, however, the floodway boundary extends beyond the minimum 75-foot buffer and therefore, pursuant to SRC 600.025(c)(2) – Table 600–1, the riparian buffer on the west side of the river is required to extend to the floodway boundary.

Finding 14: In order to conform to the water quality mitigation requirements of SRC 600.025(c), mitigation measure B – Riparian Buffer Enhancement is proposed to be utilized on both the eastern and western sides of the river. In addition, because the floodway boundary on the western side of the river extends beyond the 125-foot wider riparian buffer mitigation measure established under SRC 600.025(c)(3)(A), the proposal also satisfies mitigation measure A – Wider Riparian Buffer, on the western side of the river.

Finding 15: The proposed landscaping within the riparian buffer will conform to the riparian buffer enhancement requirements of SRC 600.025(c)(3)(B) through the following measures:

- Plant quantities will be replaced at a 1:1 ratio to comply with City and DSL replacement requirements; therefore, exceeding the minimum planting enhancement requirements established under SRC 600.025(c)(3)(B)(i);
- The entire escarpment and outer sections will be landscaped with intermixed trees and shrubs to comply with the Willamette Greenway landscaping standard of one tree per 20 feet of river frontage and one shrub per two feet of river frontage, as well as the Department of State Lands (DSL) requirement of one live tree/shrub per six linear feet; thereby providing planting quantities that exceed the 60 percent tree and shrub area required under the Willamette Greenway Enhancement Guide; and
- All plantings will be a native species selected from the City's Tree and Vegetation Technical Manual.

Finding 16: As noted above, the West Salem Neighborhood Association requested a condition that the applicant prepare an emergency response plan to address spill prevention, control and countermeasures and that the plan be reviewed and approved by the Salem Fire Department and the Salem Public Works Department prior to any excavation, grading or construction. The applicant's representative responds that Section 290 of Oregon Standard Specifications for Construction will be part of the contractor contract to require a Spill Prevention, Control and Countermeasure Plan (SPCC Plan) and contingency plan for spills. He notes that DEQ, which oversees compliance with the Clean Water Act, reviews these plans and DEQ will be notified of any spills.

It should be noted that on the west bank of the Willamette River, the applicant proposes three staging areas; two in Wallace Marine Park and one in Musgrave Lot. Of significance is that all of these staging areas are located within the floodway and that construction is projected to last from Spring of 2026 to possibly as late as mid-2029. The important emphasis of the Association's request is not that there be a SPCC plan but rather that the formation of that plan be coordinated with the City's public works department and the fire department; two agencies that will be primary responders in many spill situations. While I do not believe the Hearings Official has the authority to require approval of the plan by those agencies, I believe it is reasonable to at least require that they be given the opportunity to review the plan prior to its adoption. To that degree, the request is reasonable and I support a condition of approval that encapsulates this intent.

Condition 3: The Salem Fire and Public Works Department shall be given the opportunity to review and comment on the emergency response plan required by Section 311 of the Clean Water Act prior to its adoption.

Conclusion: The proposed development is in compliance with the water quality standards of SRC 600.025(c).

SRC 600.025(d) Structures

Finding 17: SRC 600.025(d) requires that all buildings, structures, and exterior mechanical equipment shall be screened, colored, or surfaced so as to blend with the riparian area. Colors must be natural earth or leaf tones. surfaces must be non-reflective, and that screening must be sight-obscuring. As proposed, no buildings, structures, fences, walls, or mechanical equipment are included with the proposal; and all development activity is limited to tree and vegetation removal and repaving a portion of the north parking lot of Riverfront Park. As such, this standard is not applicable to the proposal.

SRC 600.025(e) Lighting

Finding 18: Among other provisions, this standard requires that lighting not flash, be visible from the Willamette River, and should not be focused or oriented onto the surface of the river. The maximum aggregate intensity of all lighting falling onto the surface of the Willamette River must not exceed one-tenth foot-candle per square feet and no red or green lights can be visible from the river.

No lighting is proposed over the Willamette River although it will be provided in the repaved portion of the Riverfront Park north parking lot in order to replace the existing parking lot lighting that will be removed as a result of the parking lot being used for temporary construction staging. The proposed replaced parking lot lighting will not include red or green lights, flash, be focused onto the surface of the river, or exceed an aggregate intensity of one-tenth foot-candle per square feet. The proposal therefore conforms to this standard.

SRC 600.025(f) Screening of Parking and Unenclosed Storage Areas

Finding 19: This standard requires that parking, loading and unenclosed storage areas be screened from the Willamette River and from adjacent properties by a sight-obscuring berm or a sight-obscuring hedge, a minimum of six feet in height at maturity. Hedges must be no less than three feet in height at planting and be of a species capable of attaining a minimum height of six feet within three years after planting.

The proposal includes the repaving a portion of the north parking lot of Riverfront Park following its use as a temporary construction staging area for the proposed

seismic improvements to the Center Street Bridge. However, as provided under SRC 600.015(a)(2)(I), improvement of a public park in accordance with an officially approved master plan is specifically exempt from a Willamette Greenway Development Permit. As such, this standard is not applicable to the proposed parking lot repaving.

SRC 600.025(g) View Corridors

Finding 20: This provision addresses right-of-way located wholly or partially within the Willamette Greenway Overlay Zone. The proposal does not include any right-of-way that is wholly or partially within the Willamette Greenway Overlay Zone that is planned to be vacated as part of this proposal. These standards are therefore not applicable.

SRC 600.025(h) Public Access

Finding 21: This provision requires, when practical, that public access to and along the Willamette River be provided by easement, dedicated right-of-way, or by other appropriate means. This standard is not applicable as currently there are multiple paths through the Riverfront Park and Wallace Marine Park. Additional access is not currently planned by the Salem Transportation System Plan or the Parks System Master Plan.

(iv) Where applicable, the stormwater runoff water quality standards adopted and administered by the Public Works Department.

Finding 22: The public works design standards require stormwater management and treatment through the use of green stormwater infrastructure for projects involving 5,000 square feet or more of new and/or replaced impervious surfaces. For the replaced parking area within the project scope, the applicant's engineer submitted a preliminary stormwater management report demonstrating compliance with Stormwater PWDS Appendix 004- E(4) and SRC Chapter 71. The preliminary stormwater design demonstrates the use of green stormwater infrastructure to the maximum extent feasible. To ensure that stormwater runoff water quality standards are met, as required by SRC Chapter 71 and SRC 600.025(h), the following condition of approval is recommended:

Condition 4: Design and construct a storm drainage system at the time of development in compliance with Salem Revised Code (SRC) Chapter 71 and Public Works Design Standards (PWDS).

The applicant's preliminary plans show that temporary construction storage yards will be established. As identified in the revegetation plans submitted by the applicant, the temporary construction storage yards will be restored with vegetation following completion of the project in order to comply with applicable standards. As

such, stormwater management is not required for the temporary construction storage yards.

SRC 600.015(e)(2)(B): The proposed intensification, development, or change of use complies with all applicable development standards of the UDC.

Finding 23: The findings of fact in support of the Class 2 site plan review and the vegetation removal permit, with conditions of approval, comply with all applicable standards of the UDC.

SRC 600.015(e)(2)(C): The proposed intensification, development of change of use will, to the greatest extent possible, provide the maximum possible landscaped area, open space, or vegetation.

Finding 24: The proposed revegetation plan shows that trees and vegetation that are required to be removed to accommodate the temporary staging areas for the seismic improvements to the bridge will be replaced at a 1:1 ratio in compliance with the landscaping requirements established by the Oregon Department of State Lands. This approval criterion has been met.

SRC 600.0015(f) Conditions of Approval

Finding 25: This provision requires that each Greenway Development Permit include the following conditions:

Condition 5: Prior to any excavation, grading, or construction, a survey map, certified by a licensed Professional Land Surveyor, shall be submitted to the Director showing the Willamette Greenway Boundary and its relationship to the site and survey monuments thereon.

Condition 6: Prior to any excavation, grading, or construction, plans for removal and replacement of any native vegetation shall be submitted to and approved by the Director

5. Analysis of Class 2 Site Plan Review Approval Criteria

Pursuant to SRC 220.005(b)(2), Class 2 Site Plan Review is required for any development that requires a building permit, other than development subject to Class 1 Site Plan Review, and that does not involve a land use decision or limited land use decision, as those terms are defined in ORS 197.015.

Because the proposal includes repaving a portion of the north parking lot of Riverfront Park following its use as a temporary construction staging area for the Center Street Bridge seismic improvements, Class 2 Site Plan Review is required for this specific portion of the proposal. SRC 220.005(f)(2) sets forth the

following criteria that must be met before approval can be granted to an application for Class 2 Site Plan Review.

SRC 220.005(f)(2)(A): Only clear and objective standards which do not require the exercise of discretion or legal judgment are applicable to the application.

Finding 26: This standard shall be applied to the approval criteria of SRC 220.005(f)(2)(B) below.

SRC 220.005(f)(2)(B): The application meets all the applicable standards of the UDC.

SRC 540.005 Uses

Finding 27. As noted above, the Class 2 Site Plan Review applies only to the portion of the north parking lot of the Riverfront Park. The park parking lot is located within the PA (Public Amusement) zone. Under the City's Use Classification Chapter (SRC 400), parks are classified as a Parks and Open Space use per SRC 400.060(e). The allowed uses within the PA zone are established under SRC 540.005 – Table 540–1. Within the PA zone Parks and Open space are allowed as a permitted use. Because Parks and Open Space is a permitted use in the PA zone, the parking lot serving the park is also permitted.

SRC 540.010(a) Standards

Finding 28: SRC 540.010(a), Table 540–2 establish lot standards. This standard is inapplicable as the proposed repaving of the existing parking lot does not make a change to the configuration of existing lots that make up the site.

SRC 540.010(b) Setbacks

Finding 29: SRC 540.010(b), Tables 540–3 and 540–4, establish setbacks of buildings, accessory structures, and off–street parking and vehicle use areas within the PC zone. The parking lot detail plan and the proposed revegetation plan for the parking lot comply with the applicable perimeter setbacks of the PA zone, which are a minimum of 6 to 10 feet from abutting streets and a minimum of 5 feet with Type A landscaping for the interior, front, side and interior rear portions of a lot.

SRC 540.010(c) Height

Finding 30. Because the proposal does not include the construction of any new buildings or accessory structures, the maximum height requirements of the PA zone are not applicable to the proposal.

SRC 540.010(d) Landscaping

Finding 31. SRC 540.010(d) provides that required setbacks shall conform to the standards of SRC Chapter 807 and vehicle use areas shall be landscaped to conform to the standards of SRC Chapters 806 and 807. The parking lot detail plan and the revegetation plan show that all setback areas are landscaped as required by SRC Chapter 807.

The portion of the existing parking lot to be repaved is about 30,094 square feet in area. SRC 806.035(d)(2) establishes interior landscaping requirements for parking areas greater than 5,000 square feet in area. Based on the size of the repaved portion of the parking area, a minimum of five percent (*1,505 square feet*) of interior parking lot landscaping is required to be provided. As measured on the parking lot detail plan and the revegetation plan for the parking lot, a total of approximately 3,068 square feet of interior parking lot landscaping is provided which exceeds the minimum five percent interior parking area landscaping requirement of SRC 806.035(d)(2).

Finding 32: In addition to requiring a minimum square footage of interior parking lot landscaping, SRC 806.035(d)(3) requires a minimum of one deciduous shade tree to be planted within a parking area for every 12 parking spaces. SRC 807.005 defines a shade tree as, "...a deciduous tree, or, in rare occasions, an evergreen tree, planted primarily for its high crown of foliage or overhead canopy."

Based on the 90 off-street surface parking spaces proposed within the repaved portion of the parking lot, a minimum of eight deciduous shade trees are required. As shown on the revegetation plan for the parking lot, a total of 15 deciduous shade trees are proposed within the off-street parking area, thereby complying with the interior parking area landscaping requirements of SRC 806.035(d)(3).

Finding 33: SRC 806.035(d)(4) requires landscape islands and planter bays within off-street parking areas to be a minimum of 25 square feet in size with a minimum width of five feet. The parking lot detail plan and the revegetation plan for the parking lot demonstrate that all of the proposed parking lot planter bays provided within the interior of the repaved portion of the parking lot conform to the minimum size and dimension requirements of SRC 806.035(d)(4).

Conclusion: The proposal conforms to the landscaping requirements of the PA zone.

SRC CHAPTER 71 – STORMWATER

SRC 71.045 Applicability

Finding 34: SRC 71.045 states that SRC Chapter 71 applies to all projects, all discharge to a public or private stormwater system, all discharges into receiving water within or contiguous to the City's limits, all real property, and anyone engaging on any activity on or occupying property. The proposal is therefore subject to SRC Chapter 71 and the Public Works Design Standards that require the use of

Green Stormwater Infrastructure (GSI) to treat and detain stormwater generated from the development. The proposed plan demonstrates compliance with PWDS Appendix 4E related to green stormwater infrastructure by setting aside at least ten percent of the total new impervious surface area for installation of green stormwater infrastructure. The application has been conditioned under the Willamette Greenway Permit to ensure that stormwater management facilities are designed and constructed in accordance with the Public Works Design standards at time of construction.

SRC CHAPTER 200 – URBAN GROWTH MANAGEMENT

SRC 200.025 Urban Growth Preliminary Declaration

Finding 35: This provision requires the issuance of an Urban Growth Preliminary Declaration (UGA) prior to development of property located outside the City’s Urban Service Area. Since the subject property is located inside the Urban Service Area and adequate facilities are available, no Urban Growth Area permit is required.

SRC CHAPTER 230 – HISTORIC PRESERVATION

SRC 230.105 Preservation of archeological resources

Finding 36: SRC 230.105 provides that archeological resources shall be protected and preserved in place and that a person may not excavate, injure, destroy or alter an archaeological site or object or remove an archaeological object located on public or private lands unless that activity is authorized by a permit. The proposed project is within Salem’s Cultural Resource Protection Zone and there are multiple known archaeological sites (both historic and precontact), in close proximity to the project area.

The applicant will be required to demonstrate compliance with SRC 230.105 through Archaeological Historic Clearance approval per Salem #APP 3.20 demonstrating there will be no adverse effect to archaeological resources as a result of the ground disturbance proposed during redevelopment of the site. The clearance review will require an archaeological survey of the project area, as well as an Oregon State Archaeological Permit, archaeological monitoring plan and an Inadvertent Discovery Plan. Prior to issuance of any City permits authorizing ground disturbing activity, evidence shall be provided of completed Archaeological Historic Clearance and completed consultation with the Tribes and the State Historic Preservation Office (SHPO).

SRC CHAPTER 300 – WILLAMETTE GREENWAY

SRC 600.015(a)(2)(I) Willamette Greenway development permit

Finding 37: SRC 600.015(a)(2)(I) provides that improvement of a public park made in accordance with an officially adopted master plan is exempt from having to have a Willamette Greenway development permit. Because the parking lot will be

repaved in conformance with the Riverfront Park Master Plan, the repaving of the parking lot does not trigger a Willamette Greenway Development Permit.

SRC CHAPTER 800 – GENERAL DEVELOPMENT STANDARDS

SRC 800.065 Pedestrian Access

Finding 38: SRC 800.065(a)(3) provides that pedestrian access must be provided through off-street surface parking areas greater than 25,000 square feet in size or including four or more consecutive parallel drive aisles. The repaved portion of the parking lot is greater than 25,000 square feet in size so one pedestrian connection is required to be provided through the parking area in conformance with the requirements of SRC 800.065(a)(3). As shown on the proposed plans, a new east-west pedestrian connection is provided at the southern end of the repaved portion of the parking lot to meet the applicable pedestrian connectivity requirements of SRC 800.065. The pedestrian connection exceeds the minimum required 5-foot width and will be paved with concrete to visually distinguish it from the surrounding asphalt paved parking area as required under SRC 800.065(b)(1)(B). The proposal conforms to this standard.

SRC CHAPTER 602 – PUBLIC IMPROVEMENTS

SRC 801.015 Development to be served by city utilities

Finding 39: SRC 801.015 requires development to be served by City utilities designed and constructed according to applicable provisions of the Salem Revised Code and Public Works Design Standards. In this regard, the proposed development will be served by multiple existing public water mains, public sanitary sewer mains, and public stormwater mains within and abutting the development site. The existing infrastructure is available along the perimeter of the site and the applicant's preliminary utility plan shows it to be adequate to serve the property. The applicant shall design and construct all utilities (sewer, water, and storm drainage) according to the Public Works Design Standards (PWDS) and to the satisfaction of the Public Works Director. The applicant has been advised that a sewer monitoring manhole may be required.

SRC CHAPTER 803 – STREET AND RIGHT-OF-WAY IMPROVEMENTS

SRC 803.025 Right-of-way and pavement widths

Finding 40: Front Street NE has a right-of-way width of 170 feet and an improvement width of 120 feet and Water Street SE has a right-of-way width of 54 feet and an improvement width of 55 feet. SRC 803.025 states that except when otherwise provided, right-of-way and pavement widths for streets and alleys shall conform to the standards set out in Table 803-1 and 803-2, respectively. In addition, SRC 803.040 requires dedication of right-of-way for, and construction or improvement of, boundary streets up to one-half of the right-of-way and

improvement width specified in SRC 803.025 as a condition of approval for certain development.

The development site abuts Front Street NE along the eastern property boundary and Water Street NE terminates at the northern property boundary. Front Street NE is classified as a parkway street according to the Salem Transportation System Plan (TSP) and has adequate right-of-way and improvement width according to the standards set forth in Table 803-1 (Right-of-way Width) and Table 803-2 (Pavement Width). Water Street NE is classified as a local street according to the Salem TSP and has adequate pavement width but inadequate right-of-way according to the standards set forth in Table 803-1 (Right-of-way Width) and Table 803-2 (Pavement Width). The proposal does not involve a new building or building addition subject to boundary street improvements established in SRC 803.040(a); therefore, no right-of-way dedication or street improvements are required as a condition of development.

SRC CHAPTER 86 – TREES ON CITY OWNED PROPERTY

SRC 86.050 Activities requiring permits

Finding 41: SRC 86.050(a)(1) states that a permit is required for the removal of a City tree. A permit is required as the applicant proposes to remove trees within Riverfront Park and Wallace Marine Park. The applicant has applied for a separate permit for tree removals on City Owned Property (25-111725-TR) and has provided a replanting and mitigation plan as part of that application. Additional trees in the right-of-way and on the park property are not required, outside of that process.

SRC CHAPTER 804 – DRIVEWAY APPROACHES

Finding 42: SRC Chapter 804 establishes development standards for driveway approaches providing access from the public right-of-way to private property in order to provide safe and efficient vehicular access to development sites. The development site is served by an existing driveway approach onto Water Street NE. The existing driveway approach serving the site meets the applicable sections of SRC Chapter 804 and does not warrant modification.

SRC CHAPTER 806 – OFF-STREET PARKING, LOADING, AND DRIVEWAYS

SRC 806.015(a) Maximum off-street parking

Finding 43: Table 806-1 of SRC 806.015(a) establishes maximum off-street parking requirements. The maximum allowed off-street parking for Parks and Open Space is one space per 2,700 square feet of gross site area. Based on the approximate 22.28-acre size of Riverfront Park, a maximum of 359 off-street parking spaces are allowed. The proposed repaving of the parking lot does not increase the number of off-street parking spaces currently provided in the park. As such, the proposal does

not affect the maximum number of spaces allowed.

SRC 806.015(b) Compact parking

Finding 44: SRC 806.015(b) allows for the use of compact parking stalls to satisfy up to 75 percent of the parking spaces provided on a development site. The applicant's site plan shows 90 stalls in the repaved portion of the parking lot are required to be designated as compact spaces due to depth of the stalls being 18 feet rather than the minimum required 19-foot standard size stall depth. The proposed number of compact parking spaces does not exceed the maximum 75% allowed under SRC 806.015(b).

SRC 806.035(e) Off-street parking area dimensions

Finding 45: SRC 806.035(e), Table 806-5, establishes minimum dimensions for 90-degree off-street parking stalls and the drive aisles serving them. The proposed parking spaces and drive aisles within the repaved portion of the parking lot conform to the minimum parking stall and dimension requirements of SRC 806.035(e). However, because the depths of the stalls are 18 feet, they are required to be designated as compact spaces rather than standard size spaces.

SRC 806.035(f) Off-street parking area access and maneuvering

Finding 46: SRC 806.035(f) establishes access and maneuvering requirements for off-street parking areas. Off-street parking and vehicle use areas are required to be designed so that vehicles enter and exit the street in a forward motion with no backing or maneuvering within the street and, where a drive aisle terminates at a dead-end, a turnaround is provided that conforms to the dimensions set forth in Table 806-6. As shown on the parking lot detail plan, the repaved portion of the parking lot is designed so that there are no dead-end areas and a turnaround area is therefore not required. The proposal conforms to this standard.

SRC 806.035(n) Additional standards for new off-street surface parking areas more than one-half acre in size.

Finding 47: SRC 806.035(n) provides that when a total of more than one-half acre of new off-street surface parking is proposed on one or more lots within a development site, the lot(s) proposed for development shall comply with the additional standards in this subsection, including the provision of climate mitigation measures pursuant to SRC 806.035(n)(1) and provision of tree canopy pursuant to SRC 806.035(n)(2).

Because the repaved area will exceed one-half acre in size, the additional standards for new off-street surface parking areas more than one-half acre in size are applicable to the proposal. The applicant has chosen to provide increased tree canopy coverage, pursuant to SRC 806.035(n)(1)(C), which requires tree canopy coverage to be provided covering at least 40 percent of new off-street parking and

vehicle use areas in no more than 15 years. Provision of increased tree canopy coverage under SRC 806.035(n)(1)(C) also satisfies the provision of tree canopy requirement included under SRC 806.035(n)(2). As shown on the proposed tree canopy and soil volume analysis plan submitted by the applicant, the percentage of tree canopy and the amount of soil volume provided exceed the requirements of SRC 806.035(n).

SRC 806.040 Driveways

Finding 48: Pursuant to SRC 806.040(d), Table 806–8, one–way driveways are required to have a minimum width of 12 feet and two-way driveways are required to have a minimum width of 22 feet. The applicant’s parking detail plan shows that the proposed driveway at the northern end of repaved portion of the parking lot is a minimum of 22 feet in width and in conformance with the minimum driveway width standards of SRC 806.040.

SRC 806.045 Bicycle parking: when required

Finding 49: SRC 806.045(a)(1) provides that bicycle parking is required for all proposed new use or activity. Because the proposal includes only the repaving of a portion of the north parking lot of the park, and no new use, change of use, or intensification, expansion, or enlargement of the existing use is proposed, the bicycle parking requirements of SRC Chapter 806 are not triggered by the proposal.

SRC 806.075 Off–street loading areas

Finding 50: SRC 806.065(a) provides that off–street loading is required to be provided for each new use or activity, any change of use or activity, when such change of use or activity results in a greater number of required off–street loading space than the previous use or activity, or an intensification, expansion, or enlargement of a use or activity. Because the proposal includes only the repaving of a portion of the north parking lot of the park, and no new use, change of use, or intensification, expansion, or enlargement of the existing use is proposed, the off–street loading requirements of SRC Chapter 806 are not triggered by the proposal.

SRC CHAPTER 601 – FLOODPLAIN OVERLAY ZONE

SRC 601.045 Establishment of development permit

Finding 51: SRC 601.045(a) provides that a floodplain development permit is required before construction or development begins in any area horizontally within a Special Flood Hazard Area (SFHA). The subject property is designated on the Federal Emergency Management Agency floodplain maps as a Zone “AE” floodway. No new structures are proposed; however, the proposed activity required a Floodplain Development Permit in accordance with SRC Chapter 601. The applicant has applied for a separate Floodplain Development Permit (25–110722–FD), which is under review and will be issued prior to any ground disturbing activity within the floodplain.

SRC CHAPTER 808 PRESERVATION OF TREES & VEGETATION

Finding 52: SRC Chapter 808 forbids the removal of significant trees (SRC 808.015) and tree and vegetation within riparian corridors (SRC 808.020) without a tree and vegetation removal permit, a tree conservation plan or a tree variance. The applicant proposes to remove all of the existing trees within the parking area, including some of which are within the 75-foot-wide riparian corridor of the Willamette River in order to accommodate the use of that portion of the parking lot for temporary construction staging for the proposed seismic improvements to the Center Street Bridge. A tree and vegetation removal permit is not required, however, for the removal of a City tree, as defined under SRC 86.010, that is subject to the requirements of SRC Chapter 86 (SRC 808.030(a)(2)(C)). Under SRC Chapter 86, a City tree is defined as one that is located on City property, and City property means real property and improvements, if any, owned by, dedicated to, managed by, or subject to an easement in favor of, the City for the public's use, including, but not limited to, City parks, City rights-of-way and other City properties within city limits.

Because all of the trees within the portion of the Riverfront Park parking lot proposed for temporary construction staging are on City property, they are classified as City trees and are therefore exempt from a Tree and Vegetation Removal Permit under SRC Chapter 808 and instead require a City tree removal permit under SRC Chapter 86. The applicant has applied for a separate permit for the tree removals on City-owned property (*Application Folder No. 25-111725-TR*). That permit application includes a replanting and mitigation plan and is being reviewed concurrently by the Development Services Division.

SRC CHAPTER 809 – WETLANDS

SRC 809.025 Required notification of the Oregon Division of State Lands

Finding 53: SRC 809.025(a) requires the City to send a wetland land use notification to the Oregon Division of State Lands within five working days of receiving a completed application for development or land use in an area designated as a wetland on the local wetlands map. The Salem-Keizer Local Wetland Inventory shows that there are wetland channels and/or hydric soils mapped on the property. The applicant should contact the Oregon Department of State Lands to verify if any permits are required for development or construction in the vicinity of the mapped wetland area(s), including any work in the public right-of-way. Wetland notice was sent to the Oregon Department of State Lands pursuant to SRC 809.025.

SRC CHAPTER 810 LANDSLIDE HAZARDS

SRC 810.020 Landslide hazard construction permit

Finding 54: SRC 810.020(a)(1) requires a landslide hazard construction permit for various activities occurring within an area identified as having landslide hazard susceptibility. The proposed repaving of the parking lot is not a regulated activity under SRC Chapter 810; therefore, a geotechnical assessment has not been required for the Site Plan Review application. As part of the Willamette Greenway Development Permit application, geotechnical investigation has been completed to ensure compliance with the approval criteria of SRC Chapter 600.

6. Analysis of Tree and Vegetation Removal Permit Approval Criteria

Salem Revised Code (SRC) 808.030(d) sets forth the criteria that must be met before approval can be granted to an application for a Tree and Vegetation Removal Permit. Pursuant to this subsection, a tree removal and vegetation removal permit shall be granted if one or more of the criteria included under SRC 808.030(d)(1)–(5) are met. As identified in their written statement, the applicant has chosen to address approval criterion SRC 808.030(d)(2).

SRC 808.030(d)(2): Repair, alteration, or replacement of existing structures.

Finding 55: SRC 808.030(d)(2) allows the removal of a tree or native vegetation when it is reasonably necessary to effect the otherwise lawful repair, alteration, or replacement of an existing structure, the footprint of the repaired, altered, or replacement structure is not enlarged, and no additional riparian corridor area is disturbed beyond that essential to the repair, alteration, or replacement of the existing structure.

As shown on the tree protection and removal plans for the proposal on Sheets FB02, FB03, and FB04, trees and native vegetation are proposed for removal along both sides of the Willamette River within the 75-foot-wide riparian corridor in order to establish temporary construction staging areas for the proposed seismic improvements to the Center Street Bridge. The trees and native vegetation proposed for removal in the riparian corridor on the west side of the river are located on City-owned property within Wallace Marine Park and the trees and native vegetation proposed for removal within the riparian corridor on the east side of the river are located on City-owned property within Riverfront Park. As provided under SRC 808.030(a)(2)(C), removal of trees on City property are exempt from tree and vegetation removal permits under the City's tree preservation ordinance (SRC Chapter 808) and are instead subject to the City tree removal permit requirements under SRC Chapter 86. As such, because all of the trees proposed for removal from the subject properties are either City trees within parks or public right-of-way, their removal is regulated through SRC Chapter 86 rather than SRC Chapter 808. Because of this, the applicant has applied for a separate City tree removal permit for the proposed tree removals on City-owned property (*Application Folder No. 25-111725-TR*). That permit application includes a replanting and mitigation plan and is being reviewed concurrently by the Development Services Division.

Finding 56: The permit exemption under SRC Chapter 808 for City trees does not, however, apply to non-tree native vegetation within riparian corridors. As such, because the proposal requires not only the removal of existing trees to accommodate the proposed seismic improvements to the bridge but also the removal of existing native vegetation within the temporary construction staging areas, a tree and vegetation removal permit under SRC 808.030 is required.

In order for a tree and vegetation removal permit to be approved under SRC 808.030(a)(2), it must be demonstrated that the native vegetation removal is reasonably necessary to effect the otherwise lawful repair, alteration, or replacement of an existing structure; the footprint of the repaired, altered, or replacement structure is not enlarged; and no additional riparian corridor area is disturbed beyond that essential to the repair, alteration, or replacement of the existing structure.

The Center Street Bridge is an existing structure in need of seismic improvements to improve its structural stability and resiliency during an earthquake. The seismic improvements are therefore a necessary alteration to an existing structure and the footprint of the bridge will not be enlarged and the amount of disturbance to the riparian corridor is limited to the least amount necessary in order to conduct the necessary improvements to the bridge. Existing native vegetation impacted within the riparian corridors will be replaced at a 1:1 ratio with native trees and vegetation selected from the City's Tree and Vegetation Technical Manual and planted in conformance with the Willamette Greenway Riparian Buffer Enhancement Guide and SRC 808.055 in order to ensure the restoration of the disturbed areas. This approval criterion is met.

DECISION

Based upon the Facts and Findings herein, the Hearings Officer **APPROVES** the consolidated applications for a Class 2 Willamette Greenway permit, a Class 2 site plan review, and a tree and vegetation removal permit associated with the establishment of temporary construction and staging areas required for proposed seismic improvements to the Center Street Bridge; and the future repaving of a portion of the north parking lot of Riverfront Park following its use as a temporary construction staging area located at 314 Water Street NE, 116 Marion Street NE, and the 200 to 300 blocks of Musgrave Lane NW, (Marion County Assessor Map and Tax Lot Numbers: 073W22CD00400, 073W22CD00401, 073W22CD00500, 073W22CD00600, 073W22CD00700, & 073W22DB07400; and Polk County Assessor Map and Tax Lot Numbers: 073W22C00100 & 073W22C00101), subject to the following conditions of approval:

- A. **APPROVE** the Class 2 Willamette Greenway Development Permit subject to the following conditions of approval:

Condition 1: Prior to establishing the proposed temporary construction staging areas for the bridge, the applicable temporary use

permit approvals under SRC Chapter 701 shall be obtained.

- Condition 2:** Prior to construction, provide a final Geotechnical Report signed and stamped by the project's Certified Engineering Geologist (CEG) and Engineer (PE).
- Condition 3:** The Salem Fire and Public Works Department shall be given the opportunity to review and comment on the emergency response plan required by Section 311 of the Clean Water Act prior to its adoption.
- Condition 4:** Design and construct a storm drainage system at the time of development in compliance with *Salem Revised Code (SRC)* Chapter 71 and *Public Works Design Standards (PWDS)*.
- Condition 5:** Prior to any excavation, grading, or construction, a survey map, certified by a licensed Professional Land Surveyor, shall be submitted to the Director showing the Willamette Greenway Boundary and its relationship to the site and survey monuments thereon.
- Condition 6:** Prior to any excavation, grading, or construction, plans for removal and replacement of any native vegetation shall be submitted to and approved by the Director.

B. **APPROVE** the Class 2 Site Plan Review.

C. **APPROVE** the Tree and Vegetation Removal Permit.

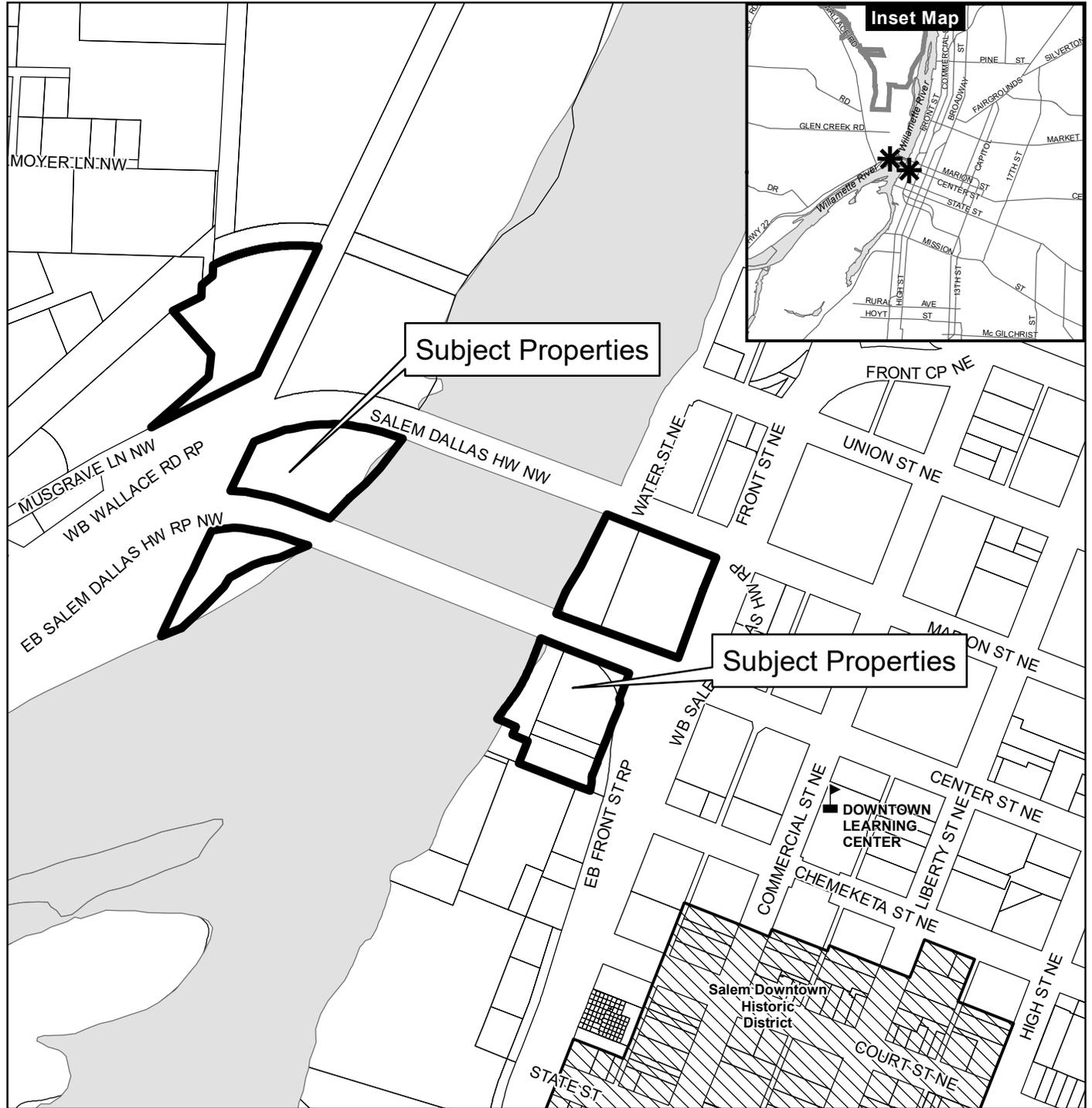
DATED: July 30, 2025



Gary Darnielle, Hearings Officer

Vicinity Map

314 Water Street NE, 116 Marion Street NE, & 200 to 300 Blocks of Musgrave Lane NW



Legend

Taxlots	Outside Salem City Limits	Parks
Urban Growth Boundary	Historic District	CITY OF Salem AT YOUR SERVICE Community Planning and Development
City Limits	Schools	<p>This product is provided as is, without warranty. In no event is the City of Salem liable for damages from the use of this product. This product is subject to license and copyright limitations and further distribution or resale is prohibited.</p>
0 100 200 400 Feet		

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INDEX OF SHEETS	
SHEET NO.	DESCRIPTION
A01	Title Sheet
A02	Index Of Sheets Cont.
A03	Index Of Sheets Cont. & Std. Dwg. Nos.

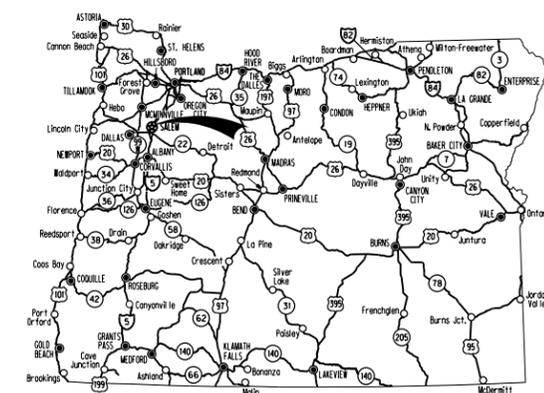
STATE OF OREGON
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED PROJECT

**GRADING, DRAINAGE, STRUCTURES, PAVING,
 SIGNING, ILLUMINATION, AND ROADSIDE DEVELOPMENT**

OR22: CENTER ST BRIDGE (SALEM) PROJECT
WILLAMINA-SALEM HIGHWAY

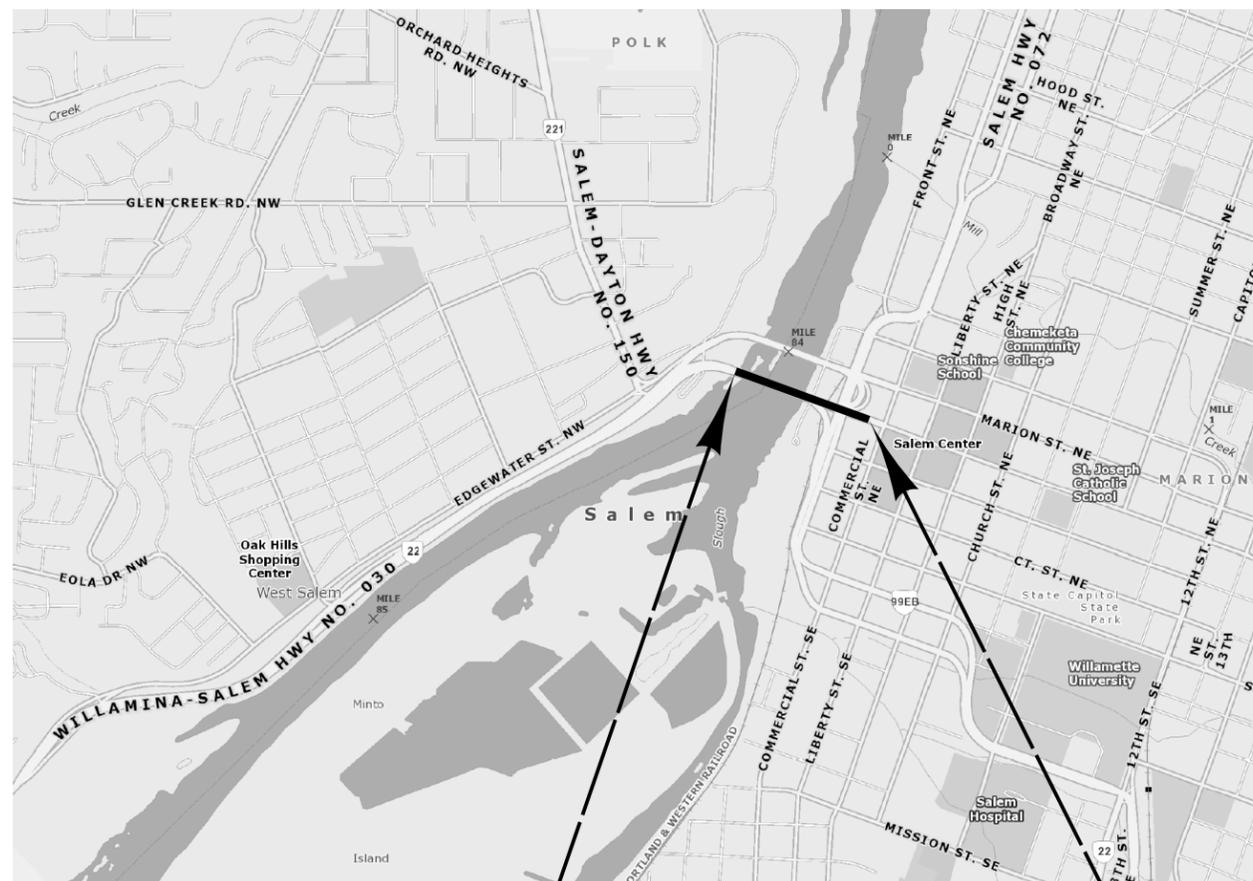
MARION & POLK COUNTIES
JANUARY 2026



Overall Length Of Project - XX.XX Miles



ATTENTION:
 Oregon Law Requires You To Follow Rules Adopted By The Oregon Utility Notification Center. Those Rules Are Set Forth In OAR 952-001-0001 Through OAR 952-001-0100. You May Obtain Copies Of The Rules By Calling The Center (Note: The Telephone Number For The Oregon Utility Notification Center Is (503) 232-1987).



BEGINNING OF PROJECT

STA. "CE" 41+84.65
(M.P. 25.82)

END OF PROJECT

STA. "CE" 56+51.48
(M.P. 26.08)



SEC. 22, T. 7 S., R. 3 W., W.M.



PLANS PREPARED FOR
 OREGON DEPARTMENT OF TRANSPORTATION

DOWL WWW.DOWL.COM

OREGON TRANSPORTATION COMMISSION

Julie Brown	CHAIR
Lee Beyer	VICE CHAIR
Phil Chang	COMMISSIONER
Alicia Chapman	COMMISSIONER
Jeff Baker	COMMISSIONER
Kristopher W. Strickler	DIRECTOR OF TRANSPORTATION

These plans were developed using AASHTO design standards. Exceptions to these standards, if any, have been submitted and approved by the ODOT Chief Engineer or their delegated authority.

Approving Authority: _____
 Signature & date

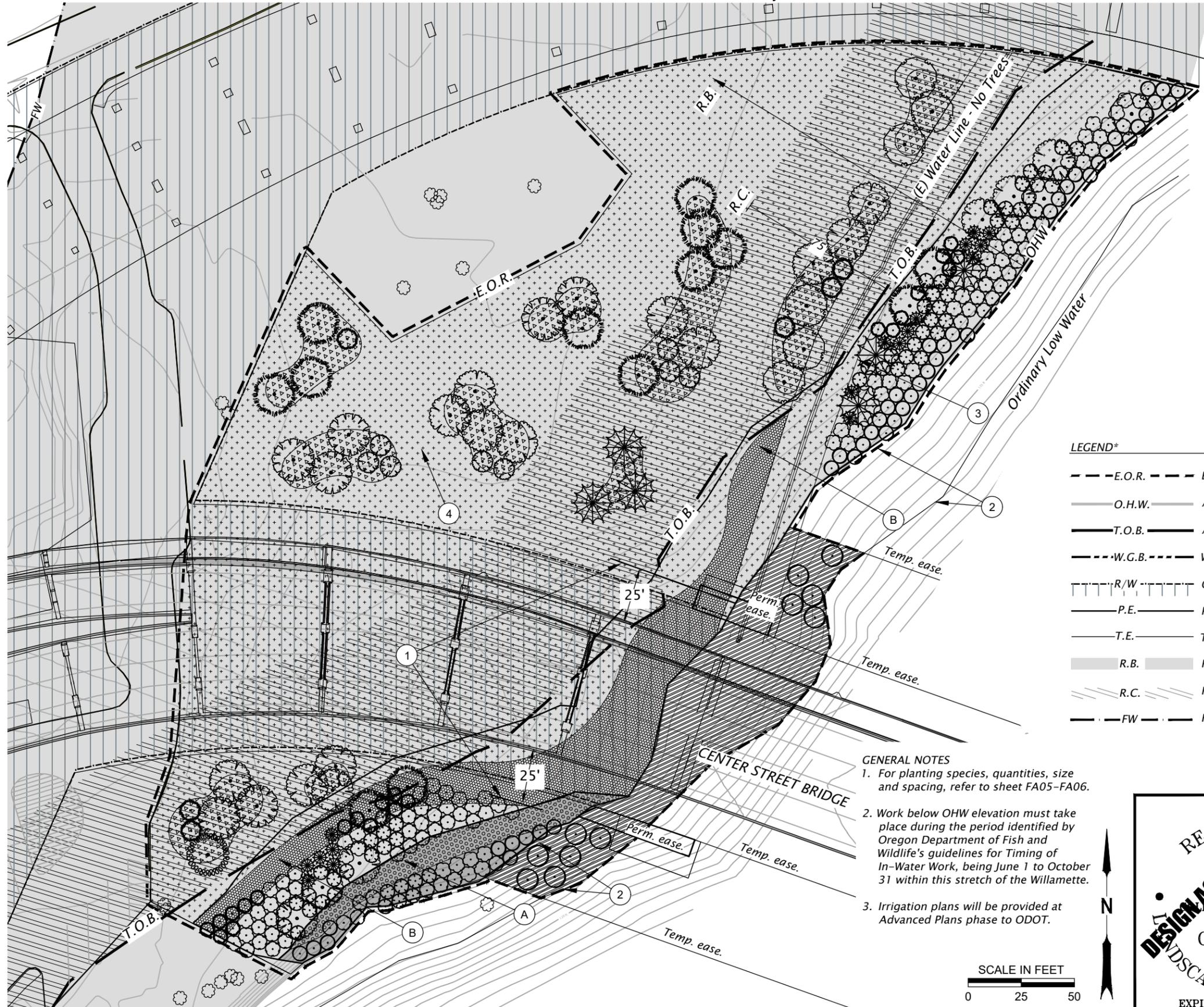
Print name and title: _____

Concurrence by ODOT Chief Engineer

OR22: CENTER ST BRIDGE (SALEM) PROJECT
 WILLAMINA-SALEM HIGHWAY
 MARION & POLK COUNTIES

FEDERAL HIGHWAY ADMINISTRATION	PROJECT NUMBER	SHEET NO.
OREGON DIVISION	S030(031)	A01

DESIGN ACCEPTANCE
PLANS
INFORMATION ONLY



WEST SIDE LANDSCAPE SUMMARY

Chapter 600: Willamette Greenway

Riparian Buffer Area Enhancement – measured from Ordinary High Water Line

- Floodway is greater than 75 feet, therefore Riparian Buffer goes from OHW line to the extent of the floodway (Riparian Buffer Method 2).

Riparian Buffer Area Enhancement:

- (A) **ESCARPMENT SECTION**
 - Assume 25% slopes or greater.
 - TOTAL = 2,950 SF
 - TREES @ 40% = 1,180 SF planted @7' OC
 - SHRUBS @59% = 1,740 SF planted @4' OC
- (B) **OUTER SECTION**
 - Assume 20% slopes or less.
 - Extends 40' after from Escarpment Section
 - TOTAL = 24,685 SF
 - TREES @ 40% = 9,875 SF planted @7' OC
 - SHRUBS @30% = 7,405 SF planted @4' OC

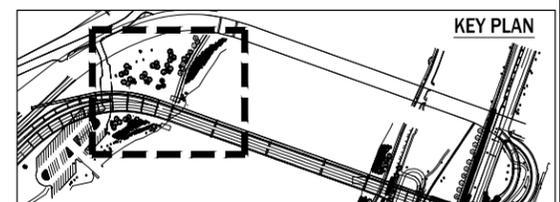
KEY NOTES

- 1 No new tree planting within 25 feet of overhead bridge structure. No proposed tree planting within 10 feet of park roadway.
- 2 Native riparian revegetation planting shown from elevation 130' down to approximately 2 feet below ordinary high water elevation north of the bridge, 3 feet below ordinary high water elevation south of the bridge.
- 3 Existing waterline and easement, 10 feet each side; no tree planting.
- 4 Minimum 10 foot width clear for mowing.

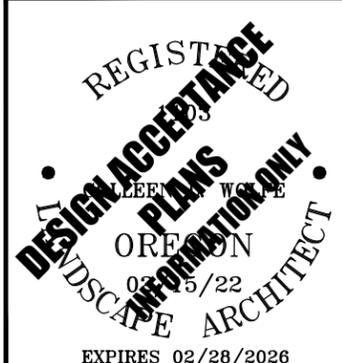
LEGEND*

--- E.O.R. ---	Extent of Revegetation	[Pattern]	Permanent Seeding - Native Meadow Mix
— O.H.W. —	Approx. Ordinary High Water line	[Pattern]	Riparian Buffer: Escarpment Section Provide establishment irrigation
— T.O.B. —	Approximate Top of Bank line	[Pattern]	Riparian Buffer: Outer Section Provide establishment irrigation
--- W.G.B. ---	Willamette Greenway Boundary	[Pattern]	Park Restoration Native Planting Provide establishment irrigation
--- R/W ---	ODOT Right-of-Way	[Pattern]	Federal and State Mitigation Area Provide establishment irrigation
— P.E. —	Permanent Easement		
— T.E. —	Temporary Easement		
[Pattern]	R.B.		Riparian Buffer
[Pattern]	R.C.		Riparian Corridor
--- FW ---	FEMA 100 Year Floodway		

*See FA05 for Tree species and replacement totals
**See FA06 for Typical Escarpment, Outer Section and Park Restoration planting plan enlargements.



- GENERAL NOTES**
1. For planting species, quantities, size and spacing, refer to sheet FA05-FA06.
 2. Work below OHW elevation must take place during the period identified by Oregon Department of Fish and Wildlife's guidelines for Timing of In-Water Work, being June 1 to October 31 within this stretch of the Willamette.
 3. Irrigation plans will be provided at Advanced Plans phase to ODOT.



OR22: CENTER ST BRIDGE (SALEM) PROJECT
WILLAMINA-SALEM HIGHWAY
MARION & POLK COUNTIES

Designer: Grace Youngblood, LD Reviewer: Colleen Wolfe, PLA
Drafter: Grace Youngblood, LD Checker: Jared Trowbridge, PE

REVEGETATION PLAN SHEET NO. FA01

EAST SIDE LANDSCAPE SUMMARY

Chapter 600: Willamette Greenway

Riparian Buffer Area Enhancement – Measured from Ordinary High Water Line

- Assume 25% slopes or greater.
- 75 foot Riparian Buffer in this location.

(A) ESCARPMENT SECTION FOR ENTIRE AREA

TOTAL = 17,435 sf

TREES @ 40% = 6,975 sf planted @ 7' OC

LEGEND*

- Permanent Seeding
Native Meadow Mix
- Storm Water Facility Planting
- Riparian Buffer Escarpment
Provide establishment irrigation
- Bark mulch - No Irrigation
- Federal and State Mitigation Area
Provide establishment irrigation
- East Parking Lot Repair
Type A Landscaping Standard
(1 PU/20 SF plus groundcover)

*See FA05 for Tree species and replacement totals

**See FA06 for Typical Escarpment planting plan enlargement.

Soil Cell / Unit Paver Area
30,000 CF Required for 22 new trees
(16 large trees & 6 small trees)

- 18,700 CF soil volume provided with soil cells
- 2,735 CF soil volume provided under pervious concrete
- 10,620 CF soil volume provided in planters (24" min. depth)

LEGEND

- - - E.O.R. - - - Extent of Revegetation
- O.H.W. — Approx. Ordinary High Water line
- T.O.B. — Approximate Top of Bank line
- - - W.G.B. - - - Willamette Greenway Boundary
- R/W — ODOT Right-of-Way
- P.E. — Permanent Easement
- T.E. — Temporary Easement
- R.B. Riparian Buffer
- R.C. Riparian Corridor
- FW — FEMA 100 Year Floodway

KEY NOTES:

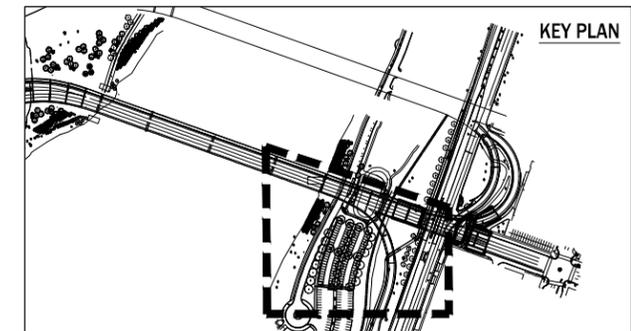
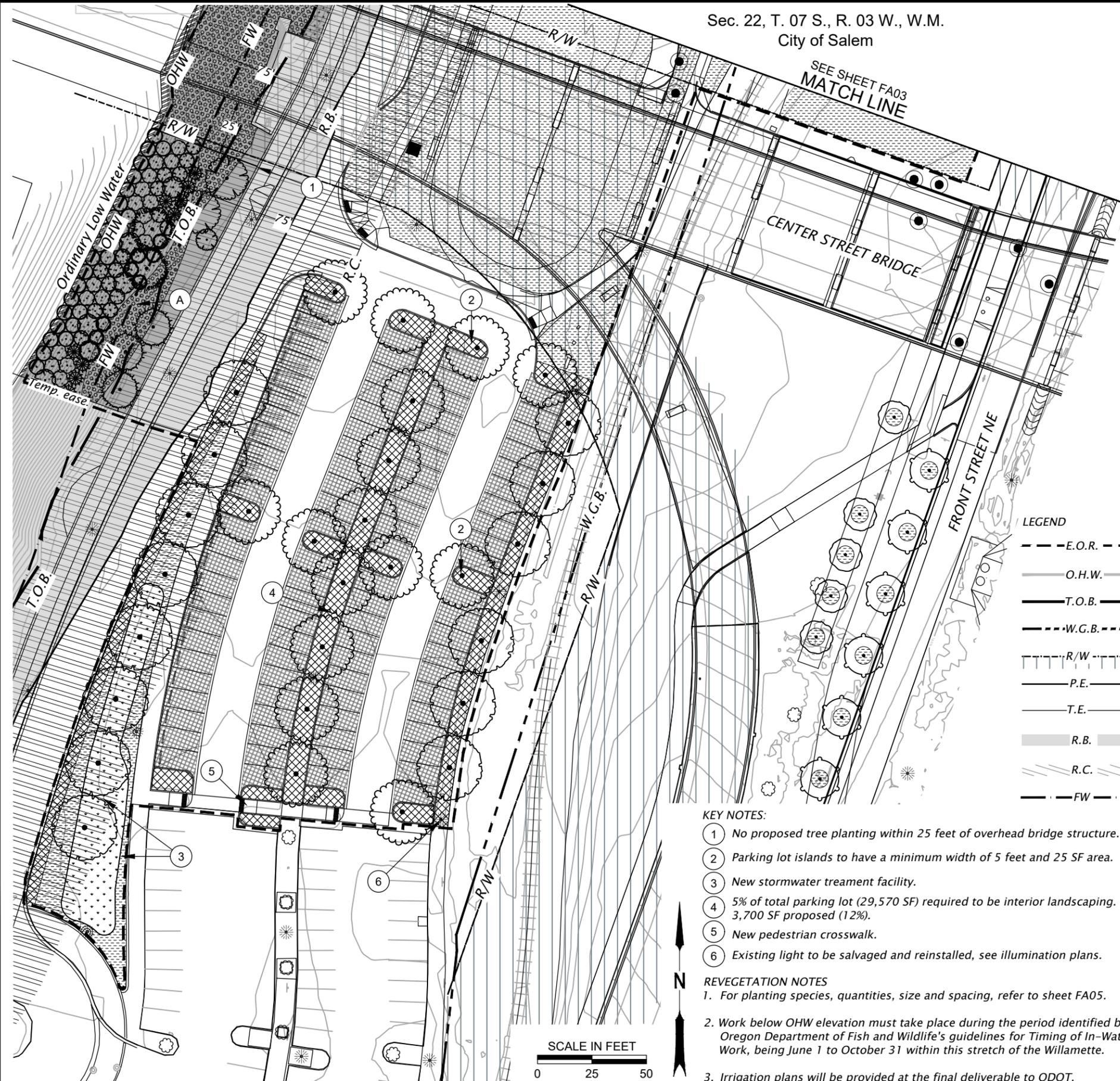
- 1 No proposed tree planting within 25 feet of overhead bridge structure.
- 2 Parking lot islands to have a minimum width of 5 feet and 25 SF area.
- 3 New stormwater treatment facility.
- 4 5% of total parking lot (29,570 SF) required to be interior landscaping. 3,700 SF proposed (12%).
- 5 New pedestrian crosswalk.
- 6 Existing light to be salvaged and reinstalled, see illumination plans.

REVEGETATION NOTES

1. For planting species, quantities, size and spacing, refer to sheet FA05.
2. Work below OHW elevation must take place during the period identified by Oregon Department of Fish and Wildlife's guidelines for Timing of In-Water Work, being June 1 to October 31 within this stretch of the Willamette.
3. Irrigation plans will be provided at the final deliverable to ODOT.



SCALE IN FEET



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OR22: CENTER ST BRIDGE (SALEM) PROJECT
WILLAMINA-SALEM HIGHWAY
MARION & POLK COUNTIES

Designer: Grace Youngblood, LD Reviewer: Colleen Wolfe, PLA
Drafter: Grace Youngblood, LD Checker: Jared Trowbridge, PE

REVEGETATION PLAN SHEET NO. FA02

Sec. 22, T. 07 S., R. 03 W., W.M.
City of Salem

LEGEND*

- Permanent Seeding - Native Meadow Mix
- Riparian Buffer Escarpment Planting
Provide establishment irrigation
- Bark mulch - No Irrigation
- Federal and State Mitigation Area
Provide establishment irrigation

*See FA05 for Tree species and replacement totals

**See FA06 for Typical Escarpment planting plan enlargement.

KEY NOTES:
① Existing light to be salvaged and reinstalled, see illumination plans.

REVEGETATION NOTES:

1. For planting species, quantities, size and spacing, refer to sheet FA05.
2. Work below OHW elevation must take place during the period identified by Oregon Department of Fish and Wildlife's guidelines for Timing of In-Water Work, being June 1 to October 31 within this stretch of the Willamette.
3. Irrigation plans will be provided at Advanced Plans phase to ODOT.

EAST SIDE LANDSCAPE SUMMARY

Chapter 600: Willamette Greenway

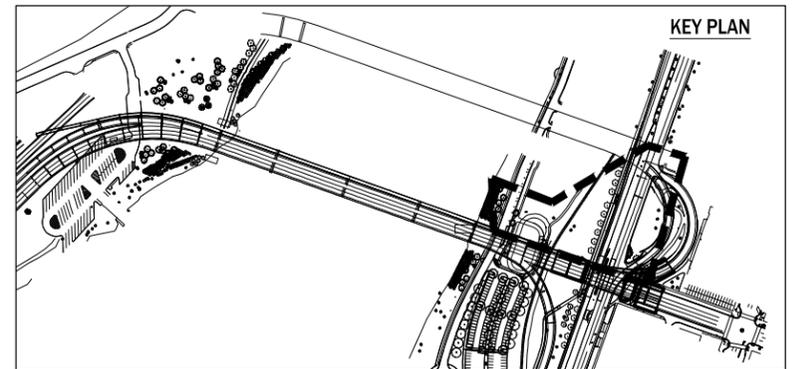
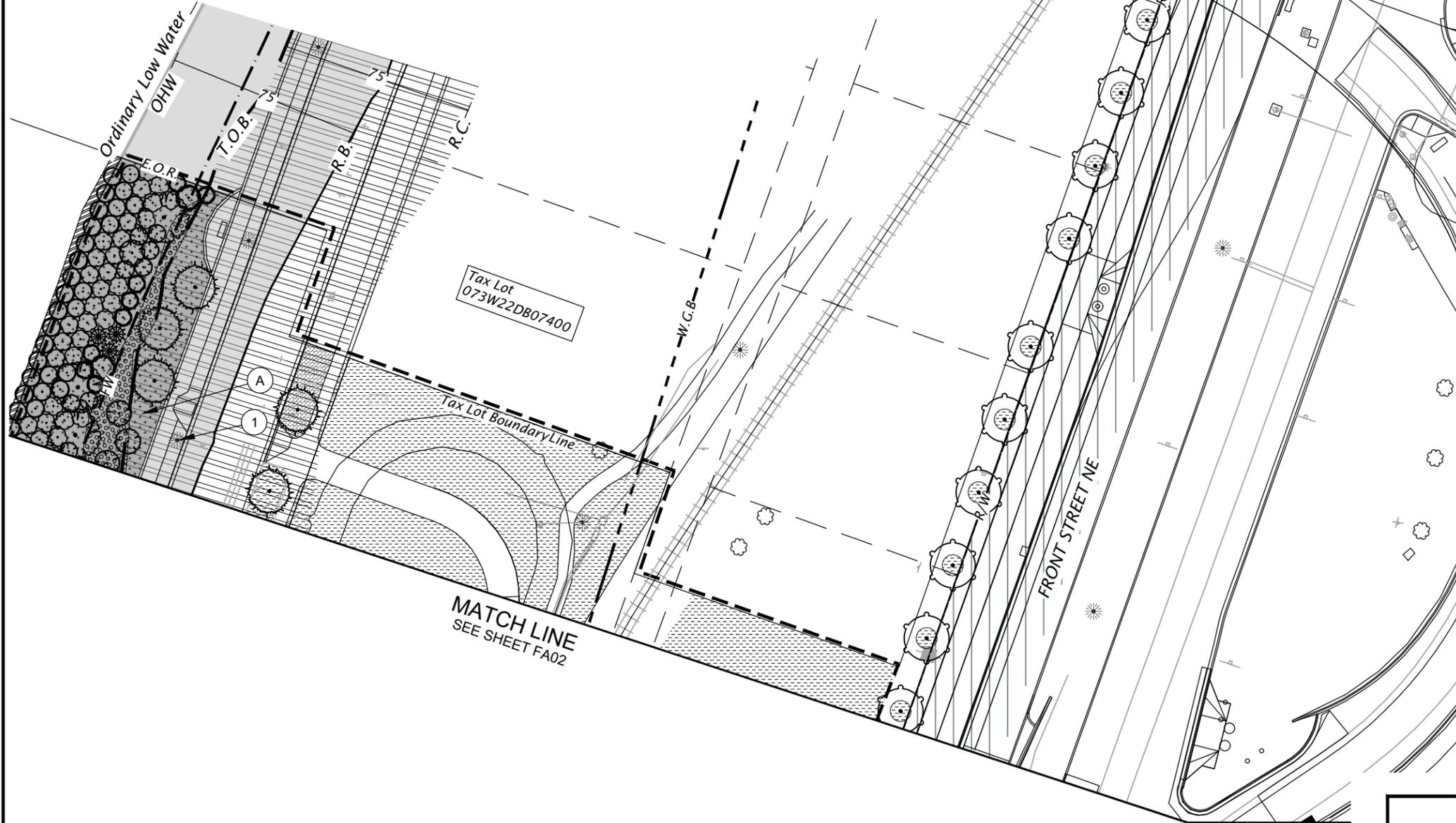
- Riparian Buffer Area Enhancement – Measured from Ordinary High Water Line
- Assume 25% slopes or greater.
 - 75 foot Riparian Buffer in this location.

① ESCARPMENT SECTION FOR ENTIRE AREA

TOTAL = 17,435 sf
 TREES @ 40% = 6,975 sf planted @ 7' OC
 SHRUBS @48% = 8,270 sf planted @ 4' OC

LEGEND

- E.O.R. Extent of Revegetation
- O.H.W. Approx. Ordinary High Water line
- T.O.B. Approximate Top of Bank line
- W.G.B. Willamette Greenway Boundary
- R/W ODOT Right-of-Way
- P.E. Permanent Easement
- T.E. Temporary Easement
- R.B. Riparian Buffer
- R.C. Riparian Corridor
- FW FEMA 100 Year Floodway



OR22: CENTER ST BRIDGE (SALEM) PROJECT
 WILLAMINA-SALEM HIGHWAY
 MARION & POLK COUNTIES

Designer: Grace Youngblood, LD Reviewer: Colleen Wolfe, PLA
 Drafter: Grace Youngblood, LD Checker: Jared Trowbridge, PE

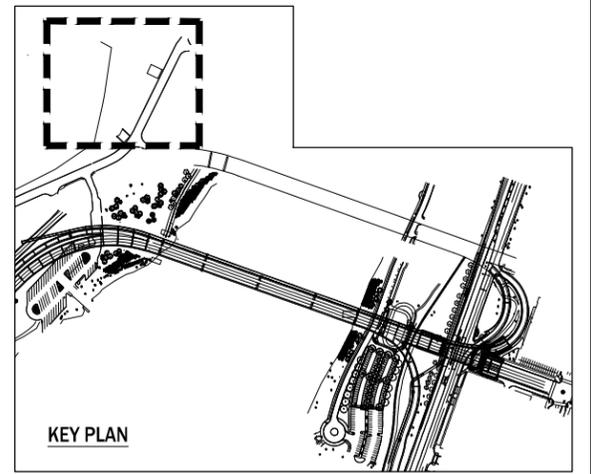
REVEGETATION PLAN SHEET NO. FA03



LEGEND

	Permanent Seeding - Native Meadow Mix
	E.O.R. - Extent of Revegetation
	W.G.B. - Willamette Greenway Boundary
	R/W - ODOT Right-of-Way
	P.E. - Permanent Easement
	T.E. - Temporary Easement
	R.B. - Riparian Buffer
	FW - FEMA 100 Year Floodway

- KEY NOTES:**
- ① Existing vegetation to remain and protect.
- GENERAL NOTES:**
1. For planting species, quantities, size and spacing, refer to sheet FA05.
 2. Work below OHW elevation must take place during the period identified by Oregon Department of Fish and Wildlife's guidelines for Timing of In-Water Work, being June 1 to October 31 within this stretch of the Willamette.



REGISTERED
DESIGN ACCEPTANCE
 LANDSCAPE ARCHITECT
 PLANS
 INFORMATION ONLY
 OREGON
 02/25/22
 EXPIRES 02/28/2026

 DOWL WWW.DOWL.COM	 OREGON DEPARTMENT OF TRANSPORTATION
OR22: CENTER ST BRIDGE (SALEM) PROJECT WILLAMINA-SALEM HIGHWAY MARION & POLK COUNTIES	
Designer: Grace Youngblood, LD Drafter: Grace Youngblood, LD	Reviewer: Colleen Wolfe, PLA Checker: Jared Trowbridge, PE
REVEGETATION PLAN	
SHEET NO. FA04	

TREE REPLACEMENTS (WEST)

SYMBOL	CODE	BOTANICAL NAME	COMMON NAME	SIZE	QTY
ESCARPMENT PLANTING					
	ACMA-E	ACER MACROPHYLLUM	BIG LEAF MAPLE	#1 CONT.	52
	ALRU-E	ALNUS RUBRA	RED ALDER	#1 CONT.	58
	CADE-E	CALOCEDRUS DECURRENS	INCENSE CEDAR	#1 CONT.	22
	PSDO-E	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	#1 CONT.	17
	RHPU-E	RHAMNUS PURSHIANA	CASCARA	#1 CONT.	54
	SASC-E	SALIX SCOULERIANA	SCOULER'S WILLOW	#1 CONT.	45
RIPARIAN CORRIDOR TREE REPLACEMENT					
	ACMA-W	ACER MACROPHYLLUM	BIG LEAF MAPLE	#5 CONT.	8
	ACMA	ACER MACROPHYLLUM	BIG LEAF MAPLE	1.5" CAL.	19
	ARME	ARBUTUS MENZIESII	PACIFIC MADRONE	1.5" CAL.	1
	CADE-W	CALOCEDRUS DECURRENS	INCENSE CEDAR	#5 CONT.	6
	CADE	CALOCEDRUS DECURRENS	INCENSE CEDAR	4' HT.	13
	PSDO	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	4' HT.	6
	QUGA	QUERCUS GARRYANA	OREGON WHITE OAK	1.5" CAL.	7
	RHPU	RHAMNUS PURSHIANA	CASCARA	1.5" CAL.	8
	SASC	SALIX SCOULERIANA	SCOULER'S WILLOW	#5 CONT.	11

PRELIMINARY SHRUB PLANTING SCHEDULE (WEST)

BOTANICAL NAME	COMMON NAME	CONTAINER SIZE	SPACING	TOTAL
RIPARIAN BUFFER: ESCARPMENT SECTION PLANTING = 1,740 SF				
CORNUS SERICEA	REDTWIG DOGWOOD	NO. 1 CONTAINER	4' O.C.	20
SALIX HOOKERIANA	DUNE WILLOW	NO. 1 CONTAINER	4' O.C.	20
SYMPHORICARPOS ALBUS	SNOWBERRY	NO. 1 CONTAINER	4' O.C.	20
SPIRAEA DOUGLASII	DOUGLAS SPIREA	NO. 1 CONTAINER	4' O.C.	20
PHYSOCARPUS CAPITATUS	PACIFIC NINEBARK	NO. 1 CONTAINER	4' O.C.	20
ROSA PISOCARPA	SWAMP ROSE	NO. 1 CONTAINER	4' O.C.	20
RIPARIAN BUFFER: OUTER SECTION PLANTING = 7,430 SF				
CORNUS SERICEA	REDTWIG DOGWOOD	NO. 1 CONTAINER	4' O.C.	90
SALIX HOOKERIANA	DUNE WILLOW	NO. 1 CONTAINER	4' O.C.	90
SYMPHORICARPOS ALBUS	SNOWBERRY	NO. 1 CONTAINER	4' O.C.	90
SPIRAEA DOUGLASII	DOUGLAS SPIREA	NO. 1 CONTAINER	4' O.C.	90
PHYSOCARPUS CAPITATUS	PACIFIC NINEBARK	NO. 1 CONTAINER	4' O.C.	90
ROSA PISOCARPA	SWAMP ROSE	NO. 1 CONTAINER	4' O.C.	90
PARK RESTORATION NATIVE PLANTING = 10,100 SF				
ACER CIRCINATUM	VINE MAPLE	NO. 1 CONTAINER	4' O.C.	245
ROSA PISOCARPA	SWAMP ROSE	NO. 1 CONTAINER	4' O.C.	245
MAHONIA AQUIFOLIUM	OREGON GRAPE	NO. 1 CONTAINER	4' O.C.	245
BELOW ORDINARY HIGH WATER = 230 LF				
CORNUS SERICEA	REDTWIG DOGWOOD	NO. 1 CONTAINER	4' O.C.	13
SALIX HOOKERIANA	DUNE WILLOW	NO. 1 CONTAINER	4' O.C.	13
PERMANENT SEEDING: NATIVE MEADOW MIX = 72,400 SF				

TREE REPLACEMENT TOTALS & SIZING

TREE REPLACEMENT TOTAL
WEST
(19) Trees replaced within Riparian Corridor to 1:1 ratio @ 1.5" cal (per 808.055)
(35) Trees replaced above the Riparian Enhancement Buffer and not required by Riparian Corridor replacement requirements at 1:1 ratio @1.5" cal
(263) Trees replaced within Riparian Enhancement Buffer to Escarpment & Outer Section Requirements (7' O.C.)
(327) Total Tree Replacements
•• (54) Trees at 1.5" Caliper
•• (263) Trees at #1 container
•• (10) Trees at #5 container

VEGETATION REPLACEMENT (EAST & WEST)
 Assuming all non-paved area within Riparian Buffer has existing vegetation and is native. Replacement vegetation is calculated using Riparian Buffer Enhancement standards which relies on percentage of area in square foot. Plan meets Riparian Buffer Enhancement requirements.

TREE REPLACEMENTS (EAST)

SYMBOL	BOTANICAL NAME	COMMON NAME	SIZE	QTY
ESCARPMENT PLANTING				
	ACER MACROPHYLLUM	BIG LEAF MAPLE	#1 CONT.	24
	ALNUS RUBRA	RED ALDER	#1 CONT.	27
	CALOCEDRUS DECURRENS	INCENSE CEDAR	#1 CONT.	15
	PSEUDOTSUGA MENZIESII	DOUGLAS FIR	#1 CONT.	12
	RHAMNUS PURSHIANA	CASCARA	#1 CONT.	23
	SALIX SCOULERIANA	SCOULER'S WILLOW	#1 CONT.	54
STREET TREES				
	ZELKOVA SERRATA 'GREEN VASE'	GREEN VASE ZELKOVA	1.5" CAL.	16
	ZELKOVA SERRATA 'WIRELESS'	WIRELESS ZELKOVA	1.5" CAL.	5
LANDSCAPE REPAIR - EAST PARKING LOT				
	NYSSA SYLVATICA	TUPELO	1.5" CAL.	14
	QUERCUS RUBRA	NORTHERN RED OAK	1.5" CAL.	16
RIPARIAN CORRIDOR TREE REPLACEMENT				
	ACER MACROPHYLLUM	BIG LEAF MAPLE	#5 CONT.	10
	ACER MACROPHYLLUM	BIG LEAF MAPLE	1.5" CAL.	5
	CALOCEDRUS DECURRENS	INCENSE CEDAR	#5 CONT.	3
	CALOCEDRUS DECURRENS	INCENSE CEDAR	4' HT.	2
	THUJA PLICATA	WESTERN RED CEDAR	5-6' HT.	8

PRELIMINARY SHRUB PLANTING SCHEDULE (EAST)

BOTANICAL NAME	COMMON NAME	CONTAINER SIZE	SPACING	TOTAL
RIPARIAN BUFFER: ESCARPMENT SECTION PLANTING = 8,270 SF				
CORNUS SERICEA	REDTWIG DOGWOOD	NO. 1 CONTAINER	4' O.C.	100
SALIX HOOKERIANA	DUNE WILLOW	NO. 1 CONTAINER	4' O.C.	100
SYMPHORICARPOS ALBUS	SNOWBERRY	NO. 1 CONTAINER	4' O.C.	100
SPIRAEA DOUGLASII	DOUGLAS SPIREA	NO. 1 CONTAINER	4' O.C.	100
PHYSOCARPUS CAPITATUS	PACIFIC NINEBARK	NO. 1 CONTAINER	4' O.C.	100
ROSA PISOCARPA	SWAMP ROSE	NO. 1 CONTAINER	4' O.C.	100
EAST PARKING LOT REPAIR - TYPE A STANDARD (1 PU/ 20 SF) PLUS GROUND COVER = 6,690 SF				
MAHONIA AQUIFOLIUM 'COMPACTA'	COMPACT OREGON GRAPE	NO. 5 CONTAINER	4' O.C.	170
CHOYSIA TERNATA	MEXICAN ORANGE	NO. 5 CONTAINER	4' O.C.	170
MAHONIA REPENS	CREeping MAHONIA	NO. 1 CONTAINER	3' O.C.	280
PLANTINGS BELOW ORDINARY HIGH WATER = 360 LF				
CORNUS SERICEA	REDTWIG DOGWOOD	NO. 1 CONTAINER	4' O.C.	27
SALIX HOOKERIANA	DUNE WILLOW	NO. 1 CONTAINER	4' O.C.	27
STORM WATER FACILITY SEEDING = 2,700 SF				

TREE REPLACEMENT TOTALS & SIZING

TREE REPLACEMENT TOTAL
EAST
(16) Trees replaced in Riparian Corridor to 1:1 ratio @ 1.5" cal (per 808.055)
(50) Trees replaced above the Riparian Enhancement Buffer and not required by Riparian Corridor replacement requirements at 1:1 ratio @1.5" cal
(168) Trees replaced within Riparian Enhancement Buffer to Escarpment Section Requirements (7' O.C.)
(234) Total Tree Replacement
•• (58) Trees at 1.5" Caliper
•• (8) Trees at 5-6' height
•• (155) Trees at #1 container
•• (13) Trees at #5 container

- GENERAL PLANTING NOTES:**
- See "American Standard for Nursery Stock" (ANSI Z60.1 -2014) for minimum plant quality standards.
 - Seed permanent seed mix and native plant seed mix with compost mulch blanket application. Include all incidental disturbed roadside areas beyond those indicated with permanent seed mix(es).
 - See environmental permit(s) for any permit conditions that may affect planting work, including bank planting areas.
 - Work below OHW elevation must take place during the period identified by Oregon Department of Fish and Wildlife's guidelines for Timing of In-Water Work, being June 1 to October 31 within this stretch of the Willamette.
 - Trees and native vegetation removed within Riparian Corridor shall be replaced at an area replacement ratio of one-to-one. Replacement trees shall have a minimum 1.5-inch caliper and shall be of species authorized in the Tree and Vegetation Technical Manual.

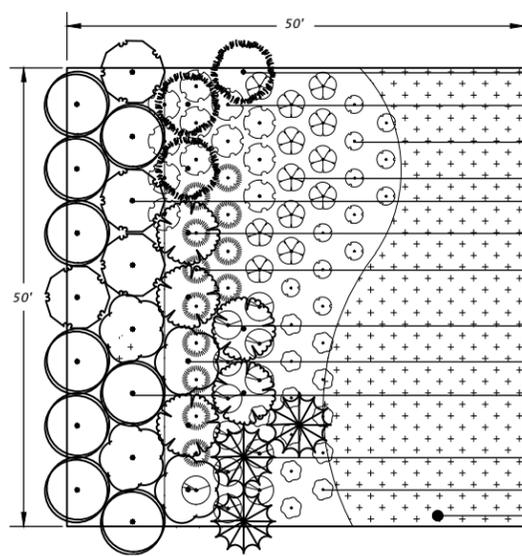


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OR22: CENTER ST BRIDGE (SALEM) PROJECT
 WILLAMINA-SALEM HIGHWAY
 MARION & POLK COUNTIES

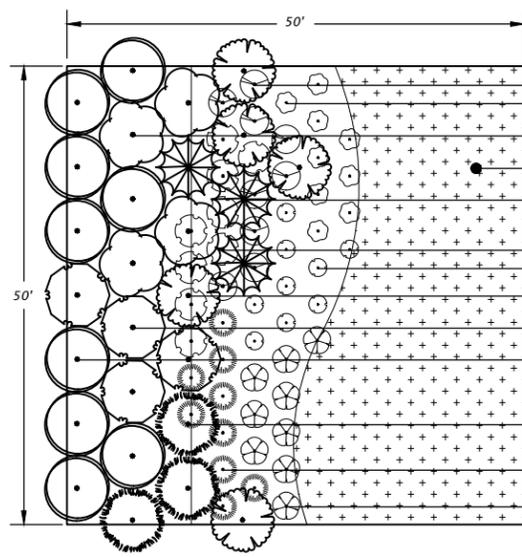
Designer: Grace Youngblood, LD Reviewer: Colleen Wolfe, PLA
 Drafter: Grace Youngblood, LD Checker: Jared Trowbridge, PE

REVEGETATION SCHEDULES SHEET NO. FA05



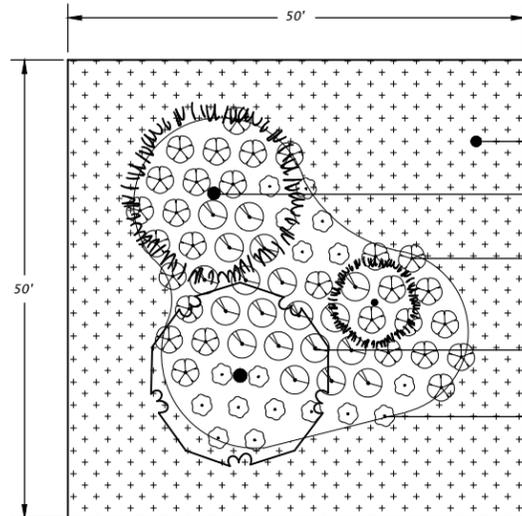
ESCARPMENT SECTION - Plan Enlargement
Typical Plant Layout per 2,500 SF NTS

- (3) Native Trees - Species Vary, See FA01-FA03
- (12) *Cornus sericea*
- (12) *Salix hookeriana*
- (12) *Symphoricarpos albus*
- (3) Native Trees - Species Vary, See FA01-FA03
- (5) Native Trees - Species Vary, See FA01-FA03
- (12) *Spiraea douglasii*
- (12) *Physocarpus capitatus*
- (3) Native Trees - Species Vary, See FA01-FA03
- (9) *Salix scouleriana*
- (3) Native Trees - Species Vary, See FA01-FA03
- (12) *Rosa pisocarpa*
- Native Seed Mix



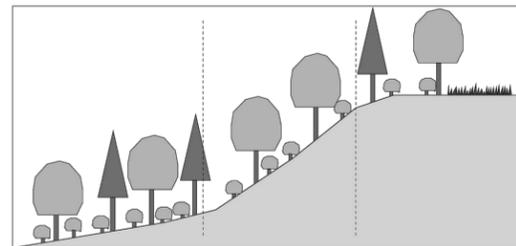
OUTER SECTION - Plan Enlargement
Typical Plant Layout per 2,500 SF NTS

- (9) *Physocarpus capitatus*
- (9) *Rosa pisocarpa*
- (4) Native Trees - Species Vary, See FA01-FA03
- Native Seed Mix
- (3) Native Trees - Species Vary, See FA01-FA03
- (9) *Cornus sericea*
- (9) *Salix hookeriana*
- (5) Native Trees - Species Vary, See FA01-FA03
- (4) Native Trees - Species Vary, See FA01-FA03
- (9) *Salix scouleriana*
- (3) Native Trees - Species Vary, See FA01-FA03
- (9) *Spiraea douglasii*
- (9) *Symphoricarpos albus*

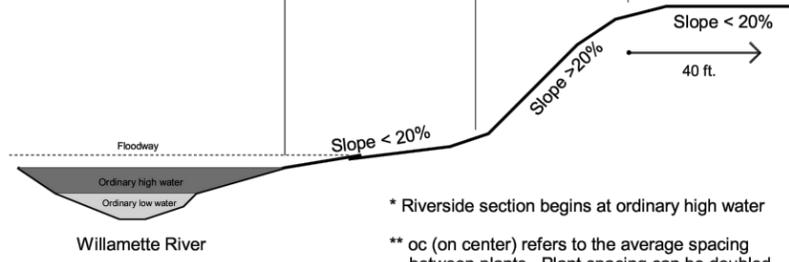


PARK RESTORATION - Plan Enlargement
Typical Plant Layout per 2,500 SF NTS

- Native Seed Mix
- (3) Native Trees - Species & Sizes Vary See FA01-FA03
- (29) *Symphoricarpos albus*
- (19) *Physocarpus capitatus*
- (17) *Rosa pisocarpa*



Riverside Section	Escarpment Section	Outer Section
Trees 70-90% Cover 6-7 ft. oc	Trees 40-60% Cover 6-7 ft. oc	Trees 30-50% Cover 7 ft. oc
Shrubs 10-30% Cover 3-4 ft. oc	Shrubs 40-60% Cover 3-4 ft. oc	Shrubs 10-30% Cover 3-4 ft. oc
Groundcover openings up to 20%		Groundcover openings up to 40%



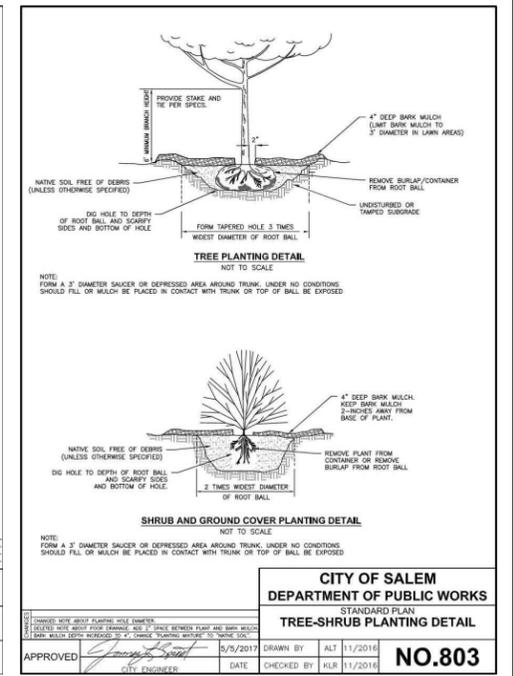
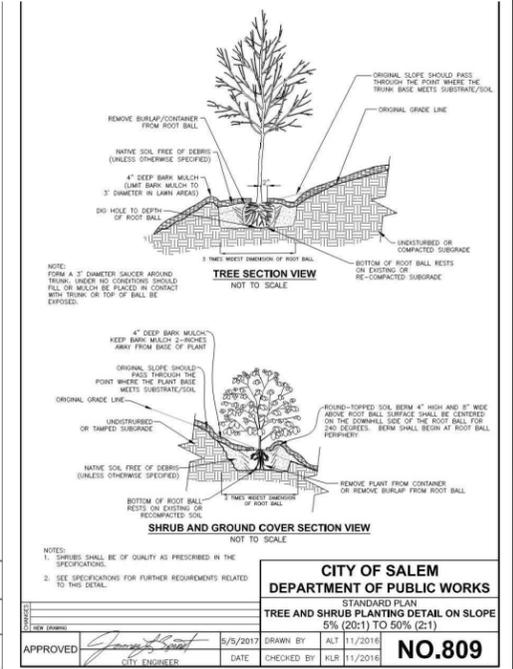
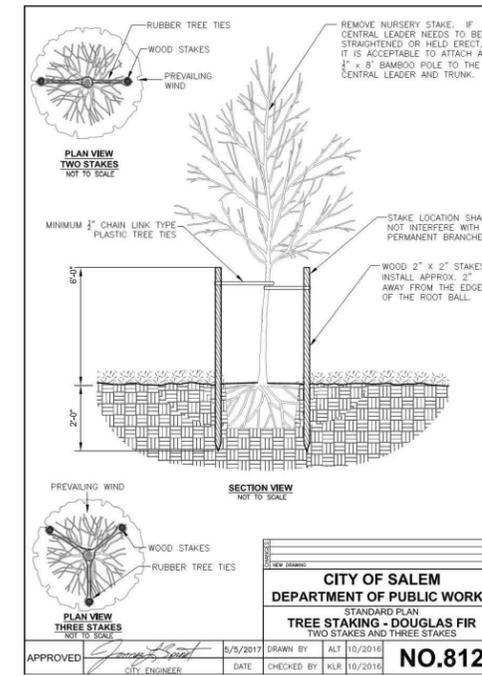
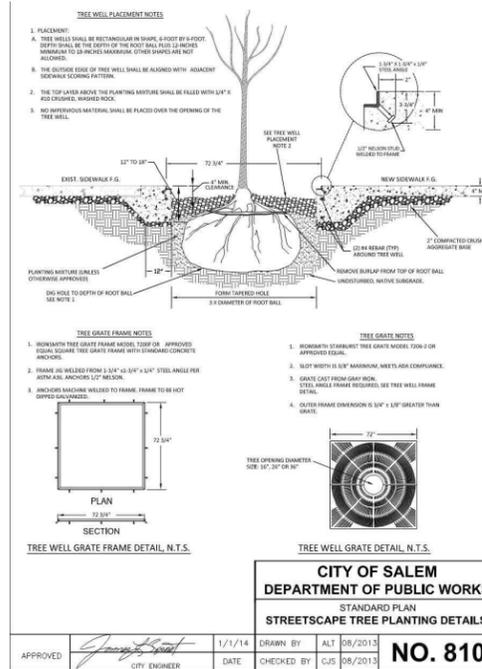
- * Riverside section begins at ordinary high water
- ** oc (on center) refers to the average spacing between plants. Plant spacing can be doubled if irrigation is provided for two years
- *** Groundcover shall be a native grass and forbs seed mixture

Escarpment Section. The escarpment section will begin at the landward side of the riverside section and extend landward to the break in the slope where the slope becomes less than 20%. Species listed in the "Riparian" or "Forest Slope" columns of the City of Salem Native Plan List shall be planted in this section.

Outer Section. The outer section will begin at the landward side of the escarpment section and extend landward to the edge of the Riparian Buffer Area 40 feet. Species listed in the "upland forest," "thicket," and "grass" columns of the City of Salem Native Plan List shall be planted in this section.

- City of Salem Definitions:**
Enhancement means increasing the net ecological functional values of the riparian buffer by any of the following:
- (a) Removal of impervious surfaces;
 - (b) Restoring natural bank slopes; or
 - (c) Increasing the cover and diversity of native vegetation

Riparian buffer means a designated area adjoining the Willamette River intended to maximize the ecological, scenic, and aesthetic values of the river through maintaining the natural river corridor, minimizing erosion, nutrient, and pollutant loading, providing for filtration and infiltration of stormwater runoff, and stabilizing slopes to prevent landslides and accelerated rates of channel migration contributing to sedimentation. The riparian buffer is measured from the ordinary high water line.

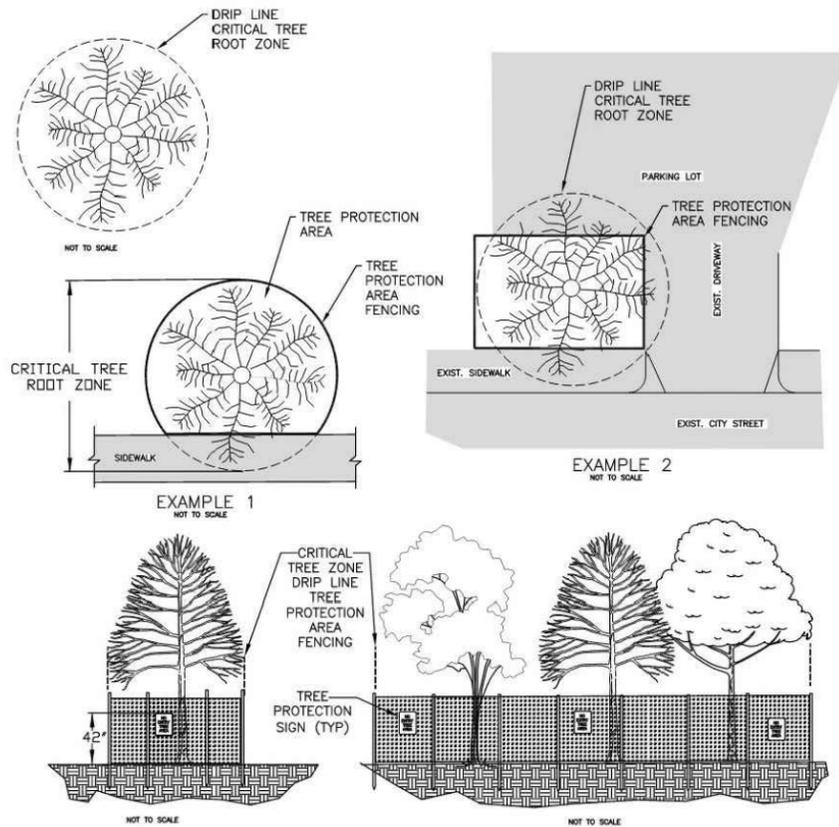


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OR22: CENTER ST BRIDGE (SALEM) PROJECT
WILLAMINA-SALEM HIGHWAY
MARION & POLK COUNTIES

Designer: Grace Youngblood, LD Reviewer: Colleen Wolfe, PLA
Drafter: Grace Youngblood, LD Checker: Jared Trowbridge, PE

PLANTING DETAILS SHEET NO. FA06

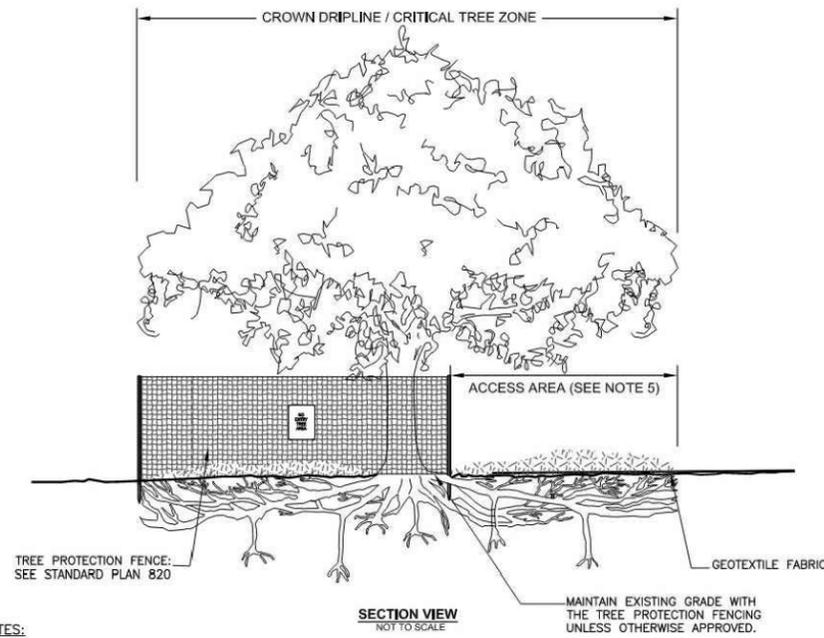


- NOTES:**
- FENCE SHALL BE 4 TO 6 FT. IN HEIGHT AND SET AT TREE DRIP LINE, OR EDGE OF HARDSCAPE AS INDICATED ABOVE, OR AS DIRECTED BY CITY'S URBAN FORESTER.
 - FENCE MATERIALS SHALL CONSIST OF A 4 FT. HIGH, HIGH DENSITY POLYETHYLENE FENCING WITH 3.5" X 1.5" OPENINGS ORANGE COLOR, AND 1.25" X 6" "T" POSTS, OR APPROVED EQUAL.
 - POSTS SHALL BE SPACED 8 FEET APART AND SHALL BE DRIVEN TO SUCH A DEPTH TO SECURELY ANCHOR THE POSTS, TYPICALLY 18" DEEP.
 - FENCE SHALL REMAIN IN PLACE UNTIL THE COMPLETION OF ADJACENT CONSTRUCTION ACTIVITIES. MOVEMENT OR REMOVAL OF FENCE REQUIRES APPROVAL BY CITY'S AUTHORIZED REPRESENTATIVE.
 - NO EQUIPMENT SHALL OPERATE INSIDE THE TREE PROTECTIVE FENCING INCLUDING DURING FENCE INSTALLATION AND REMOVAL UNLESS APPROVED BY THE URBAN FORESTER.
 - PLACE TREE PROTECTION SIGNS (STANDARD PLAN 822) SECURELY ON TREE PROTECTION FENCING WITH WIRE TIES OR PLASTIC "ZIP" TIES. SIGN SHALL BE PLACED 42 INCHES ABOVE GROUND ON 50 FT. SPACING, O.C. AND MUST BE CLEARLY VISIBLE TO CONTRACTORS AND PUBLIC.

**CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS
STANDARD PLAN
TREE PROTECTION FENCING**

APPROVED		6/03/16	DRAWN BY	ALT	6/2016	NO.820
	CITY ENGINEER	DATE	CHECKED BY	KLR	6/2016	

1 City of Salem Detail NO.820- Tree Protection Fencing
FB01 Not to Scale



NOTES:

- SEE SPECIFICATIONS FOR ADDITIONAL TREE PROTECTION REQUIREMENTS.
- IF THERE IS NO EXISTING IRRIGATION, SEE SPECIFICATIONS FOR WATERING REQUIREMENTS.
- TREE PRUNING SHALL BE PREPARED BY A CERTIFIED ARBORIST.
- NO EQUIPMENT SHALL OPERATE INSIDE THE TREE PROTECTION FENCING INCLUDING DURING FENCE INSTALLATION AND REMOVAL UNLESS OTHERWISE APPROVED BY THE URBAN FORESTER.
- FOR ACCESS WITHIN THE CRITICAL TREE ZONE WHERE MATERIALS WILL BE STORED TEMPORARILY OR WHERE OPERATING OR MOVING EQUIPMENT IS PERMITTED, PLACE GEOTEXTILE FABRIC ON GROUND SURFACE WHERE THE ACCESS AREA IS LOCATED WITHIN THE CRITICAL TREE ZONE. PROVIDE ONE OF THE SURFACE PROTECTION MEASURES LISTED BELOW OVER THE GEOTEXTILE FABRIC:
 - APPLY 6-INCHES TO 12-INCHES OF WOOD MULCH.
 - PLACE 3/4-INCH PLYWOOD OR 4" X 4" WOODEN BEAMS OVER A MINIMUM 4-INCHES OF WOOD CHIP MULCH.
 - APPLICATION OF 4-INCHES TO 6-INCHES OF 3" OPEN GRADED ROCK.
 - PLACEMENT OF STEEL PLATES ON TOP OF A MINIMUM 4-INCH LAYER OF WOOD MULCH.
 - PLACEMENT OF COMMERCIAL OR LOGGING ROAD MATS ON TOP OF A MINIMUM 4-INCH LAYER OF WOOD CHIP MULCH.



**CITY OF SALEM
DEPARTMENT OF PUBLIC WORKS
STANDARD PLAN
MITIGATION MEASURES FOR WORK
IN CRITICAL TREE AREA**

APPROVED		6/03/16	DRAWN BY	ALT	12/2015	NO.821
	CITY ENGINEER	DATE	CHECKED BY	KLR	12/2015	

2 City of Salem Detail NO.821 - Mitigation Measures for Work Critical Tree Area
FB01 Not to Scale

TREE PROTECTION NOTES

- The Critical Root Zone (CRZ) is defined as one foot radius per inch of trunk diameter or the crown radius, whichever is greater, as depicted on the Tree Protection and Removal Plan.
- Trees to be retained shall be protected by construction fencing. See City of Salem Tree Protection detail. The protection fencing shall not be moved, removed, or entered by equipment, and shall be installed prior to any development activities including clearing, grading, excavation and demolition work, and maintained in good repair throughout construction.
- Signage designating the protection zone shall be secured in a prominent location periodically on protection fences. Signs shall state required protection language in accordance with jurisdictional requirements.
- Erosion control measures required to be installed within a CRZ, if any, shall not be trenched to avoid root damage. Instead, use straw wattles staked into the ground, compost socks, or other similar methods that do not require trenching.
- Where trees that are planned for removal are located within the CRZ of protected trees, tree protection fencing may be temporarily opened or adjusted to allow access. Directionally fell trees away from protected trees. Stumps of trees planned for removal that are located within the CRZ of protected trees should remain in the ground where feasible. Otherwise, stumps may be removed by stump grinding or else extracted from the ground under the on-site supervision of the City of Salem arborist.
- The following is not allowed within CRZs unless approved in advance by the City of Salem arborist in coordination with the applicable jurisdictional requirements:
 - Ground disturbance or construction activity including vehicle and equipment access or maneuvering;
 - Staging, dumping or storage of materials such as building supplies, soil, waste items, or chemically injurious materials and liquids including paints, thinners, cleaning solutions, petroleum products, and concrete or dry wall excess, construction debris, or run-off;
 - Equipment, including passage or parking of vehicles or equipment and machinery repair or cleaning; Grade change of cut and fill during or after construction, new impervious surfaces, or utility or drainage field placement or trenching, except as specifically depicted on the tree removal and protection plan and conducted in accordance with tree protection notes below.
- The CRZ may be entered for tasks like surveying, measuring and sampling. Fences must be closed upon completion of these tasks.
- Removal of existing infrastructure shall proceed with caution to minimize damage to roots of protected trees. The contractor shall coordinate with the City of Salem arborist to provide on-the-ground recommendations if tree roots are exposed beneath existing hardscape. Root pruning, if needed, shall be conducted under the direction of the City of Salem arborist.
- Excavation shall be avoided within the CRZ unless otherwise directed by the City of Salem arborist.
- Changes in soil hydrology due to soil compaction and site drainage within CRZs shall be avoided. Excessive site run-off shall be directed to appropriate storm drainage facilities and away from trees designated for conservation or protection.

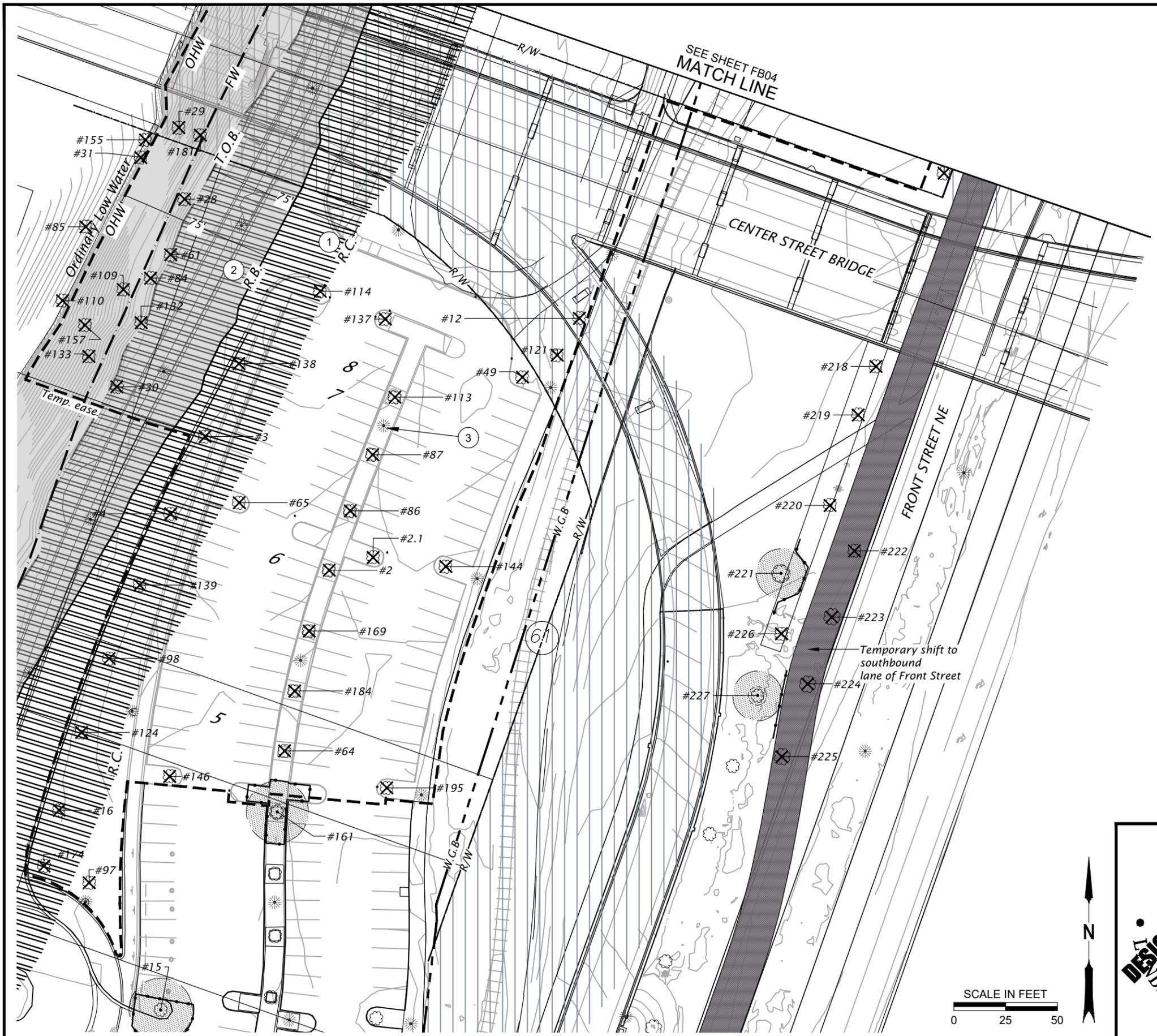


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OR22: CENTER ST BRIDGE (SALEM) PROJECT
WILLAMINA-SALEM HIGHWAY
MARION & POLK COUNTIES

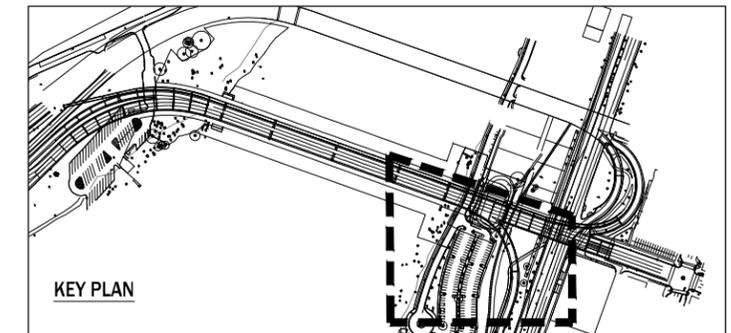
Designer: Grace Youngblood, LD Reviewer: Colleen Wolfe, PLA
Drafter: Grace Youngblood, LD Checker: Jared Trowbridge, PE

TREE PROTECTION DETAILS & NOTES SHEET NO. FB01



- LEGEND**
- L.O.W. --- Limit of Work
 - O.H.W. — Approx. Ordinary High Water line
 - T.O.B. — Approximate Top of Bank line
 - W.G.B. --- Willamette Greenway Boundary
 - ||||| R/W ||||| ODOT Right-of-Way
 - P.E. — Permanent Easement
 - T.E. — Temporary Easement
 - ||||| R.B. ||||| Riparian Buffer
 - ||||| R.C. ||||| Riparian Corridor
 - FW --- FEMA 100 Year Floodway
 - #XX (with X in circle) Existing tree to be removed
 - #XX (with tree symbol) Tree or tree massing to protect
 - (with fence symbol) Tree protection fencing
 - (with circle symbol) Tree protection area at root zone
 - (with circle symbol) Approximate tree crown

- Refer to tree codes & tables on sheets FB06 - FB07.
- KEY NOTES:**
- ① Riparian Corridor measured 75 feet horizontally from the from Top of Bank (T.O.B) on each side of a waterway.
 - ② Riparian Buffer measured 75' from Ordinary High Water in this location.
 - ③ Existing light to be salvaged and reinstalled, see illumination plans.

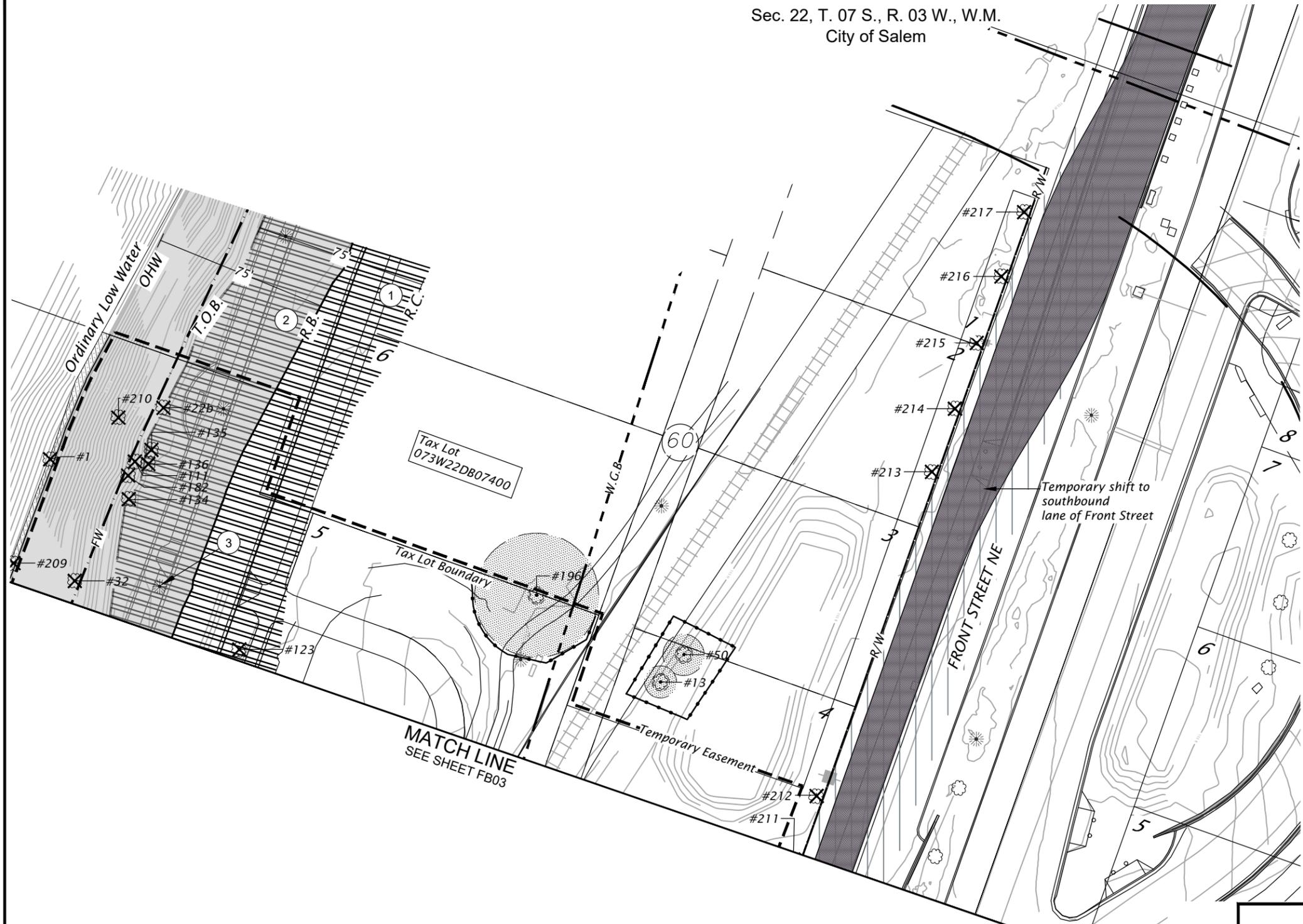


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OR22: CENTER ST BRIDGE (SALEM) PROJECT WILLAMINA-SALEM HIGHWAY MARION & POLK COUNTIES		
<small>Designer: Grace Youngblood, LD</small>	<small>Reviewer: Colleen Wolfe, PLA</small>	
<small>Drafter: Grace Youngblood, LD</small>	<small>Checker: Jared Trowbridge, PE</small>	
TREE PROTECTION & REMOVAL PLAN		SHEET NO. FB03

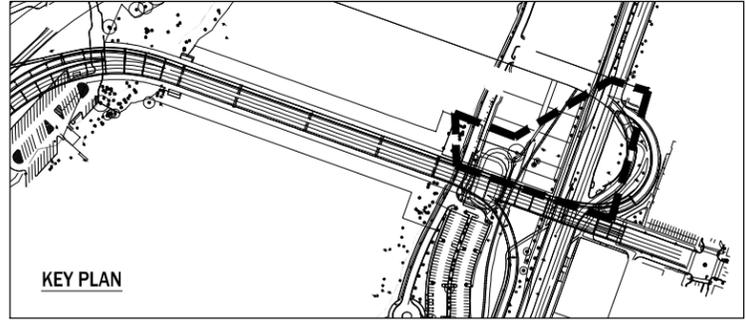
Sec. 22, T. 07 S., R. 03 W., W.M.
City of Salem

??V-???



- LEGEND**
- L.O.W. --- Limit of Work
 - O.H.W. — Approx. Ordinary High Water line
 - T.O.B. — Approximate Top of Bank line
 - W.G.B. --- Willamette Greenway Boundary
 - R/W --- ODOT Right-of-Way
 - P.E. — Permanent Easement
 - T.E. — Temporary Easement
 - Revegetation Area Below OHW
 - R.B. Riparian Buffer
 - R.C. Riparian Corridor
 - FW FEMA 100 Year Floodway
 - #XX Existing tree to be removed
 - #XX Tree or tree massing to protect
 - Tree protection fencing
 - Tree protection area at root zone
 - Approximate tree crown

- Refer to tree codes & tables on sheets FB06 - FB07.
- KEY NOTES:**
- 1 Riparian Corridor measured 75 feet horizontally from the from Top of Bank (T.O.B) on each side of a waterway.
 - 2 Riparian Buffer measured 75' from Ordinary High Water in this location.
 - 3 Existing light to be salvaged and reinstalled, see illumination plans.



MATCH LINE
SEE SHEET FB03



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WILLAMINA-SALEM HIGHWAY
MARION & POLK COUNTIES

Designer: Grace Youngblood, LD Reviewer: Colleen Wolfe, PLA
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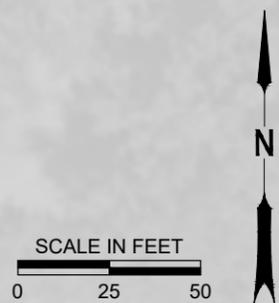
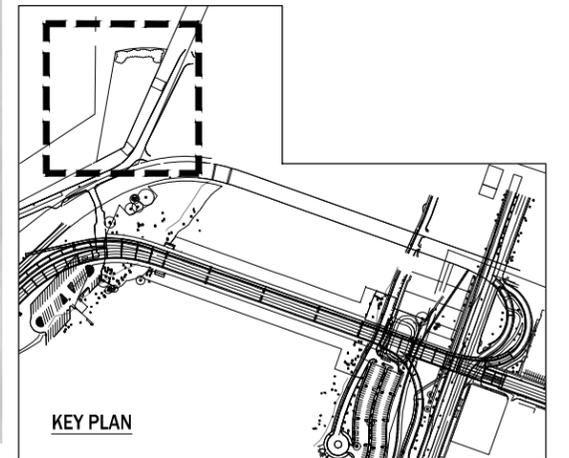
TREE PROTECTION & REMOVAL PLAN

SHEET NO.
FB04



- LEGEND**
- L.O.W. --- Limit of Work
 - W.G.B. --- Willamette Greenway Boundary
 - R/W --- ODOT Right-of-Way
 - P.E. — Permanent Easement
 - T.E. — Temporary Easement
 - R.B. ■ Riparian Buffer
 - FW --- FEMA 100 Year Floodway
 - Tree protection fencing
- Refer to tree codes & tables on sheets FB06 - FB07.

- KEY NOTES:**
- ① Existing vegetation to remain and protect.



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Designer: Grace Youngblood, LD Drafter: Grace Youngblood, LD	Reviewer: Colleen Wolfe, PLA Checker: Jared Trowbridge, PE
TREE PROTECTION & REMOVAL PLAN	
SHEET NO. FB05	

EXISTING TREE TABLE (EAST) - CITY OF SALEM OWNED TO REMOVE

Table with columns: NUMBER, SIZE (DBH)*, SPECIES, STATUS, NOTES. Lists trees to be removed in the East, including species like POPULUS TRICHOCARPA, ACER MACROPHYLLUM, and ZELKOVA SERRATA.

EXISTING TREE TABLE (EAST) - CITY OF SALEM OWNED TO PROTECT

Table with columns: NUMBER, SIZE (DBH)*, SPECIES, CRZ, STATUS, NOTES. Lists trees to be protected in the East, including species like QUERCUS RUBRA and ZELKOVA SERRATA.

EXISTING TREE TABLE (WEST) - CITY OF SALEM OWNED TO REMOVE

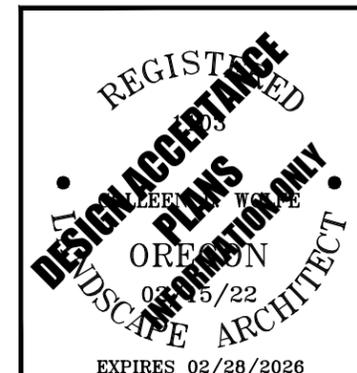
Table with columns: NUMBER, SIZE (DBH)*, SPECIES, STATUS, NOTES. Lists trees to be removed in the West, including species like FRAXINUS LATIFOLIA, ACER MACROPHYLLUM, and POPULUS TRICHOCARPA.

EXISTING TREE TABLE (WEST) - CITY OF SALEM OWNED TO PROTECT

Table with columns: NUMBER, SIZE (DBH)*, SPECIES, TPZ, STATUS, NOTES. Lists trees to be protected in the West, including species like ACER MACROPHYLLUM and FRAXINUS LATIFOLIA.

GENERAL NOTES:

- 1. DBH = Diameter at Breast Height.
2. Below Ordinary High Water line (O.H.W.) assumed to be under jurisdiction of Department of State Lands (D.S.L).
3. See FB07 for Riparian Corridor (R.C.) definition.



Project information block including DOWL logo, project name 'OR22: CENTER ST BRIDGE (SALEM) PROJECT', designer/rafter 'Grace Youngblood, LD', reviewer/checker 'Colleen Wolfe, PLA/Jared Trowbridge, PE', and sheet number 'FB06'.

CITY OF SALEM REMOVAL & REPLACEMENT REQUIREMENTS

CITY OF SALEM CODE OF ORDINANCES

Chapter 600. - Willamette Greenway

Sec. 600.025 - Development Standards

- (1) Landscaping shall conserve, or if disturbed by the development activity restore to the greatest extent possible, vegetative cover within the Willamette Greenway Boundary. Landscaping is not required where it would significantly interfere with a water-dependent or water-related use or activity.
- (2) Native vegetation removed from the riparian buffer shall be replaced with native vegetation which is compatible with and enhances the functions of the riparian buffer.
- (3) Trees and shrubs shall be provided as follows:
 - (A) A minimum of one tree shall be provided for every 20 feet of river frontage.
 - (B) A minimum of one shrub shall be provided for every two feet of river frontage.
 - (C) All trees and shrubs shall be planted within and generally riverward of the Willamette Greenway Boundary.
 - (D) Groupings of trees, shrubs, or both are encouraged, particularly along the riverbank.

Riparian Buffer Enhancement:

- (i) *Enhancement shall be provided at ratio of 0.25:1, where 0.25 represents enhancement area and 1 represents development area.
*Enhancement areas are consistent with the Enhancement Guide (2019) and the Native Plant List.

*If the floodway is wider than the required 75 feet, the riparian buffer shall extend to the floodway boundary per Salem Unified Development Code (UDC) Section 600.025, Table 600-2, Riparian Buffer Method 2.

*When the riparian buffer measures more than 125 feet, depending on the bank slope, from the OHWL, the property shall receive credit for meeting the wider riparian buffer mitigation measure under SRC 600.025(c)(3)(A).

East Bank

- Assume 25% slopes or greater.
- 75 foot Riparian Buffer in this location.

West Bank

- Assume 25% slopes or greater.
- Floodway is greater than 75 feet, therefore Riparian Buffer goes to the extent of the floodway.
- Floodway is greater than 125 feet, therefore the property receives credit for meeting the wider riparian buffer mitigation measure.

Riparian Corridor Boundary is measured 75 feet horizontally from the top of bank on each side of a waterway

CHAPTER 806 : OFF-STREET PARKING, LOADING

Sec. 806.035.- Off-street parking and vehicle use are developments standards for uses or activities other than single family, two family, three family or four family

- 1. Climate Mitigation: developments that include a total of more than one-half acre of new off-street surface parking shall provide one or more of climate mitigation measures.

Increased tree canopy coverage:

- 40% coverage of new off-street parking and vehicle use areas in more than 15-years.
- Canopy area based off Table 806-7
- Tree trunks must be located within 10' of parking area

Sec. 806.040. - Driveway Development Standards

Setbacks and Landscaping

- Required for driveways abutting street and abutting interior front, side and rear property lines
- Where landscaping is required under the UDC without a reference to a specific landscaping type, the required landscaping shall meet the Type A standard.

Sec. 808.055 - Tree and Native Vegetation Replacement Standards within Riparian Corridors*

All trees shown in plans are publicly owned. Trees have been replaced at a minimum at ratio 1:1.

All trees and shrubs replaced within the riparian corridor are native to the Willamette Valley.

Riverfront Parking lot trees are to be planted at 1.5" caliper and have been selected from approved species in the Tree and Vegetation Technical Manual.

TREE REMOVAL AND REPLACEMENT TABLE: See Plant Schedules on sheet FA05

TREE REMOVAL TOTAL	
WEST	EAST
(19) Trees removed within Riparian Corridor boundary	(16) Trees removed within Riparian Corridor boundary
(35) Trees removed outside of Riparian Corridor boundary	(50) Trees removed outside of Riparian Corridor boundary
(54) Trees removed total	(66) Trees removed total
TREE REPLACEMENT TOTAL	
WEST	EAST
(19) Trees replaced within Riparian Corridor to 1:1 ratio @ 1.5" cal (per 808.055)	(16) Trees replaced in Riparian Corridor to 1:1 ratio @ 1.5" cal (per 808.055)
(35) Trees replaced above the Riparian Enhancement Buffer and not required by Riparian Corridor replacement requirements at 1:1 ratio @1.5" caliper.	(50) Trees replaced above the Riparian Enhancement Buffer and not required by Riparian Corridor replacement requirements at 1:1 ratio @1.5" cal
(263) Trees replaced within Riparian Enhancement Buffer to Escarpment & Outer Section Requirements (7' O.C.)	(168) Trees replaced within Riparian Enhancement Buffer to Escarpment Section Requirements (7' O.C.)
(327) Total Tree Replacements	(234) Total Tree Replacement
•• (54) Trees at 1.5" caliper	•• (58) Trees at 1.5" caliper
•• (263) Trees at #1 container	•• (8) Trees at 5-6' height
•• (10) Trees at #5 container	•• (155) Trees at #1 container
	•• (13) Trees at #5 container

DEPARTMENT OF STATE LANDS REPLACEMENT REQUIREMENTS

Oregon Secretary of State Administrative Rules:

Chapter 141 – Administrative Rules Governing the Issuance and Enforcement of General Permits within Water of this State.

1. Sec. 141-093-0141 – Transportation Related Structures

(h). Streambanks and newly exposed floodplains must provide or be planted to achieve a density of at least 1 live native shrub or tree per 6 linear feet, except under a bridge or within areas needed for site distance and safety, unless otherwise approved by the Department.



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Drafter: Grace Youngblood, LD	Checker: Jared Trowbridge, PE	
TREE REMOVAL & REPLACEMENT REQUIREMENTS		SHEET NO. FB07